



MISSISSAUGA

DECLARATION

Section 17 of the Planning Act

Applicant: Queenscorp Group

Municipality: City of Mississauga

Our File: OPA 75

I, Sacha Smith, Deputy Clerk, solemnly declare,

1. That the decision in respect of the above-noted matter was made on January 24, 2018 when By-law Number 0009-2018 was enacted and that notice as required by Section 17 of the Planning Act was given on February 1, 2018.
2. That no appeal to the Ontario Municipal Board of the decision in respect of the above-noted matter was received under Section 17 of the Planning Act within the time specified for submitting an appeal.

And I make this solemn declaration conscientiously believing it to be true and knowing that it is of the same force and effect as if made under oath.

Declared before me

at the City of Mississauga

in the Regional Municipality of Peel

This 21st day of February, 2018.

Commissioner of Oaths

Donabelle Simtha Higgs, a Commissioner, etc.,
Province of Ontario, for the
Corporation of the City of Mississauga.
Expires September 24, 2020.

Declarant
Sacha Smith



MISSISSAUGA

NOTICE OF THE PASSING OF AN OFFICIAL PLAN AMENDMENT AND A ZONING BY-LAW

DATE OF NOTICE	February 01, 2018	
OPA NUMBER	OPA 75 (By-law 0009-2018)	
ZONING BY-LAW NUMBER	0010-2018	
DATE PASSED BY COUNCIL	January 24, 2018	
LAST DATE TO FILE APPEAL	February 20, 2018	
FILE NUMBER	OZ 16/002	Ward 1
APPLICANT	Queenscorp Group	
PROPERTY LOCATION	The lands are located on the west side of Cawthra Rd. south of Atwater Ave.	

TAKE NOTICE that on January 24, 2018 the Council of the Corporation of the City of Mississauga passed the above noted Official Plan Amendment OPA 75 and Zoning By-law, under Section 17 or 21 of the Planning Act, R.S.O., 1990, c.P.13, as amended.

THE PURPOSE AND EFFECT of the Official Plan Amendment is to amend the Special Site 1 policies in the Mineola Neighbourhood Character Area to permit horizontal multiple dwellings.

The purpose of the Zoning By-law is to permit 148 horizontal multiple dwellings on the subject property. **The Zoning By-law shall not come into force until Mississauga Official Plan Amendment Number 75 is in full force and effect.**

The decision of Council is final if a notice of appeal is not received on or before the last day for filing a notice of appeal.

IF YOU WISH TO APPEAL to the Ontario Municipal Board (OMB), a copy of an appeal form is available from the OMB website at www.omb.gov.on.ca An appeal must be filed with the Clerk of the City of Mississauga, Attention: Diana Rusnov, 300 City Centre Drive, Mississauga, Ontario L5B 3C1 no later than **February 20, 2018**.

Only individuals, corporations and public bodies may appeal a by-law to the Ontario Municipal Board. A notice of appeal may not be filed by an unincorporated association or group. However, a notice of appeal may be filed in the name of an individual who is a member of the association or the group on its behalf.

No person or public body shall be added as a party to the hearing of the appeal unless, before the official plan amendment and/or by-law was passed, the person or public body made oral submissions at a public meeting or written submissions to the City of Mississauga Council or, in the opinion of the Ontario Municipal Board, there are reasonable grounds to add the person or public body as a party.

The Notice of Appeal must:

- 1) set out reasons for the appeal;
- 2) be accompanied by the fee in the amount of \$300.00 payable to the Minister of Finance, and
- 3) be accompanied by a fee of \$150.00, payable City of Mississauga.
- 4) Four (4) copies of the appeal package.

MORE INFORMATION: A copy of the Official Plan Amendment and Zoning By-law in their entirety can be found at www.mississauga.ca/portal/cityhall/publicnotices or from **Ashlee Rivet** of the City of Mississauga, Planning and Building Department at (905) 615-3200 X 5751, or in person at the Office of the City Clerk, 300 City Centre Drive, 2nd Floor, Mississauga, Ontario.

Sacha Smith, Deputy Clerk
Legislative Services,
Corporate Services Department
905-615-3200 X 4516

Amendment No. 75
to
Mississauga Official Plan

By-law No. 0009-2018

A by-law to Adopt Mississauga Official Plan Amendment No. 75

WHEREAS in accordance with the provisions of sections 17 or 21 of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, ("*Planning Act*") Council may adopt an Official Plan or an amendment thereto;

AND WHEREAS, pursuant to subsection 17(10) of the *Planning Act*, the Ministry of Municipal Affairs authorized the Regional Municipality of Peel ("Region" or "Regional"), an approval authority, to exempt from its approval any or all proposed Local Municipal Official Plan Amendments;

AND WHEREAS, Regional Council passed By-law Number 1-2000 which exempted all Local Municipal Official Plan Amendments adopted by local councils in the Region after March 1, 2000, provided that they conform with the Regional Official Plan and comply with conditions of exemption;

AND WHEREAS, the Commissioner of Public Works for the Region has advised that, with regard to Amendment No. 75, in his or her opinion the amendment conforms with the Regional Official Plan and is exempt;

AND WHEREAS, Council desires to adopt certain amendments to Mississauga Official Plan regarding changes to the Special Site 1 policies for the Mineola Neighbourhood Character Area;

NOW THEREFORE the Council of The Corporation of the City of Mississauga ENACTS as follows:

1. The document attached hereto, constituting Amendment No. 75 to Mississauga Official Plan, is hereby adopted.

ENACTED and PASSED this 24 day of January, 2018.

Signed Bonnie Cumbee

MAYOR

Signed [Signature]

CLERK

Amendment No. 75
to
Mississauga Official Plan

The following text constitutes Amendment No. 75.

Also attached but not constituting part of the Amendment are Appendices I and II.

Appendix I is a description of the Public Meeting held in connection with this Amendment.

Appendix II is a copy of the Planning and Building Department report dated April 7, 2017, pertaining to this Amendment.

PURPOSE

The purpose of this Amendment is to amend the Special Site 1 policies in the Mineola Neighbourhood Character Area to permit horizontal multiple dwellings.

LOCATION

The lands affected by this Amendment are located on the west side of Cawthra Road, south of Atwater Avenue. The subject lands are located in the Mineola Neighbourhood Character Area, as identified in Mississauga Official Plan.

BASIS

Mississauga Official Plan came into effect on November 14, 2012, save and except for the outstanding site specific appeals to the Ontario Municipal Board.

The subject lands are designated Residential Medium Density and are within the area subject to Special Site 1 policies in the Mineola Neighbourhood Character Area. The policies permit detached, semi-detached and townhouse dwellings within a net density range of 25-37 units per net residential hectare.

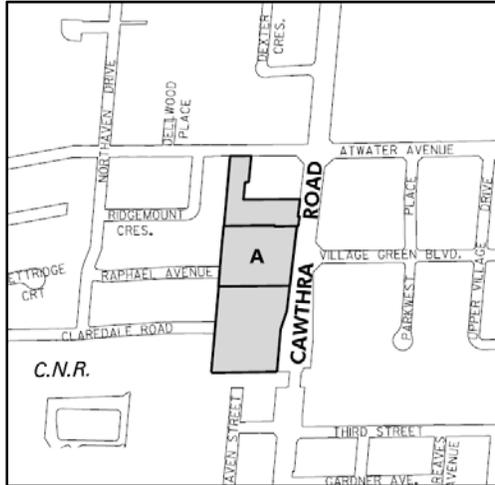
The proposed Amendment to the Special Site 1 policies adds horizontal multiple dwellings as a permitted use with a permitted density of 112 units per net residential hectare.

The proposed Amendment is acceptable from a planning standpoint and should be approved for the following reasons:

1. The proposed development is appropriate as the subject site is located on the periphery of the Mineola Neighborhood Character Area and will not impact the stable residential neighborhood character of Mineola.
2. Cawthra Road is a designated Corridor in Mississauga Official Plan, where growth is to be directed.
3. The proposed built form is appropriate given the surrounding medium density land uses, and the development has been designed to minimize impacts from shadowing and overlook onto adjacent low density properties.

DETAILS OF THE AMENDMENT AND POLICIES RELATIVE THERETO

1. Section 16.18.5, Site 1, Special Site Policies, Mineola Neighbourhood Character Area, of Mississauga Official Plan, is hereby amended by deleting Special Site 1 map and replacing with the following:



2. Section 16.18.5, Site 1, Special Site Policies, Mineola Neighbourhood Character Area, of Mississauga Official Plan, is hereby amended by adding the following:

16.18.5.1.3 Notwithstanding the provisions of the Residential Medium Density designation, for the lands identified as Area A, horizontal multiple dwellings to a maximum density of 112 units per ***net residential hectare*** will also be permitted.

IMPLEMENTATION

Upon the approval of this Amendment by the Council of the Corporation of the City of Mississauga, Mississauga Official Plan will be amended in accordance with this Amendment.

The lands will be rezoned to implement this Amendment.

This Amendment has been prepared based on the Office Consolidation of Mississauga Official Plan August 2, 2017.

INTERPRETATION

The provisions of Mississauga Official Plan, as amended from time to time regarding the interpretation of that Plan, will apply in regard to this Amendment.

This Amendment supplements the intent and policies of Mississauga Official Plan.

APPENDIX I

PUBLIC MEETING

All property owners within a radius of 120 m of the subject lands were invited to attend a Public Meeting of the Planning and Development Committee held on December 5, 2016 in connection with this proposed Amendment.

A number of area residents were in attendance at the December 5, 2016, Planning and Development Committee Meeting and raised issues relating to the proposed development including the appropriateness of the built form, height and density; traffic and signal warrants; school accommodations; visitor parking; access to Cawthra Road and internal road connections; and, storm water management. These issues have been resolved by increasing setbacks to adjacent properties, setting back roof top terraces from the building edges to minimize overlook conditions, and submitting the necessary studies and justification for matters concerning traffic, signal warrants, parking, access and road connection design, and storm water management.

City of Mississauga
Corporate Report



Date: April 7, 2017

To: Chair and Members of Planning and Development Committee

From: Edward R. Sajecki, Commissioner of Planning and Building

Originator's file:
OZ 16/002 W1

Meeting date:
2017/05/01

Subject

RECOMMENDATION REPORT (WARD 1)

Applications to permit 148 horizontal multiple dwellings on a private condominium road 1174-1206 Cawthra Road

West side of Cawthra Road, south of Atwater Avenue

Owner: Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Recommendation

1. That notwithstanding that subsequent to the public meeting, changes to the applications have been proposed, Council considers that the changes do not require further notice and, therefore, pursuant to the provisions of subsection 34(17) of the *Planning Act*, any further notice regarding the proposed amendment is hereby waived.
2. That the applications under File OZ 16/002 W1, 1174 -1206 Cawthra Road to amend Mississauga Official Plan to **Residential Medium Density – Special Site** and to change the zoning to **RM9 – Exception** (Horizontal Multiple Dwellings with more than 6 dwelling units) to permit 148 horizontal multiple dwellings on a private condominium road in conformity with the provisions outlined in Appendix 6, be approved subject to the conditions referenced in the staff report.
3. That the applicant agree to satisfy all of the requirements of the City and any other external agency concerned with the development.
4. That the decision of Council for approval of the rezoning application be considered null and void, and a new development application be required unless a zoning by-law is passed within 18 months of the Council decision.

5. Notwithstanding subsection 45.1.3 of the *Planning Act*, subsequent to Council approval of the development application, the applicant can apply for a minor variance application, provided that the height and FSI shall remain the same.

