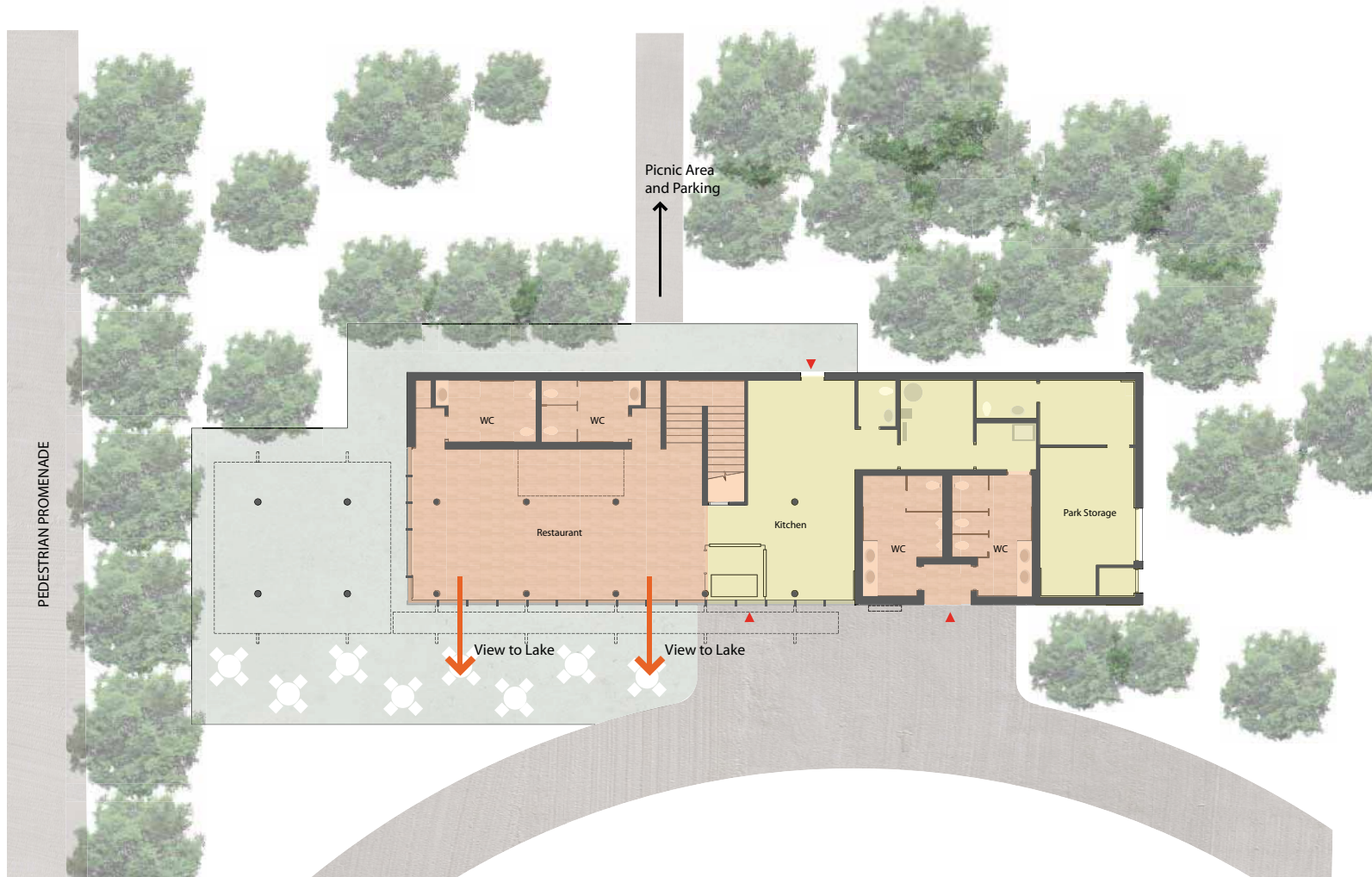


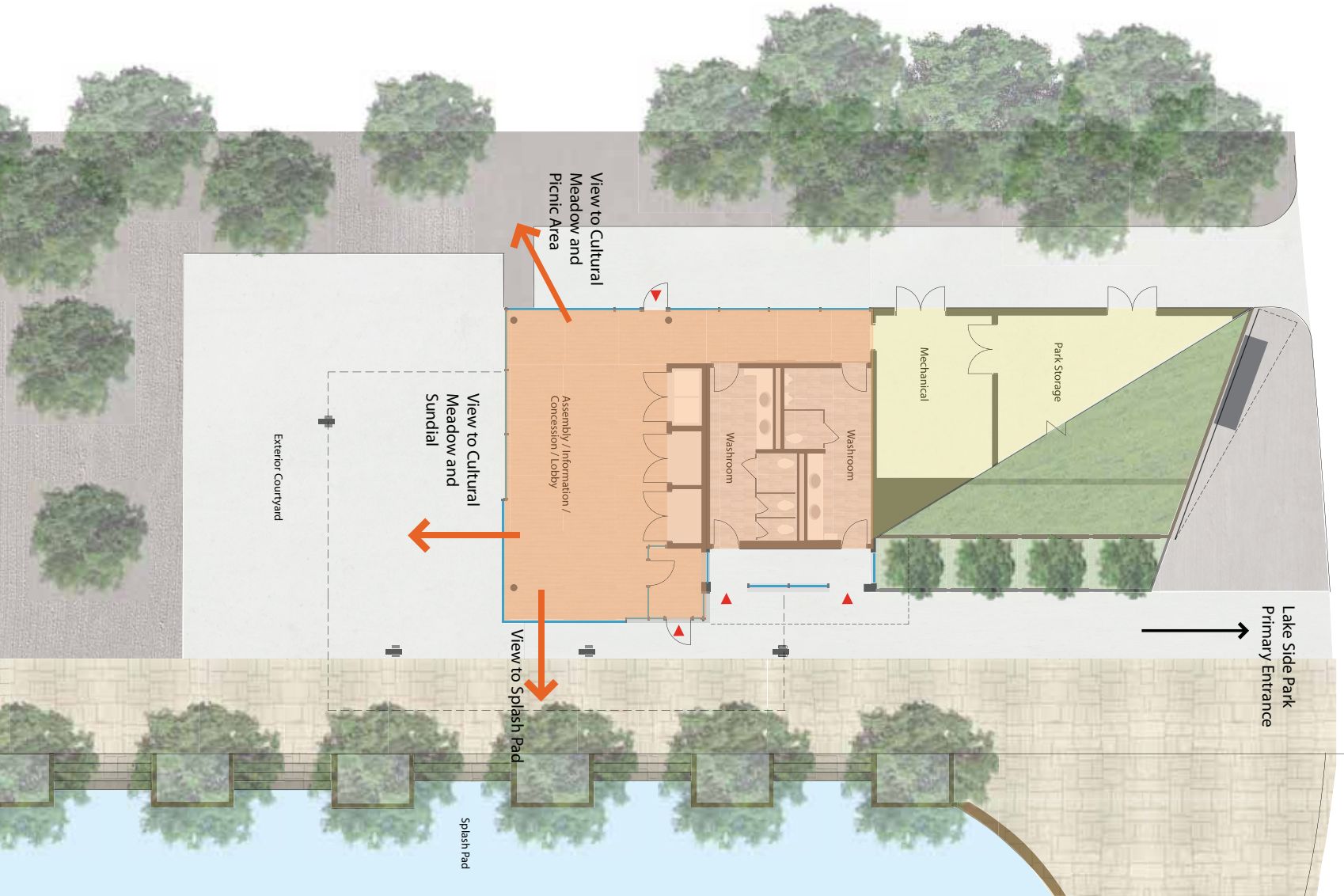
# A Appendix

Demonstration Design for J.C. Saddington Park Restaurant Building - Plan 01  
Demonstration Design for Lakeside Park Comfort Building- Plan and Elevations 02  
Demonstration Design for Lakeside Park Promontory - Elevations 03  
Demonstration Design for Fusion Property Promontory - Side Elevations 04

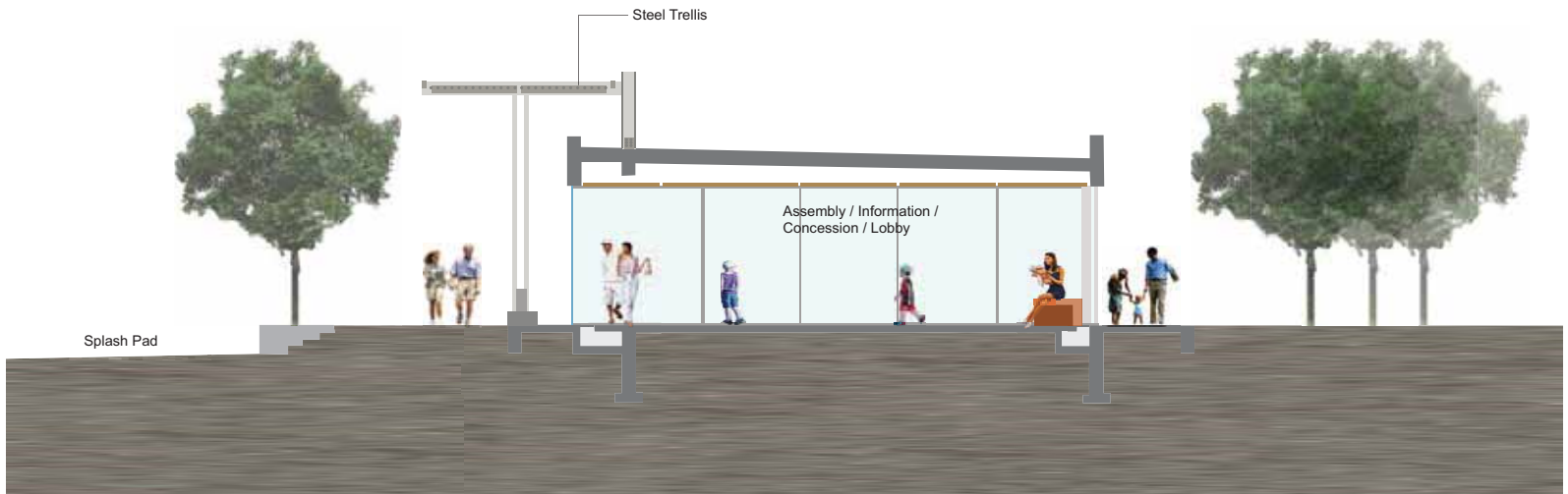
The drawing below demonstrates the key design opportunities for this potential new structure. The demonstration plan makes full use of its views to the lake with large expanses of glazing and both roof and main floor patios. The materials suggested are natural stone, tinted glass and lightly stained woods. The form of the building respond to the existing structures located to the north of the proposed restaurant and to the existing landscape of the park. Landscaping can be used to create seamless connections between the Waterfront Trail and the entrances into the Waterfront Restaurant.



Demonstration Design - Possible layout for a two storey building - J.C. Saddington Park , all-season restaurant building, ground floor.