Report Highlights

- Comments were received from the public regarding appropriateness of the built form; height and density; traffic and signal warrants; school accommodation; visitor parking; access to Cawthra Road and internal road connections; and, storm water management
- The applicant has made minor revisions to the proposal to address issues raised at the public meeting and by Planning staff, including a reduction in the number of proposed units, increases to some of the minimum internal setbacks, addition of road connections to the properties to the north and south and relocation of the garbage staging area to a more central location on the property
- Staff are satisfied with the changes to the proposal and find it to be acceptable from a planning standpoint, and recommend that the applications be approved

Background

A public meeting was held by the Planning and Development Committee on December 5, 2016, at which time an Information Report (Appendix 1) was received for information.

Recommendation PDC-0089-2016 was then adopted by Council on December 14, 2016.

1. That the report dated November 15, 2016, from the Commissioner of Planning and Building regarding the applications by Queenscorp (Cawthra South) Inc. to permit 154 horizontal multiple dwellings on a private condominium road under File OZ 16/002 W1, 1174-1206 Cawthra Road, be received for information.
2. That the oral submissions made at the Planning and Development Committee Meeting dated December 5, 2016 be received.

Subsequent to the public meeting, meetings were held with the applicant to try to have the proposal revised to address concerns raised by the public and staff. Meetings were also held with adjacent property owners. These are discussed in the Comments section of this report.

Comments

See Appendix 1 – Information Report prepared by the Planning and Building Department.

REVISED DEVELOPMENT PROPOSAL

The applicant has made some minor modifications to the proposed concept plan including:

- Reducing the number of proposed units from 154 to 148

- Increasing the minimum internal setbacks throughout the development, including adjacent to the existing detached homes to the north and south and between the proposed road interconnection and ramp to the underground garage
- Protecting for an internal road connection to the properties to the north and south
- Relocating the proposed waste staging area to the interior of the development, away from existing neighbouring homes

COMMUNITY COMMENTS

Since the December 5, 2016 Public meeting and the community meeting held prior to that on September 27, 2016 by Ward 1 Councillor Jim Tovey, City and Region of Peel staff have met with the adjacent property owners to discuss the proposed development.

On March 14, 2017, Planning and Building, Transportation and Works and Region of Peel staff met with 8 unit owners from Peartree Estates at 1130 Cawthra Road (the existing condominium townhouse development, located one property south of the development site). During the meeting, the residents expressed the following concerns:

- The proposed development is too dense. A density that is similar to what exists at 1130 Cawthra Road would be more appropriate
- The proposed internal road connection from 1130 Cawthra Road through the development site to the north and out to Cawthra Road is not wanted
- Traffic signals are needed at Cawthra Road and Village Green Boulevard
- The construction of the sales trailer on-site and the pre-sale of units is inappropriate prior to Council's consideration and potential approval of any redevelopment of the subject lands

Staff is in receipt of a petition signed by 41 of the 52 Peartree Estates condominium owners opposing the proposed internal road connection. Comments on the petition indicate that there may be a misunderstanding about information on the road connection. For example, a number of comments speak to Peartree Estates' road no longer being a private road. This is not the case; Peartree Estates' road will remain a private road regardless of a connection to the north. Additionally, several comments imply that Peartree Estates' existing access to Cawthra Road would be lost. This is not correct; the Region of Peel will restrict the existing access to a right-in and right-out only, thereby removing the current left-in movement.

Planning and Building and Region of Peel staff also met with the owners of the property at 1168 Cawthra Road (immediately south of the development site) on several occasions. While they are not opposed to a road connection through the rear of their property, they have expressed concerns with the configuration of the proposed internal road connection and the impacts this will have on the future redevelopment potential of the rear of their property.

On March 16, 2017, Planning and Building, Transportation and Works and Region of Peel staff met with a representative for the 3 properties north of the development site. The representative was seeking clarification regarding access arrangements to Cawthra Road and grading. He

indicated his desire to use the proposed road connection through the development site to the south to access Cawthra Road, should he redevelop his property at some point in the future.

The following summarizes issues raised at all the above mentioned meetings.

Comment

The proposed development is out of character with the surrounding neighbourhood. It is too dense and too tall.

Response

Although the interior of the Mineola neighbourhood is characterized by detached and semi-detached homes, Cawthra Road, which is identified as a Corridor in Mississauga Official Plan (MOP), is distinctly different. Cawthra Road is the boundary between the Mineola and Lakeview neighbourhoods and is characterized by a wider range of uses and built forms. The area between Atwater Avenue and the CN rail corridor is designated **Residential High Density** and **Residential Medium Density** and is therefore intended to accommodate higher density residential uses than the interior of the Neighbourhood. The appropriateness of the proposed horizontal multiple dwelling development is assessed based on how compatible it is to the existing and planned character of the area. Further comments regarding the appropriateness of the development are included in the Planning Comments section of this report.

Comment

Traffic is already an issue on Cawthra Road, especially during school drop off/pick up hours. Additional traffic generated by the proposed development and other developments in the immediate area will create unsafe conditions. There should be a traffic signal installed at the intersection of Cawthra Road and Village Green Boulevard.

Response

Comments from the Region of Peel regarding traffic volumes on Cawthra Road and signal warrants at Village Green Boulevard are included in the Updated Agency and City Departments section of this report. The proposed development does not satisfy the requirements to warrant traffic signals at this intersection.

Comment

Insufficient visitor parking will result in on-street parking on the surrounding residential streets.

Response

The applicant is proposing 30 visitor parking spaces, whereas 37 spaces are required based on the Zoning By-law parking rates. A Parking Study was submitted and deemed to be acceptable. The proposed parking rates reflect what has been approved for other similar developments throughout the City.

Comment

The proposed pedestrian connection from Raphael Avenue into the proposed development is a concern.

Response

MOP contains policies which encourage pedestrian oriented development, connections to adjacent streets and neighbourhoods at regular intervals, and encourages accessible paths for walking and cycling where cul-de-sacs and dead end streets exist. The pedestrian connection from Raphael Avenue through the site to Cawthra Road is appropriate and consistent with these MOP policies.

Comment

Insufficient building setbacks are proposed to the adjacent properties, which will impact the amount of available sunlight.

Response

The applicant has increased the proposed building setbacks adjacent to the side lot lines which will further increase the amount of available sunlight to adjacent properties. Furthermore, the upper floor of the proposed buildings is setback from the building edge to minimize the building mass.

Comment

The developer has already removed a number of large mature trees from the property, which is unacceptable.

Response

A number of the trees removed from the site were ash trees infested with Emerald Ash Borer and were either dead or dying. Therefore, either property standards orders or tree removal permits were issued to remove the trees.

Comment

The neighbourhood schools are at capacity and cannot accommodate any more students.

Response

Comments received from the School Boards regarding school capacity as outlined in Appendix 7 of the Information Report state that they are satisfied with the current provision of educational facilities for the catchment area.

Comment

There have been flooding issues in the area.

Response

Comments from the City Transportation and Works Department and the Region of Peel regarding storm water management are included in the Updated Agency and City Departments section of this report.

Comment

Will there be additional bus routes added to Cawthra Road to accommodate the expected population from the proposal?

Response

Comments received from the Transit Division of the City Transportation and Works Department as outlined in Appendix 7 of the Information Report state that within the next five years MiWay has plans to reconfigure transit service to modify the current Route 8 to provide north/south service along Cawthra Road.

Comment

Will there be an internal road connection from 1130 Cawthra Road (Peartree Estates) through the site to Cawthra Road?

Response

At the request of the Region of Peel and the City, the applicant has demonstrated how the proposed internal condominium road can connect to the properties to the north and south. The applicant has also agreed to provide public easements over the proposed internal condominium road to allow for the future connections. Further comments regarding access and road connections are included in the Planning Comments section of this report.

UPDATED AGENCY AND CITY DEPARTMENT COMMENTS**Region of Peel**

Comments updated April 6, 2017, state:

Functional Servicing Report/Capacity

Regional staff have reviewed the complete and satisfactory Functional Servicing Report submitted in March 2017. They have no objection to the water servicing of this proposal from the existing 300 mm (12 in.) diameter watermain but note that this watermain is under high demand during the summer months and recommend that the fire flows be recalculated once the internal building water system is designed for a more accurate fire flow. With regard to storm water servicing we note that the report, in Section 6.1, states that when water exceeds the storage capacity, an overland flow route will convey the overland flow towards Cawthra Road. Overland flow towards Cawthra Road is not acceptable and a more detailed review which includes a review of the grading at the property line will be done during the site plan stage. Detailed modelling conducted for sanitary sewer capacity on the existing 250 mm (10 in.) sanitary sewer on Cawthra Road has determined that there is available capacity under existing and future flow conditions.

Traffic Impact Study/Signals/Land Dedication

Regional staff have reviewed a satisfactory Traffic Impact Study (TIS) and functional design materials for this development. Through the review of the TIS and functional design the Region supports a full movement access onto Cawthra Road across from Village Green Boulevard, contingent upon the provision of internal connections and easements to the properties to the north and south. At this time based on the existing and future traffic volumes, this intersection does not warrant signals. This intersection will become part of the Region's intersection review conducted annually and they will monitor any changes to traffic volumes at this intersection and the potential need for signals in the future. The functional design provided by the applicant has confirmed that the gratuitous land dedication requirements for Cawthra Road (a 36 m (118 ft.) right of way) will be 18 m (59 ft.) from the centreline and a 0.3 m (1 ft.) reserve behind the property line along the frontage of Cawthra Road except at the approved access. Confirmation of any land dedication to meet the 18 m (59 ft.) from centreline and 0.3 m (1 ft.) reserve will be confirmed and dedicated through the subsequent site plan application.

North-South Internal Road

Regional staff have reviewed the north-south connecting road as shown on the applicant's revised concept plan. This review included review of the vehicle path (waste collection vehicle) and ramp site lines. The Region finds the conceptual north-south connecting road to be satisfactory.

Waste Collection Area

The alternate waste collection area shown at the stub road between Blocks 5 and 6 is satisfactory and with approval from Mississauga Fire Services, the Region is satisfied with this location as an alternate collection area as opposed to previous proposals with the collection area within the north-south connecting road.

City Transportation and Works Department

Comments updated April 4, 2017, state that a satisfactory Traffic Impact Study (TIS) including an addendum (Signal Warrant Analysis) in support of the applications concludes that the increased traffic flows as a result of the proposed development can be accommodated within the existing surrounding road network. As Cawthra Road is under the jurisdiction of the Region of Peel, final approval of the TIS, access and widenings will be required from the Region.

Additionally, the Region is requiring an interconnection to the adjacent lands north and south of the subject property to support future access restrictions for the neighbouring properties along Cawthra Road. The current plan includes the geometric design and associated mitigation measures of the interconnection to provide sufficient setback between the interconnection and the site garage ramp. Aspects related to the turning templates and circulation to accommodate fire and waste collection trucks are being addressed by the Region and Fire Services.

The Noise Impact Study submitted confirmed that with the installation of central air conditioning and registration of the appropriate noise warning clauses, compliance with Ministry of the

Environment and Climate Change Guidelines will be achieved. Notwithstanding the findings of the submitted reports and drawings, the applicant has been requested to provide additional grading details and cross sections, revised Functional Servicing and Stormwater Management Report, and final clearances from Fire and Peel Waste Collection regarding Fire/Garbage Truck Route design for the interim and ultimate design. As the above-noted items and additional specific technical details requested remain outstanding, the Transportation and Works department is not in favour of these applications proceeding to By-law enactment until the outstanding matters have been satisfactorily resolved.

In the event these applications are approved by Council and prior to enactment of the Zoning By-law, the applicant will be required to:

- Enter into a Development Agreement with the City
- Enter into an Encroachment Agreement with the City
- Establish Public Access Easements
- Provide any required securities and fees

The storm sewer outlet for these lands is the existing storm sewer system located on Cawthra Road, which is a Regional road. As part of the proposal, on-site storm water management controls will be implemented. Site specific details will be addressed through the site plan review and approval process.

City Fire & Emergency Services Department

Comments updated March 21, 2017, state that Fire has reviewed the applications from an emergency response perspective and has no concerns. Emergency response time to the site and watersupply available are acceptable. While fire route access and protection will be assessed through the site plan and building permit application processes, they have conducted a cursory review of the applicant's revised concept plan. In order to comply with Fire Route By-law 1036-81, the following is required:

- Block 7 shall be designed in conformance with Part 3 of the Ontario Building Code
- A walkway providing an unobstructed path of travel is required from the fire route to unit 401, in the area north of Block 4

The turning templates submitted for the road interconnection to the south have been prepared using the specifications for Aerial 101 which is the largest vehicle in the City's emergency services fleet. Therefore, Fire and Emergency Services has confirmed that the road design is acceptable.

PLANNING COMMENTS

Provincial Policy Statement (PPS) and Growth Plan for the Greater Golden Horseshoe

The *Provincial Policy Statement* (PPS) contains the Province's policies concerning land use planning for Ontario. All planning decisions are required to be consistent with these policies. The PPS encourages intensification of land within urban areas, promotes efficient use of infrastructure and public facilities, encourages mixed use developments and the support of public transit.

The *Provincial Growth Plan for the Greater Golden Horseshoe* (Growth Plan) directs municipalities to "identify the appropriate type and scale of development in intensification areas". It states that intensification areas will be planned and designed to "achieve an appropriate transition of built form to adjacent areas". The PPS and Growth Plan indicate that development must be governed by appropriate standards including density and scale. These policies are implemented through Mississauga's Official Plan.

The site is located at the periphery of the Mineola Neighbourhood on Cawthra Road, a Corridor in MOP. The proposed horizontal multiple dwelling development achieves a level of intensification that is deemed to be appropriate and meets the intent of the Growth Plan. The proposed development adequately take into account the existing context and provides an appropriate transition of built form to adjacent areas as referenced in the Official Plan section below.

Official Plan

The proposal requires an amendment to the MOP Policies for the Mineola Neighbourhood Character Area from **Residential Medium Density – Special Site 1** to **Residential Medium Density – Special Site** to permit horizontal multiple dwellings on a private condominium road and an increase in density to 112 units per net residential hectare (45 units per net residential acre). Section 19.5.1 of MOP provides the following criteria for evaluating site specific Official Plan Amendments:

- ***Will the proposal adversely impact or destabilize the overall intent, goals and objectives of the Official Plan; and the development or functioning of the remaining lands which have the same designation, or neighbouring lands?***
- ***Are the lands suitable for the proposed uses, and are the proposed land uses compatible with existing and future uses of the surrounding lands?***
- ***Are there adequate engineering services, community infrastructure and multi-modal transportation systems to support the proposed application?***
- ***Has a planning rationale with reference to Mississauga Official Plan policies, other relevant policies, good planning principles and the merits of the proposed amendment in comparison with the existing designation been provided by the applicant?***

Planning staff have undertaken an evaluation of the criteria against this proposed development application.

Directing Growth

The site is located in the Mineola Neighbourhood Character Area, which is generally characterized as a stable residential neighbourhood. According to the City Structure policies of MOP, Neighbourhoods are not intended to be the focus for intensification but it is recognized that these areas are not meant to stay static and that new development need not imitate previous development patterns. Where new development is proposed, it should be sensitive to the existing and planned character of the Neighbourhood and should be located in specific areas, including along Corridors such as Cawthra Road. The site is an assembly of seven properties on the west side of Cawthra Road, between Atwater Avenue and the CN rail corridor.

These properties are designated **Residential Medium Density** and are part of the lands identified as Special Site 1. Detached, semi-detached and townhomes are permitted on lands in Special Site 1, within a density range of 25-37 units per net residential hectare (10-15 units per net residential acre). The current designation and the location of the site are important factors in evaluating the appropriateness of the proposed development. The introduction of horizontal multiple dwellings on lands that already permit medium density residential uses and at the periphery of a Neighbourhood along a Corridor will not adversely impact or destabilize the overall goals and objectives of MOP.

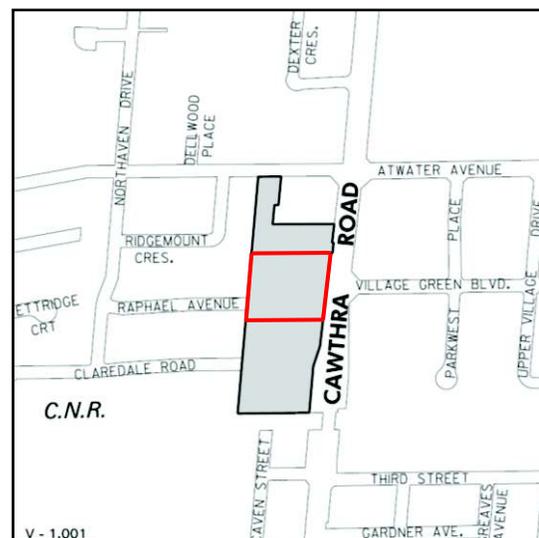


Figure 1 – Mississauga Official Plan Map of Special Site 1

Separate parcels in Special Site 1 are encouraged to be assembled. The lands at 1130 Cawthra Road (south of development site and referred to as Peartree Estates) were previously assembled and redeveloped with 52 condominium townhomes. The applicant has attempted to assemble the remaining lands in Special Site 1 but was successful in acquiring only seven of the remaining eleven parcels. There are three remaining properties north of the development site and one remaining property between the development site and Peartree Estates. The applicant has provided a concept plan demonstrating how the adjacent lands can potentially redevelop (Appendix 4).

Compatibility with the Neighbourhood

Intensification within Neighbourhoods is to be compatible in built form and scale to surrounding development and will be sensitive to the existing and planned context. The proposed horizontal multiple dwellings are a suitable built form for the site, which is surrounded by detached, semi-detached and townhomes; and, horizontal multiple dwellings and apartment buildings opposite Cawthra Road.

A Site Plan application is in process for 650 Atwater Avenue (southwest corner of Atwater Avenue and Cawthra Road) to develop the site for 110 3 ½ storey horizontal multiple dwellings on a private condominium road. The vacant lands at the opposite corner zoned for all forms of horizontal multiples and apartments up to 12 storeys. The Queenscorp development ranges in height from 3 - 3 ½ storeys, consistent with the maximum 4 storey building height in Neighbourhoods. Each building has roof top terraces which are setback from the building edges to create an appropriate transition in height, minimize overlook conditions and maximize sun exposure to adjacent properties.

Building and Site Design

The proposed development has been designed to be sensitive to the existing and planned character of the neighbourhood and provides an appropriate transition to adjacent uses. The proposed buildings have been sited to face Cawthra Road and the internal condominium road. Setbacks to Cawthra Road are consistent with what has been provided on adjacent properties on this portion of Cawthra Road, which is characterized by wide boulevards. Overlook conditions and shadow impacts have been addressed through the increase in building setbacks to property lines and the setback of roof top terraces to building edges.

The internal condominium road has been designed to align with Village Green Boulevard on the east side of Cawthra Road. The condominium road comes to a T-intersection within the development site and extends north and south to the adjacent property lines. The north/south portion of the road is parallel to the west property line which provides a greater separation distance between the proposed buildings and the existing 1 storey detached and semi-detached homes on Raphael Avenue. A number of the surface visitor parking spaces have been relocated to the underground garage to limit the impacts on the adjacent properties and to allow for a greater amount of landscaping. Although the proposed parking rates are reduced from what is required under the Zoning By-law, Planning staff are satisfied with the reduction, which reflects what has been approved on similar developments elsewhere in the City.

The buildings are designed at a pedestrian scale with articulated façade treatments that are aesthetically pleasing. The combination of light coloured materials and glass serve to break up the building mass and create visual interest. Rooftop mechanical equipment has been integrated into the building design to minimize impacts on the public realm.

A common outdoor amenity area is proposed at a central location on the site. The applicant has proposed both active and passive amenity spaces, the design of which will be further refined through the Site Plan application.

Infrastructure

Based on the comments received from the applicable City departments and external agencies, the existing infrastructure is adequate to support the proposed development.

Summary

The proposed development has been designed to be sensitive to the existing and planned character of the neighbourhood and provides an appropriate transition to adjacent uses. Overlook conditions and shadow impacts have been addressed through the increase in building setbacks to the property lines and the setback of roof top terraces to building edges. The applicant has also provided a Planning Justification Report in support of the applications that has adequately demonstrated that the proposal represents good planning and is consistent with the intent of MOP policies.

Other Development Matters

Proposed Access to Cawthra Road and Internal Road Connections

Cawthra Road is under Regional jurisdiction and therefore any access to the site is subject to approval by the Region of Peel. The Region is supportive of a full movement access onto Cawthra Road across from Village Green Boulevard, contingent upon the provision of internal road connections to the properties to the north and south.

City staff support the Region's position and are seeking public easements over the internal private road connection to allow for the future shared use of the road by adjacent properties. This requirement from the Region echoes the agreements that were entered into when Peartree Estates (existing townhouse condominium at 1130 Cawthra Road) was approved in 2003. Appendix 5 contains the Overall Concept Plan that was included in the Supplementary Report for the development at 1130 Cawthra Road.

The agreements between the developer of Peartree Estates and the Region of Peel and City of Mississauga speak to the fact that the configuration of the existing access to 1130 Cawthra Road is intended to be a temporary condition until "*such time as the lands to the north ... are redeveloped providing access to Cawthra Road through the future extension of Village Green Boulevard.*" Once the lands to the north are redeveloped, access to Cawthra Road would be obtained through the development site and the current access to 1130 Cawthra Road will be restricted to a right-in and right-out only. A critical component to the connection is the intervening property at 1168 Cawthra Road, which does not form part of the development site. The road connection between the development site and 1130 Cawthra Road cannot occur until the property at 1168 Cawthra Road is redeveloped. A condition of any redevelopment of 1168 Cawthra Road would be the requirement to complete the internal private road connection.

The applicant has modified their proposal to provide a proposed road connection through their lands to connect to the properties to the north and south, when they ultimately redevelop. To accommodate the road connection, the applicant has removed 2 units from Block 3 and will be required to implement special design features to the ramp to the underground garage through the Site Plan application process. Although the layout of the proposed road connection differs from the Overall Concept Plan in Appendix 5, City and Region of Peel staff are satisfied with the proposed connection from a functional perspective. The alignment also protects for

redevelopment opportunities at the rear of the property at 1168 Cawthra Road. In addition to providing the road connection, the applicant has also agreed to provide public easements over their proposed internal private road in favour of the City of Mississauga, which will be secured through the Site Plan process.

Zoning

The proposed **RM9-Exception** (Horizontal Multiple Dwelling – with more than 6 dwelling units) zone is appropriate to accommodate the proposed 148 horizontal multiple dwelling development. Appendix 6 contains a summary of the proposed site specific zoning provisions.

Bonus Zoning

Council adopted Corporate Policy and Procedure 07-03-01 – Bonus Zoning on September 26, 2012. In accordance with Section 37 of the *Planning Act* and policies contained in the Official Plan, this policy enables the City to secure community benefits when increases in permitted height and/or density are deemed to be good planning by Council through the approval of a development application.

Should these applications be approved by Council, staff will hold discussions with the applicant to secure community benefits and return to Council with a Section 37 report outlining the recommended benefits and corresponding contribution amount.

Site Plan

Prior to development of the lands, the applicant will be required to obtain Site Plan approval. No site plan application has been submitted to date for the proposed development.

While the applicant has worked with City departments to address many site plan related issues through review of the Rezoning concept plan, further revisions will be needed to address matters such as architectural elements, landscaping, detailed grading and site circulation.

Financial Impact

Development charges will be payable in keeping with the requirements of the Development Charges By-law of the City. Also, the financial requirements of any other commenting agency must be met.

Conclusion

In accordance with subsection 34(17) of the *Planning Act*, Council is given authority to determine if further public notice is required. Since the requested revisions to the applications are not considered major changes to the development, it is recommended that no further public notice be required.

The proposed Official Plan Amendment and Rezoning are acceptable from a planning standpoint and should be approved for the following reasons:

1. The proposed 148 horizontal multiple dwellings on a private condominium road is consistent with the overall intent, goals and objectives of Mississauga Official Plan as the site is located on the periphery of the Mineola Neighbourhood, on Cawthra Road and will not destabilize the surrounding residential neighbourhood.
2. The proposed built form is appropriate given the surrounding land uses and has been designed to minimize impacts from shadowing and overlook onto adjacent properties. Building heights and setbacks are consistent with existing and planned developments in the immediate area.
3. The proposed official plan provisions and zoning standards, as identified, are appropriate to accommodate the requested uses.

Prior to the passage of the implementing official plan amendment and zoning by-law by Council, the applicant will be required to execute a Section 37 agreement to the satisfaction of the City.

Attachments

Appendix 1: Information Report

Appendix 2: Revised Concept Plan

Appendix 3: Revised Elevations

Appendix 4: Context Plan

Appendix 5: Overall Concept Plan, May 22,2003, Information Report for OZ 02/036 W1

Appendix 6: Revised Summary of Existing and Proposed Zoning Provisions



Edward R. Sajecki, Commissioner of Planning and Building

Prepared by: Ashlee Rivet, Development Planner

City of Mississauga
Corporate Report



<p>Date: November 15, 2016</p> <p>To: Chair and Members of Planning and Development Committee</p>	<p>Originator's file: OZ 16/002 W1</p>
<p>From: Edward R. Sajecki, Commissioner of Planning and Building</p>	<p>Meeting date: 2016/12/05</p>

Subject

PUBLIC MEETING INFORMATION REPORT (WARD 1)

Applications to permit 154 horizontal multiple dwellings on a private condominium road

1174 - 1206 Cawthra Road

West side of Cawthra Road, south of Atwater Avenue

Owner: Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Recommendation

That the report dated November 15, 2016, from the Commissioner of Planning and Building regarding the applications by Queenscorp (Cawthra South) Inc. to permit 154 horizontal multiple dwellings on a private condominium road under File OZ 16/002 W1, 1174 - 1206 Cawthra Road, be received for information.

Report Highlights

- This report has been prepared for a public meeting to hear from the community
- The proposed development requires amendments to the official plan and zoning by-law
- Community concerns identified to date relate to height and density, traffic, school accommodations, visitor parking, and stormwater management
- Prior to the next report, matters to be considered include the appropriateness of the proposed amendments and the satisfactory resolution of other technical requirements and studies related to the project

Background

The applications have been circulated for technical comments and a community meeting has been held. The purpose of this report is to provide preliminary information on the applications and to seek comments from the community.

Comments

THE PROPERTY AND THE NEIGHBOURHOOD

Size and Use	
Frontage:	105.7 m (346.8 ft.) along Cawthra Road
Depth:	125.5 m (411.7 ft.)
Gross Lot Area:	1.3 ha (3.3 ac.)
Existing Uses:	1198 and 1206 Cawthra Road are occupied by detached dwellings and related accessory buildings. The detached dwellings that occupied the remaining 5 properties have been demolished and the lands are now vacant

The 7 properties that form this application are located in the Mineola Neighbourhood Character Area on the west side of Cawthra Road, south of Atwater Avenue and north of the CN railway. Raphael Avenue terminates in a dead-end to the west of the properties. The area is an established residential neighbourhood characterized by mostly detached and semi-detached dwellings, with the exception of a condominium townhouse development abutting the CN railway to the south. The lands on the east side of Cawthra Road are in the Lakeview Neighbourhood Character Area and are characterized by townhouses and apartment buildings. Site grades generally fall from the northwest corner down to the southeast corner of the property. The lands contain mature trees around the perimeter.

The surrounding land uses are:

- North: Three properties occupied by detached dwellings and vacant lands at the southwest corner of Atwater Avenue and Cawthra Road subject to active Site Plan application SP 15/102 W1 for 110 back-to-back stacked townhouses
- East: Village Green Boulevard, townhouses and vacant lands zoned for apartments, townhouses and horizontal multiple dwellings, on the east side of Cawthra Road
- South: One detached dwelling, a 52 unit condominium townhouse development and the CN railway
- West: Detached and semi-detached dwellings

Information regarding the history of the site is found in Appendix 1.

DETAILS OF THE PROJECT

The applicant is proposing 154 horizontal multiple dwellings in the form of back-to-back townhouses, stacked townhouses and back-to-back stacked townhouses, in 7 blocks located on a private condominium road (see Appendices 5 and 6). The proposed back-to-back townhouses (Blocks 1, 3, 4 and 6) are 3 storeys plus a roof-top terrace. The proposed stacked townhouses (Blocks 2 and 7) and back-to-back stacked townhouses (Block 5) have 3 storeys above a

partially below ground basement level (4 levels of living space) and a roof-top terrace. Site access is proposed from Cawthra Road, opposite Village Green Boulevard. A total of 271 parking spaces are proposed, 260 of which are located underground and the remaining 11 are surface parking spaces. There are also 121 bicycle parking spaces proposed in the underground garage.

Development Proposal	
Applications submitted:	Received: March 18, 2016 Deemed complete: April 15, 2016
Developer Owner:	Queenscorp (Cawthra South) Inc.
Applicant:	Ed Warankie, Queenscorp Group
Number of units:	154 horizontal multiple dwellings (58 back-to-back townhouses, 64 stacked townhouses and 32 back-to-back stacked townhouses)
Height:	3 storeys + partially above ground basement level + roof-top terrace
Lot Coverage:	44.6%
Floor Space Index:	1.44
Landscaped Area:	38.9%
Net Density:	115.8 units/ha 46.7 units/ac.
Gross Floor Area:	19 196.1 m ² (206,632 ft ²)
Road type:	Private condominium road
Anticipated Population:	478* *Average household sizes for all units (by type) for the year 2011 (city average) based on the 2013 Growth Forecasts for the City of Mississauga.
Parking:	Required Proposed
resident spaces	246 240
visitor spaces	39 31
Total	285 271
Green Initiatives:	Low reflective roof-top materials to create a "white roof"

Additional information is provided in Appendices 1 to 11.



August 2011 Source: Google Streetview

Image of
previous
conditions



July 2016 Source: Google Streetview

Image of
existing
conditions



Rendering of
proposed
development

LAND USE CONTROLS

The subject lands are located within the Mineola Neighbourhood Character Area and are designated **Residential Medium Density – Special Site 1**. These policies permit detached, semi-detached and townhouse dwellings within a net density range of 25-37 units per net residential hectare (10-15 units per acre) and encourages the assembly of parcels. The applications are not in conformity with the land use designation. The applicant is proposing to change the designation to **Residential Medium Density – Special Site** to permit horizontal multiple dwellings. The proposal has a density of 115.8 units/ha (46.7 units/ac.) and a floor space index (FSI) of 1.5.

A rezoning is proposed from **R3-1** (Detached Dwellings – Typical Lots) to **RM9 - Exception** (Horizontal Multiple Dwellings with more than 6 dwelling units) to permit 58 back-to-back townhouses, 64 stacked townhouses and 32 back-to-back stacked townhouses in accordance with the proposed zone standards contained within Appendix 10.

Detailed information regarding the Official Plan and Zoning is in Appendices 9 and 10.

Bonus Zoning

On September 26, 2012, Council adopted Corporate Policy and Procedure 07-03-01 – Bonus Zoning. In accordance with Section 37 of the *Planning Act* and policies contained in the Official Plan, this policy enables the City to secure community benefits when increases in permitted height and/or density are deemed to be good planning by Council through the approval of a development application. Should these applications be approved by Council the City will report back to Planning and Development Committee on the provision of community benefits as a condition of approval.

WHAT DID THE COMMUNITY SAY?

A community meeting was held by Ward 1 Councillor, Jim Tovey on September 27, 2016.

Comments made by the community are listed below. They will be addressed along with comments raised at the public meeting in the Recommendation Report, which will come at a later date.

- The proposed development is out of character with the surrounding neighbourhood. It is too dense and too tall
- Traffic is already an issue on Cawthra Road, especially during school drop off/pick up hours. Additional traffic generated by the proposed development and other developments in the immediate area will create unsafe conditions
- Insufficient visitor parking will result in on-street parking on the surrounding residential streets
- The proposed pedestrian connection from Raphael Avenue into the proposed development is a concern
- The neighbourhood schools are at capacity
- There have been flooding issues in the area

A petition letter with 27 signatures (23 households) from members of the Mineola East Community has been received by the Planning and Building Department, opposing the proposed development.

DEVELOPMENT ISSUES

Agency comments are summarized in Appendix 7 and school accommodation information is contained in Appendix 8. Based on the comments received and the applicable Mississauga Official Plan policies, the following matters will have to be addressed:

- Are the policies and principles of Mississauga Official Plan maintained by this project?
- Is the proposed height, density and massing appropriate and compatible with the existing and planned character of the area?

- Are horizontal multiple dwellings a compatible built form with the surrounding land uses?
- Is the proposed site access and internal road configuration appropriate?
- Is the proposed grading and the resulting retaining walls and stairs appropriate?
- Are the proposed partially below ground units acceptable?
- Are the proposed zoning regulations acceptable?
- Is the design, location and orientation of the proposed amenity areas appropriate?
- Have all other technical requirements and studies, including stormwater management and traffic impacts, been addressed and found to be acceptable?

A report titled "Horizontal Multiple Dwellings – Urban Design Guidelines (All Wards)" was considered by Planning and Development Committee on September 19, 2016. The report outlined a series of concerns with recent Horizontal Multiple Dwelling development applications. The proposed development demonstrates some of these concerns, which will be addressed through the processing of the application and in the Recommendation Report at a later date.

OTHER INFORMATION

The applicant has submitted the following information in support of the applications:

- Planning Justification Report
- Draft Official Plan Amendment
- Draft Zoning By-law Amendment
- Concept Plan
- Preliminary Elevations/Floor Plans
- Site Sections
- Arborist Report and Tree Preservation Plan
- Landscape Plan
- Functional Servicing Report/Preliminary Stormwater Management Report
- Grading/Servicing Plans
- Traffic Impact and Parking Utilization Study
- Signal Warrant Analysis
- Stage 1 and 2 Archaeological Assessments
- Phase 1 Environmental Site Assessment
- Sun/Shadow Study
- Noise Feasibility Study
- Parcel Register

Development Requirements

There are engineering matters including: grading, servicing, stormwater management and noise mitigation measures which will require the applicant to enter into agreements with the City.

Prior to any development proceeding on-site, the City will require the submission and review of an application for site plan approval.

Financial Impact

Development charges will be payable as required by the Development Charges By-law of the City. Also the financial requirements of any other external commenting agency must be met.

Conclusion

Most agency and City department comments have been received. The Planning and Building Department will make a recommendation on this project after the public meeting has been held and the issues have been resolved.

Attachments

- Appendix 1: Site History
- Appendix 2: Aerial Photograph
- Appendix 3: Excerpt from Mineola Neighbourhood Character Area Land Use Map
- Appendix 4: Existing Land Use and Proposed Zoning Map
- Appendix 5: Concept Plan
- Appendix 6: Elevations
- Appendix 7: Agency Comments
- Appendix 8: School Accommodation
- Appendix 9: Summary of Existing and Proposed Mississauga Official Plan Policies and Relevant Mississauga Official Plan Policies
- Appendix 10: Summary of Existing and Proposed Zoning Provisions
- Appendix 11: General Context Map



Edward R. Sajecki, Commissioner of Planning and Building

Prepared by: Ashlee Rivet, Development Planner

Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Site History

- October 21, 2003 – Future Mutual Access Agreement and Acknowledgement Agreement made between Claredale Norstar Inc. and the City of Mississauga through rezoning application OZ 02/036 W1 for the lands at 1130 Cawthra Road (south of subject property). The agreement has the effect of allowing a temporary access driveway to Cawthra Road until such time as the lands to the north receive Site Plan approval and an Access Easement is granted over the lands to the north for the purposes of vehicular and pedestrian access to Cawthra Road at Village Green Boulevard.
- June 20, 2007 – Zoning By-law 0225-2007 came into force. The zoning of the lands changed from **R4-1824** (Detached Dwellings) to **R3-1** (Detached Dwellings).

44 23

LEGEND:



SUBJECT LANDS



DATE OF AERIAL IMAGERY: SPRING 2016

TITLE:

QUEENSCORP (CAWTHRA SOUTH) INC.

FILE NO:

OZ 16/ 002 W1

Produced by
T&W, Geomatics

MISSISSAUGA



**PART OF SCHEDULE 10 LAND USE DESIGNATIONS
OF MISSISSAUGA OFFICIAL PLAN**

LAND USE DESIGNATIONS

- Residential Low Density I
- Residential Low Density II
- Residential Medium Density
- Residential High Density
- Mixed Use
- Convenience Commercial
- Motor Vehicle Commercial
- Office
- Business Employment
- Industrial
- Airport
- Institutional
- Public Open Space
- Private Open Space
- Greenlands
- Parkway Belt West
- Utility
- Special Waterfront
- Partial Approval Area

BASE MAP INFORMATION

- Heritage Conservation District
- 1996 NEP/2000 NEF
- Composite Noise Contours
- LBPIA Operating Area Boundary
- Area Exempt from LBPIA Operating Area
- Natural Hazards
- Civic Centre (City Hall)
- City Centre Transit Terminal
- GO Rail Transit Station
- Public School
- Catholic School
- Hospital
- Community Facilities

- City Structure
- Downtown
- Major Node
- Community Node
- Neighbourhood
- Corporate Centre
- Employment Area
- Special Purpose Area

SUBJECT LANDS



TITLE:
QUEENSCORP (CAWTHRA SOUTH) INC.

FILE NO:
OZ 16/ 002 W1

MISSISSAUGA

Produced by
T&W, Geomatics



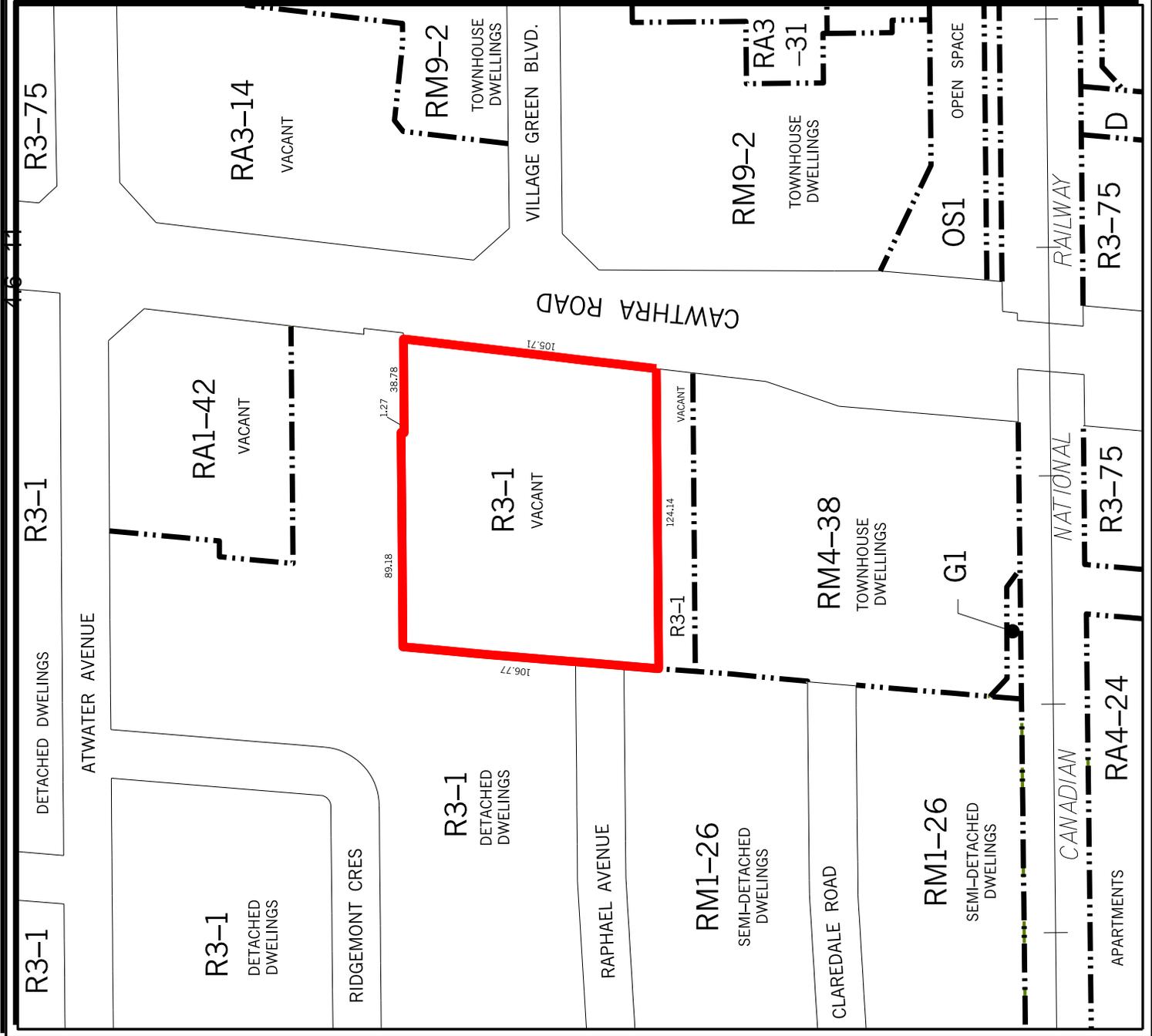
LEGEND: PROPOSED OFFICIAL PLAN AMENDMENT FROM "RESIDENTIAL MEDIUM DENSITY-SPECIAL SITE 1" TO "RESIDENTIAL MEDIUM DENSITY-SPECIAL SITE 1" AND PROPOSED REZONING FROM "R3-1" (DETACHED DWELLINGS) TO "RM9-EXCEPTION" (HORIZONTAL MULTIPLE DWELLINGS WITH MORE THAN 6 DWELLING UNITS) TO PERMIT 154 HORIZONTAL MULTIPLE DWELLINGS (58 BACK-TO-BACK TOWNHOUSES, 64 STACKED TOWNHOUSES, 32 BACK-TO-BACK STACKED TOWNHOUSES) ON A PRIVATE CONDOMINIUM ROAD.



TITLE: QUEENSCORP (CAWTHRA SOUTH) INC.

FILE NO.: OZ 16/ 002 W1

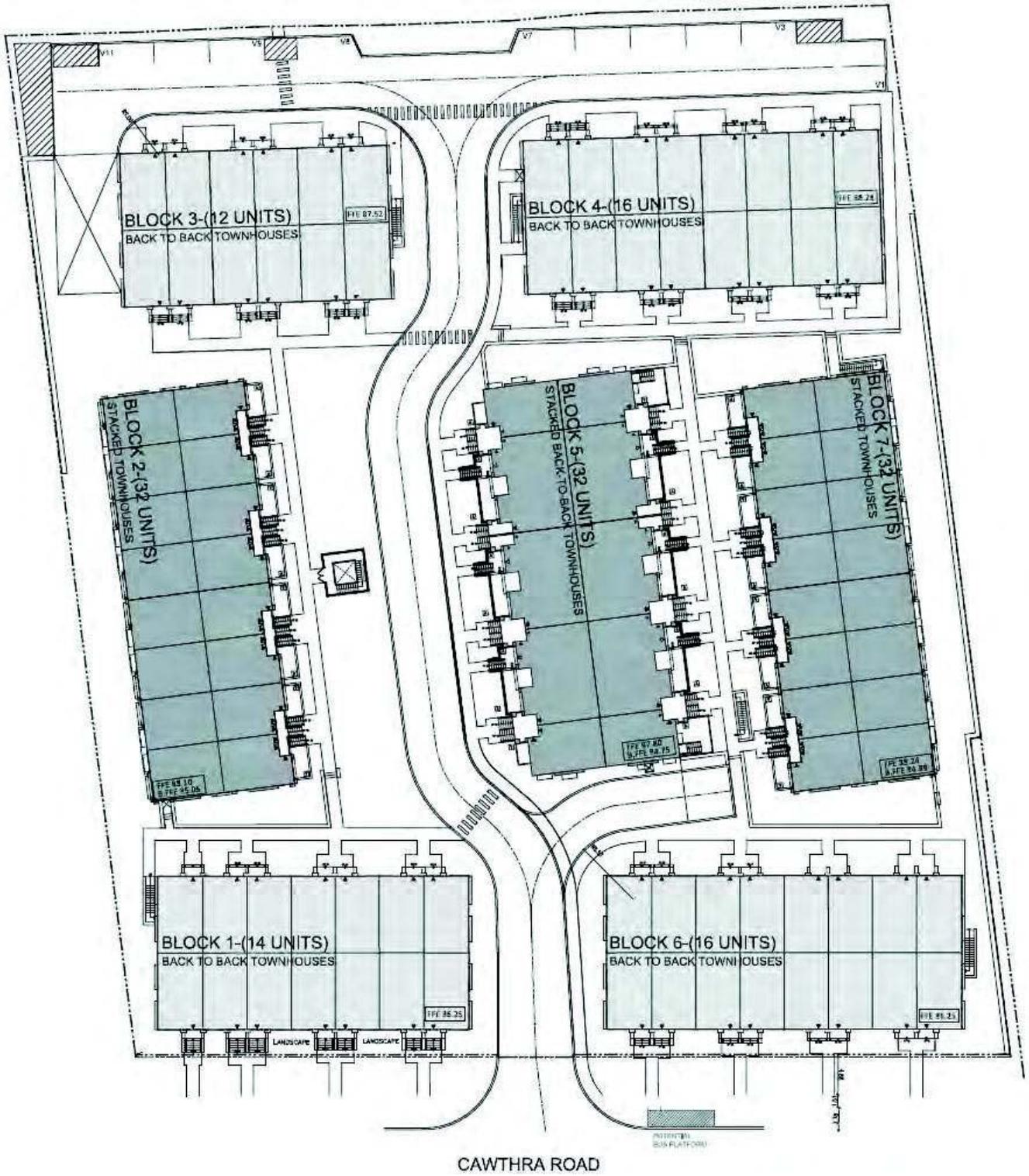
MISSISSAUGA Produced by T&W, Geomatics



Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Concept Plan



Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Elevations



Typical Back-to-Back Townhouses (Blocks 1, 3, 4 and 6)



Typical Stacked Townhouses (Block 2 and 7)



Typical Back-to-Back Stacked Townhouses (Block 5)

Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Agency Comments

The following is a summary of comments from agencies and departments regarding the applications.

Agency / Comment Date	Comment
Region of Peel (October 20, 2016)	<p>A Functional Servicing Report & Stormwater Management Report satisfactory to the Region to determine adequacy of the existing services for the proposed development is required prior to final approval. An updated report has been received with the second submission and is under review. With regards to land dedications required for Cawthra Road, Regional staff are currently working with the applicant to determine the extent of land dedications required. Additionally, the Region requires the establishment of an internal connection/easement to the properties north and south of the subject lands to facilitate a full movement access onto Cawthra Road at Village Green Boulevard. With the establishment of this internal connection to the properties to the north and south, the proposed Waste Collection staging area will be required to be relocated while still adhering to the Region of Peel Waste Design Standard Manual.</p>
Dufferin-Peel Catholic District School Board and the Peel District School Board (October 6 and 7, 2016)	<p>Both School Boards responded that they are satisfied with the current provision of educational facilities for the catchment area and, as such, the school accommodation condition as required by the City of Mississauga Council Resolution 152-98 pertaining to satisfactory arrangements regarding the adequate provision and distribution of educational facilities need not be applied for these development applications.</p> <p>If approved, both School Boards require that certain warning clauses regarding transportation, signage and temporary accommodation be included in any Development/Servicing Agreement and Agreements of Purchase and Sale.</p>
City Community Services Department – Parks and Forestry Division/Park Planning Section (October 14, 2016)	<p>The subject site is located within 220 m (721.8 ft.) of Village Green Park which is zoned OS1 (Open Space – Community Park). The park contains a play site.</p> <p>Street tree contributions will be required along Cawthra Road. Prior to the issuance of building permits for each lot or block cash-in-lieu for park or other public recreational purposes is required pursuant to Section 42 of the <i>Planning Act</i> and in accordance with City's Policies and By-laws.</p>

Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Agency / Comment Date	Comment
City Community Services Department – Heritage Planning (October 18, 2016)	Stage 1 and Stage 2 Archaeological Assessment reports have been submitted and are under review. The corresponding Ministry of Tourism, Culture and Sport letters are pending.
City Transportation and Works Department (T&W) (October 26, 2016)	<p>In comments dated October 24, 2016 this department confirmed receipt of a Functional Servicing Report, Site Grading/Servicing Plans, Noise Feasibility Study, Site Plan, Transportation Impact Study and Phase 1 Environmental Site Assessment circulated by the Planning and Building Department.</p> <p>Notwithstanding the findings of these reports and drawings, the applicant has been requested to provide additional technical details. Development matters currently under review and consideration by the department include:</p> <ul style="list-style-type: none"> • Grading and Servicing, • Stormwater Management, • Transportation Impacts, • Noise Feasibility Study update, • Overall concept / block plan (including interconnections to the adjacent lands to the north and south). <p>The above aspects will be addressed in detail prior to the Recommendation Report.</p>
Rogers Cable (May 2, 2016)	Rogers Communications Canada Inc. has aerial and buried fibre and coaxial plant in the working area and locates will be required to mark-out actual locations. Extreme caution is advised, maintain minimum of 0.6 m (1.96 ft.) clearance.
GO Transit (May 27, 2016)	<p>The subject site is not immediately adjacent to but within 300 m (984.3 ft.) of the GO Transit Lakeshore West Corridor. The Noise and Vibration Study submitted does not reflect future rail traffic levels anticipated on the Lakeshore West Corridor. The analysis is to be updated accordingly.</p> <p>The applicant shall grant Metrolinx an environmental easement for operational emissions, registered on title against the subject residential dwellings in favour of Metrolinx.</p> <p>If approved, GO Transit will require that certain warning clauses regarding noise and vibration from the GO Transit Lakeshore West Corridor shall be included in any Development/Servicing Agreement and Agreements of Purchase and Sale.</p>

Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Agency / Comment Date	Comment
Other City Departments and External Agencies	<p>The following City Departments and external agencies offered no objection to these applications provided that all technical matters are addressed in a satisfactory manner:</p> <ul style="list-style-type: none"> • Fire and Emergency Services • Culture Planning, Community Services Department • Mississauga Transit • Enersource Hydro Mississauga • Canada Post • Enbridge Gas Distribution Inc. • Greater Toronto Airports Authority <p>The following City Departments and external agencies were circulated the applications but provided no comments:</p> <ul style="list-style-type: none"> • Urban Forestry, Community Services Department • Realty Services, Corporate Services Department • Bell Canada • Conseil Scolaire de Distrique Centre-Sud • Conseil Scolaire Viamonde

Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

School Accommodation

The Peel District School Board	The Dufferin-Peel Catholic District School Board
<ul style="list-style-type: none"> • Student Yield: <ul style="list-style-type: none"> 15 Kindergarten to Grade 8 10 Grade 9 to Grade 12 • School Accommodation: <ul style="list-style-type: none"> Janet I. McDougald Public School <ul style="list-style-type: none"> Enrolment: 462 Capacity: 552 Portables: 0 Allan A. Martin Public School <ul style="list-style-type: none"> Enrolment: 466 Capacity: 538 Portables: 0 Cawthra Park Secondary School <ul style="list-style-type: none"> Enrolment: 1,310 Capacity: 1,044 Portables: 5 <p>*Note: Capacity reflects the Ministry of Education rated capacity, not the Board rated capacity, resulting in the requirement of portables.</p>	<ul style="list-style-type: none"> • Student Yield: <ul style="list-style-type: none"> 11 Junior Kindergarten to Grade 8 9 Grade 9 to Grade 12 • School Accommodation: <ul style="list-style-type: none"> St. Dominic Separate School <ul style="list-style-type: none"> Enrolment: 295 Capacity: 271 Portables: 3 St. Paul Secondary School <ul style="list-style-type: none"> Enrolment: 419 Capacity: 807 Portables: 0

Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Summary of Existing and Proposed Mississauga Official Plan Policies and Relevant Mississauga Official Plan Policies

Current Mississauga Official Plan Designation and Policies for the Mineola Neighbourhood Character Area

Residential Medium Density – Special Site 1 which permits detached, semi-detached and townhouse dwellings within a net density range of 25-37 units per net residential hectare (10-15 units per net residential acre). The policies also encourage the assembly of separate parcels.

Cawthra Road is designated as a **Corridor**.

Proposed Official Plan Amendment Provisions

The lands are proposed to be designated **Residential Medium Density – Special Site** to permit horizontal multiple dwellings.

Relevant Mississauga Official Plan Policies

There are numerous policies that apply in reviewing these applications. An overview of some of these policies is found below:

	Specific Policies	General Intent
Chapter 5 – Direct Growth	Section 5.3.5 – Neighbourhoods	<p>5.3.5.1 Neighbourhoods will not be the focus for intensification and should be regarded as stable residential areas where the existing character is to be preserved.</p> <p>5.3.5.2 Residential intensification within Neighbourhoods will generally occur through infilling and the development of existing commercial sites as mixed use areas.</p> <p>5.3.5.3 Where higher density uses are proposed, they should be located on sites identified by a local area review, along Corridors or in conjunction with existing apartment sites or commercial centres.</p> <p>5.3.5.5 Intensification within Neighbourhoods may be considered where the proposed development is compatible in built form and scale to the surrounding development, enhances the existing or planned development and is consistent with the policies of this Plan.</p> <p>5.3.5.6 Development will be sensitive to the existing and planned context and will include appropriate transitions in use, built form, density and scale.</p>

Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

	Specific Policies	General Intent
Chapter 5 – Direct Growth	Section 5.4 – Corridors	<p>5.4.4 Development on Corridors should be compact, mixed use and transit friendly and appropriate to the context of the surrounding Neighbourhood.</p> <p>5.4.5 Where higher density uses within Neighbourhoods are directed to Corridors, development will be required to have regard for the character of the Neighbourhoods and provide appropriate transitions in height, built form and density to the surrounding lands.</p> <p>5.4.7 Land uses and building entrances will be oriented to the Corridor where possible and surrounding land use development patterns permit.</p>
Chapter 7 – Complete Communities	Section 7.2 – Housing	<p>7.2.1 Mississauga will ensure that housing is provided in a manner that maximizes the use of community infrastructure and engineering services, while meeting the housing needs and preferences of Mississauga residents.</p> <p>7.2.2 Mississauga will provide opportunities for:</p> <ol style="list-style-type: none"> a. The development of a range of housing choices in terms of type, tenure and price; b. The production of a variety of affordable dwelling types for both the ownership and rental markets; and, c. The production of housing for those with special needs, such as housing for the elderly and shelters. <p>7.2.8 Design solutions that support housing affordability while maintaining appropriate functional and aesthetic quality will be encouraged.</p>
Chapter 8 – Create a Multi-Modal City	Section 8.2.2 – Road Network	<p>8.2.2.3 Mississauga will strive to create a fine-grained system of roads that seeks to increase the number of road intersections and overall connectivity throughout the city.</p> <p>8.2.2.5 Additional roads may be identified during the review of development applications and through the local area review process. The City may require the completion of road connections and where appropriate, the creation of a denser road pattern through the construction of new roads.</p> <p>8.2.2.7 Future additions to the road network should be public roads. Public easements may be required where private roads are permitted.</p>

	Specific Policies	General Intent
Chapter 9 – Building a Desirable Urban Form		<p>9.5.1.2 Developments should be compatible and provide appropriate transition to existing and planned development by having regard for the following elements:</p> <ol style="list-style-type: none"> a. Natural Heritage System; b. Natural hazards; c. Natural and cultural heritage features; d. Street and block patterns; e. The size and configuration of properties along a street, including lot frontages and areas; f. Continuity and enhancement of streetscapes; g. The size and distribution of building mass and height; h. Front, side and rear yards; i. The orientation of buildings, structures and landscapes on a property; j. Views, sunlight and wind conditions; k. The local vernacular and architectural character as represented by the rhythm, textures and building materials; l. Privacy and overlook; and, m. The function and use of buildings, structures and landscapes. <p>9.5.1.9 Development proposals will demonstrate compatibility and integration with surrounding land uses and the public realm by ensuring that adequate privacy, sunlight and sky views are maintained and that microclimatic conditions are mitigated.</p> <p>9.5.2.7 Site development should respect and maintain existing grades on-site.</p>
Chapter 11 – General Land Use Designations	Section 11.2.5 - Residential	<p>11.2.5.5 Lands designated Residential Medium Density will permit the following uses:</p> <ol style="list-style-type: none"> a. Townhouse dwelling; and, b. All forms of horizontal multiple dwellings.
Chapter 16 - Neighbourhoods	Section 16.1.1 – General	<p>16.1.1.1 For lands within a Neighbourhood, a maximum building height of four storeys will apply unless Character Area policies specify alternative building height requirements or until such time as alternative building heights are determined through the review of Character Area policies.</p>

	Specific Policies	General Intent
Chapter 16 - Neighbourhoods	Section 16.18 - Mineola	<p>16.18.2.2 Notwithstanding the Residential Medium Density policies of this Plan, the Residential Medium Density designation permits only townhouse dwellings.</p> <p>16.18.5.1 Special Site 1 -</p> <ol style="list-style-type: none"> 1. The lands identified as Special Site 1 are located on the west side of Cawthra Road, south of Atwater Avenue, and north of the CNR line. 2. Notwithstanding the provisions of the Residential Medium Density designation, detached, semi-detached and townhouse dwellings will be permitted within a net density range of 25-37 units per net residential hectare. Assembly of separate parcels will be encouraged.
Chapter 19 - Implementation	Section 19.5 – Criteria for Site Specific Official Plan Amendment	<p>19.5.1 City Council will consider applications for site specific amendments to this Plan within the context of the policies and criteria set out throughout this Plan. The proponent of an official plan amendment will be required to submit satisfactory reports to demonstrate the rationale for the amendment; including, among other matters:</p> <ol style="list-style-type: none"> a. That the proposed redesignation would not adversely impact or destabilize the following: <ul style="list-style-type: none"> • The achievement of the overall intent, goals, objectives and policies of this Plan; and, • The development or functioning of the remaining lands that have the same designation, or neighbouring lands; and, b. That a municipal comprehensive review of land use designation or a five year review is not required; c. That the lands are suitable for the proposed use, and a planning rationale with reference to the policies of this Plan, other applicable policies and sound planning principles is provided, setting out the merits of the proposed amendment in comparison with the existing designation; d. Land use compatibility with the existing and future uses of surrounding lands; and, e. The adequacy of engineering services, community infrastructure and multi-modal transportation systems to support the proposed application.

Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Summary of Existing Zoning By-law Provisions

R3-1 (Detached Dwellings – Typical Lots), which permits detached dwellings with a minimum lot area of 550 m² (5,920 ft²) and lot frontage of 15 m (49.2 ft.) for an interior lot and infill exception regulations.

Summary of Proposed Zoning By-law Provisions

Zone Standards	Required RM9 Zoning By-law Standards	Proposed RM9- Exception Zoning By-law Standards (based on Site Plan dated September 2, 2016)
Use	Horizontal Multiple Dwelling – with more than 6 dwelling units	Horizontal Multiple Dwelling – with more than 6 dwelling units
Minimum lot frontage	30.0 m (98.4 ft.)	105.7 m (346.8 ft.)
Minimum floor space index	0.4	0.4
Maximum floor space index	0.9	1.5
Maximum height – flat roof	13.0 m (42.7 ft.)	14.8 m (48.6 ft.)
Minimum front yard setback	7.5 m (24.6 ft.)	3.0 m (9.8 ft.)
Minimum interior side yard where a horizontal multiple dwelling has a height greater than 10.0 m (32.8 ft.) and any portion of the interior lot line abuts a zone permitting a detached and/or semi-detached dwelling	10.0 m (32.8 ft.)	3.0 m (9.8 ft.) to building 2.7 m (8.9 ft.) to architectural feature
Minimum rear yard where a horizontal multiple dwelling has a height greater than 10.0 m (32.8 ft.) and less than 15.0 m (49.2 ft.) and any portion of the rear lot line abuts a zone permitting a detached and/or semi-detached dwelling	10.0 m (32.8 ft.)	15.1 m (49.5 ft.)

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Zone Standards	Required RM9 Zoning By-law Standards	Proposed RM9- Exception Zoning By-law Standards (based on Site Plan dated September 2, 2016)
Maximum encroachment of a porch, inclusive of stairs, located at and accessible from the first storey or below the first storey of the horizontal multiple dwelling	1.8 m (5.9 ft.)	2.0 m (6.6 ft.)
Minimum setback from a horizontal multiple dwelling to an internal road	4.5 m (14.8 ft.)	2.6 m (7.5 ft.)
Minimum setback from a rear wall of a horizontal multiple dwelling to a side wall of another dwelling	10.0 m (32.8 ft.)	9.0 m (29.5 ft.)
Minimum setback from a rear wall of a horizontal multiple dwelling to a rear wall of another dwelling	15.0 m (49.2 ft.)	13.1 m (43 ft.)
Minimum setback from a side wall of a horizontal multiple dwelling to an internal walkway	1.5 m (4.9 ft.)	0.3 m (1 ft.)
Minimum setback between a parking space and any other lot line	3.0 m (9.8 ft.)	0.8 m (2.6 ft.)
Minimum setback of a parking structure constructed completely below finished grade to any lot line	3.0 m (9.8 ft.)	2.4 m (7.9 ft.) to stairs
Minimum landscaped area	40% of lot area	38.9% of lot area
Minimum amenity area	The greater of 5.6 m ² (60.27 ft ²) per dwelling unit (862 m ² /9,278 ft ²) or 10% of the site (1 334 m ² /14,359 ft ²)	1 148 m ² (12,357 ft ²)
Minimum percentage of total required amenity area to be provided in one contiguous area	50%	51%

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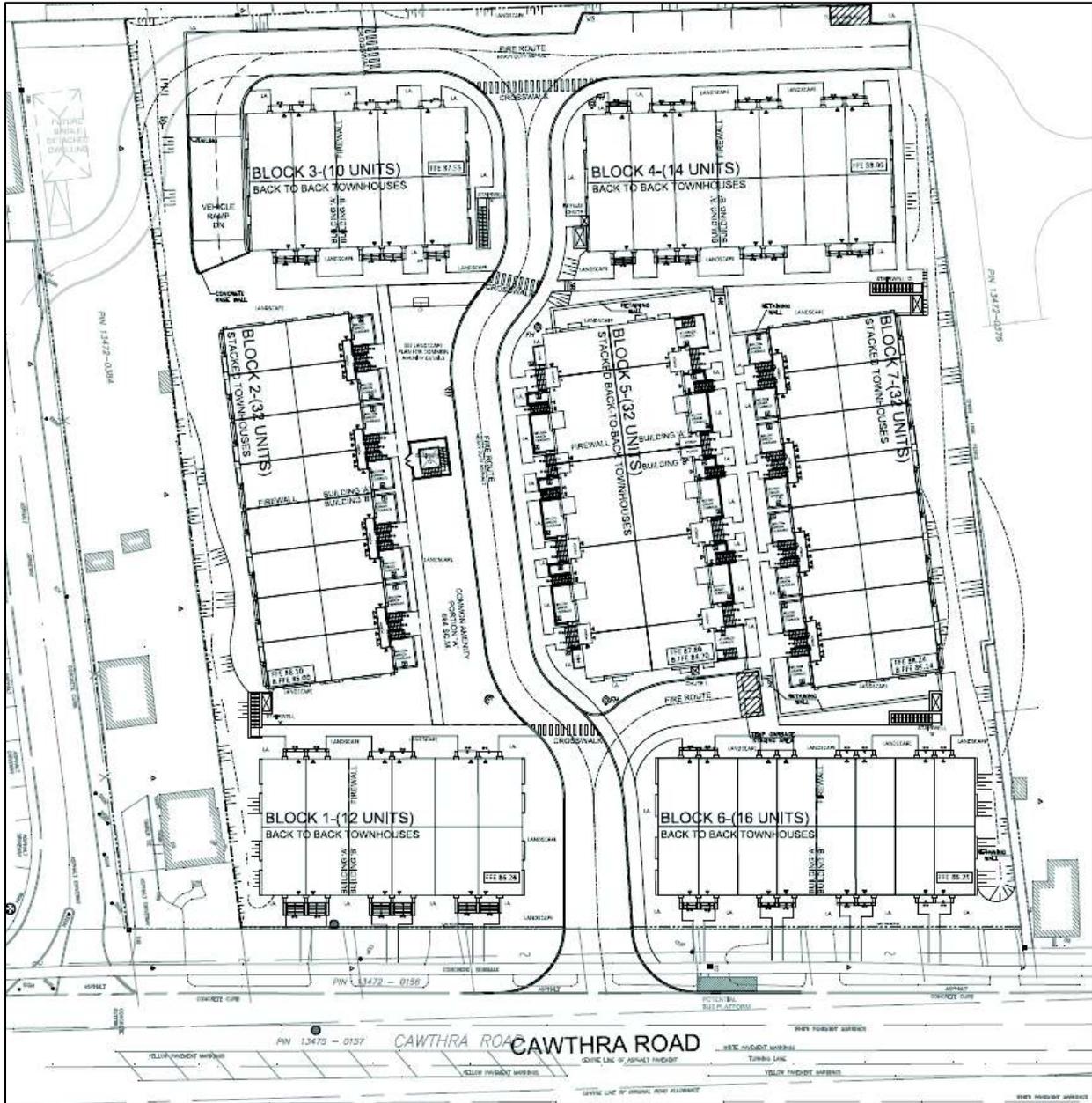
Zone Standards	Required RM9 Zoning By-law Standards	Proposed RM9- Exception Zoning By-law Standards (based on Site Plan dated September 2, 2016)
Minimum number of parking spaces	1.5 per two-bedroom unit 1.75 per three-bedroom unit 0.25 visitor spaces per unit	1.3 per two-bedroom unit 1.4 per three-bedroom unit 0.20 visitor spaces per unit

*The provisions listed are based on the preliminary concept plan and are subject to minor revisions as the plan is further refined

Queenscorp (Cawthra South) Inc.

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Revised Concept Plan



Queenscorp (Cawthra South) Inc.

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Revised Elevations

TYPICAL BACK TO BACK TOWNHOUSES (Blocks 1, 3, 4 and 6)



Front Elevation



Side Elevation

Queenscorp (Cawthra South) Inc.

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Revised Elevations

TYPICAL BACK TO BACK STACKED TOWNHOUSES (Block 5)



Front Elevation



Side Elevation

Queenscorp (Cawthra South) Inc.

File: OZ 16/002 W1

Revised Elevations

TYPICAL STACKED TOWNHOUSES (Blocks 2 and 7)



Front Elevation

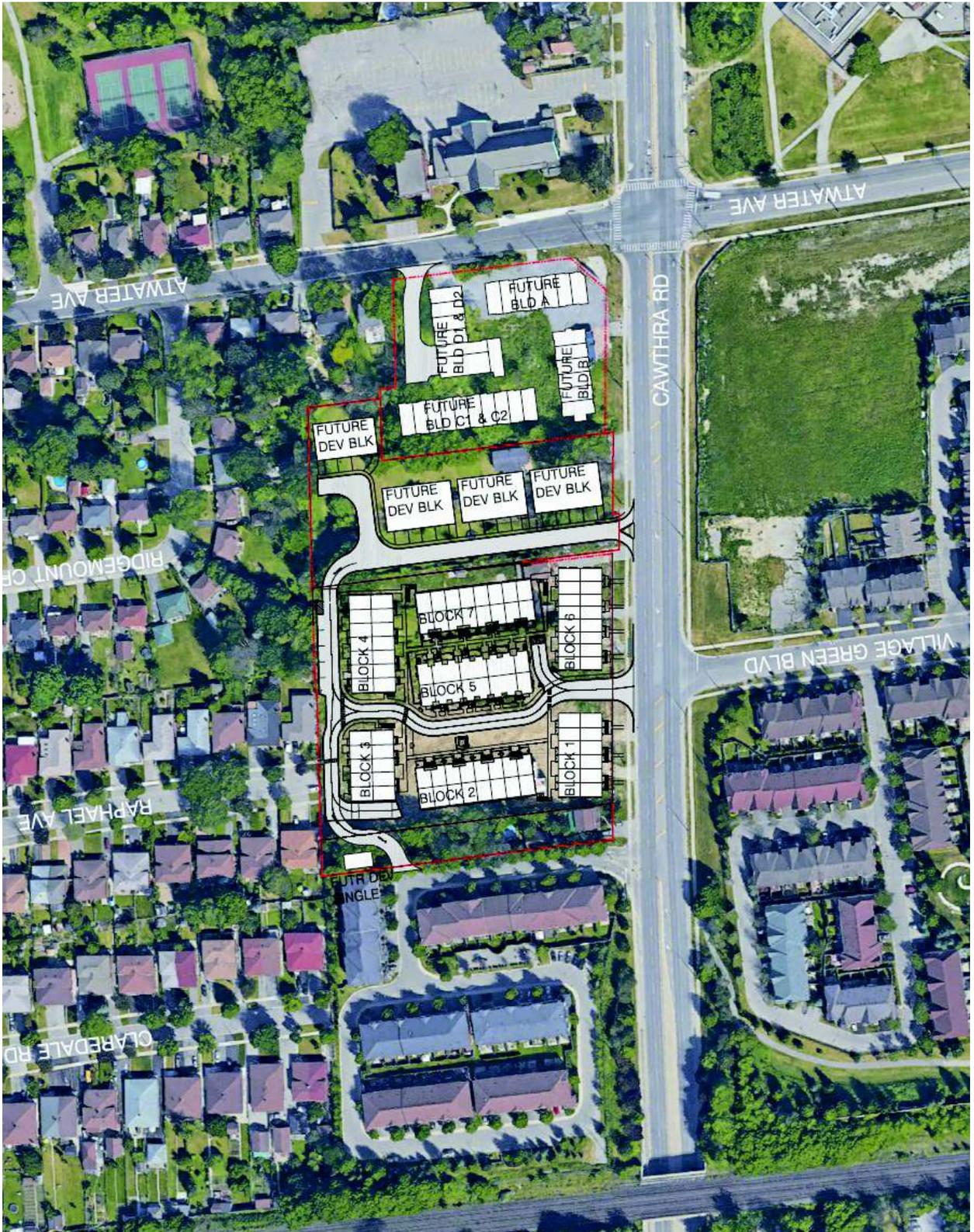


Rear Elevation



Side Elevation

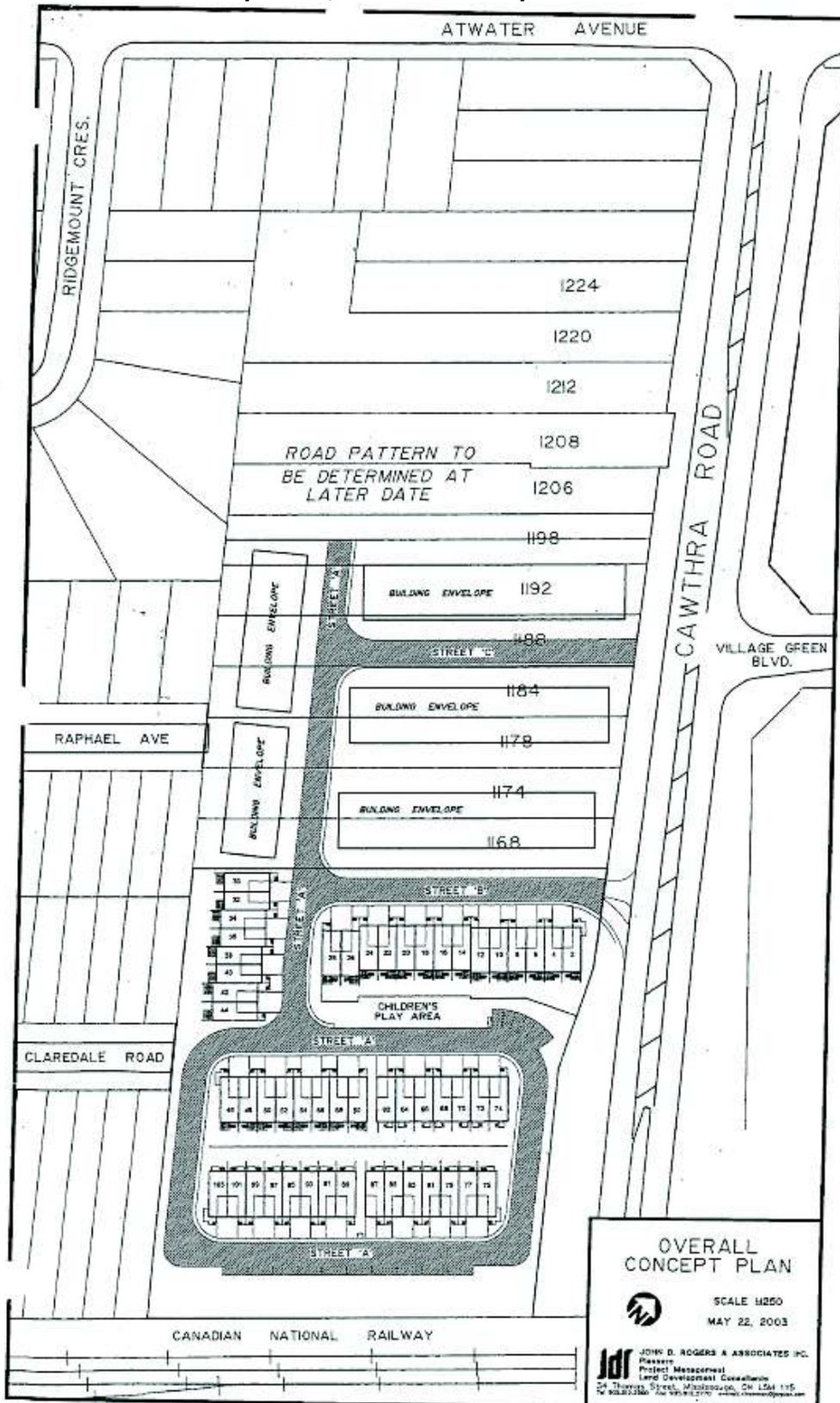
Context Plan



Queenscorp (Cawthra South) Inc.

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Overall Concept Plan, Information Report for OZ 02/036 W1



Summary of Existing Zoning By-law Provisions

"R3-1" (Detached Dwellings – Typical Lots), which permits detached dwellings with a minimum lot area of 550 m² (5,920 ft²) and lot frontage of 15 m (49.2 ft.) for an interior lot.

Summary of Proposed Zoning By-law Provisions

Zone Standards	Required "RM9" Zoning By-law Standards	Proposed "RM9-Exception" Zoning By-law Standards (based on Concept Plan dated September 2, 2016)	Proposed "RM9-Exception" Zoning By-law Standards (based on Revised Concept Plan dated March 24, 2017)
Use	Horizontal Multiple Dwelling – with more than 6 dwelling units	Horizontal Multiple Dwelling – with more than 6 dwelling units	Horizontal Multiple Dwelling – with more than 6 dwelling units
Minimum lot frontage	30.0 m (98.4 ft.)	105.7 m (346.8 ft.)	105.7 m (346.8 ft.)
Maximum floor space index	0.9	1.5	1.48
Maximum height – flat roof	13.0 m (42.7 ft.)	14.8 m (48.6 ft.)	14.7 m (48.2 ft.) including rooftop terrace
Minimum front yard setback	7.5 m (24.6 ft.)	3.0 m (9.8 ft.)	12.7 m (41.4 ft.) excluding rooftop terrace
Minimum interior side yard where a horizontal multiple dwelling has a height greater than 10.0 m (32.8 ft.) and any portion of the interior lot line abuts a zone permitting a detached and/or semi-detached dwelling	10.0 m (32.8 ft.)	Where a side wall abuts the interior side lot line: 3.1 m (10.2 ft.) Where a rear wall abuts the interior side lot line: 6.1 m (20.0 ft.)	Where a side wall abuts the interior side lot line: 3.0 m (9.8 ft.) Where a rear wall abuts the interior side lot line: 6.5 m (21.3 ft.)

Zone Standards	Required "RM9" Zoning By-law Standards	Proposed "RM9-Exception" Zoning By-law Standards (based on Concept Plan dated September 2, 2016)	Proposed "RM9-Exception" Zoning By-law Standards (based on Revised Concept Plan dated March 24, 2017)
Minimum rear yard where a horizontal multiple dwelling has a height greater than 10.0 m (32.8 ft.) and less than 15.0 m (49.2 ft.) and any portion of the rear lot line abuts a zone permitting a detached and/or semi-detached dwelling	10.0 m (32.8 ft.)	15.1 m (49.5 ft.)	13.4 m (44.0 ft.)
Maximum encroachment of a porch, inclusive of stairs, located at and accessible from the first storey or below the first storey of the horizontal multiple dwelling	1.8 m (5.9 ft.)	2.0 m (6.6 ft.)	2.8 m (9.2 ft.)
Minimum setback from a horizontal multiple dwelling to an internal road, sidewalk or visitor parking space	4.5 m (14.8 ft.)	2.3 m (7.5 ft.)	1.4 m (4.6 ft.)
Minimum setback from a porch or deck, inclusive of stairs to an internal road or sidewalk	2.9 m (9.5 ft.)	1.0 m (3.3 ft.)	1.2 m (3.9 ft.)
Minimum setback from a side wall of a horizontal multiple dwelling to an internal walkway	1.5 m (4.9 ft.)	0.3 m (1 ft.)	1.4 m (4.6 ft.)

Zone Standards	Required "RM9" Zoning By-law Standards	Proposed "RM9-Exception" Zoning By-law Standards (based on Concept Plan dated September 2, 2016)	Proposed "RM9-Exception" Zoning By-law Standards (based on Revised Concept Plan dated March 24, 2017)
Minimum setback from a side wall of a horizontal multiple dwelling to an internal road	4.5 m (14.7 ft.)	2.7 m (8.8 ft.)	3.2 m (10.5 ft.)
Minimum setback between a parking space and any other lot line	3.0 m (9.8 ft.)	0.8 m (2.6 ft.)	1.0 m (3.3 ft.)
Minimum setback of a parking structure constructed completely below finished grade to any lot line	3.0 m (9.8 ft.)	2.4 m (7.9 ft.) to stairs	3.0 m (9.8 ft.)
Minimum width of a sidewalk	2.0 m (6.6 ft.)	1.5 m (4.9 ft.)	1.5 m (4.9 ft.)
Minimum landscaped area	40% of lot area	38.9% of lot area	45% of lot area
Minimum amenity area	The greater of 5.6 m ² (60.2 ft ²) per dwelling unit (862 m ² (9,278.5 ft ²)) or 10% of the site (1 334 m ² (14,359 ft ²))	1 148 m ² (12,357 ft ²) (note: includes areas that are deemed to not meet the City's amenity area requirements)	735.3 m ² (7,914.7 ft ²)
Minimum percentage of total required amenity area to be provided in one contiguous area	50%	51%	74%
Minimum number of parking spaces	1.5 per two-bedroom unit 1.75 per three-bedroom unit 0.25 visitor spaces per unit	1.3 per two-bedroom unit 1.4 per three-bedroom unit 0.20 visitor spaces per unit	1.3 per two-bedroom unit 1.4 per three-bedroom unit 0.20 visitor spaces per unit

*The provisions listed are based on the preliminary concept plan and are subject to minor revisions as the plan is further refined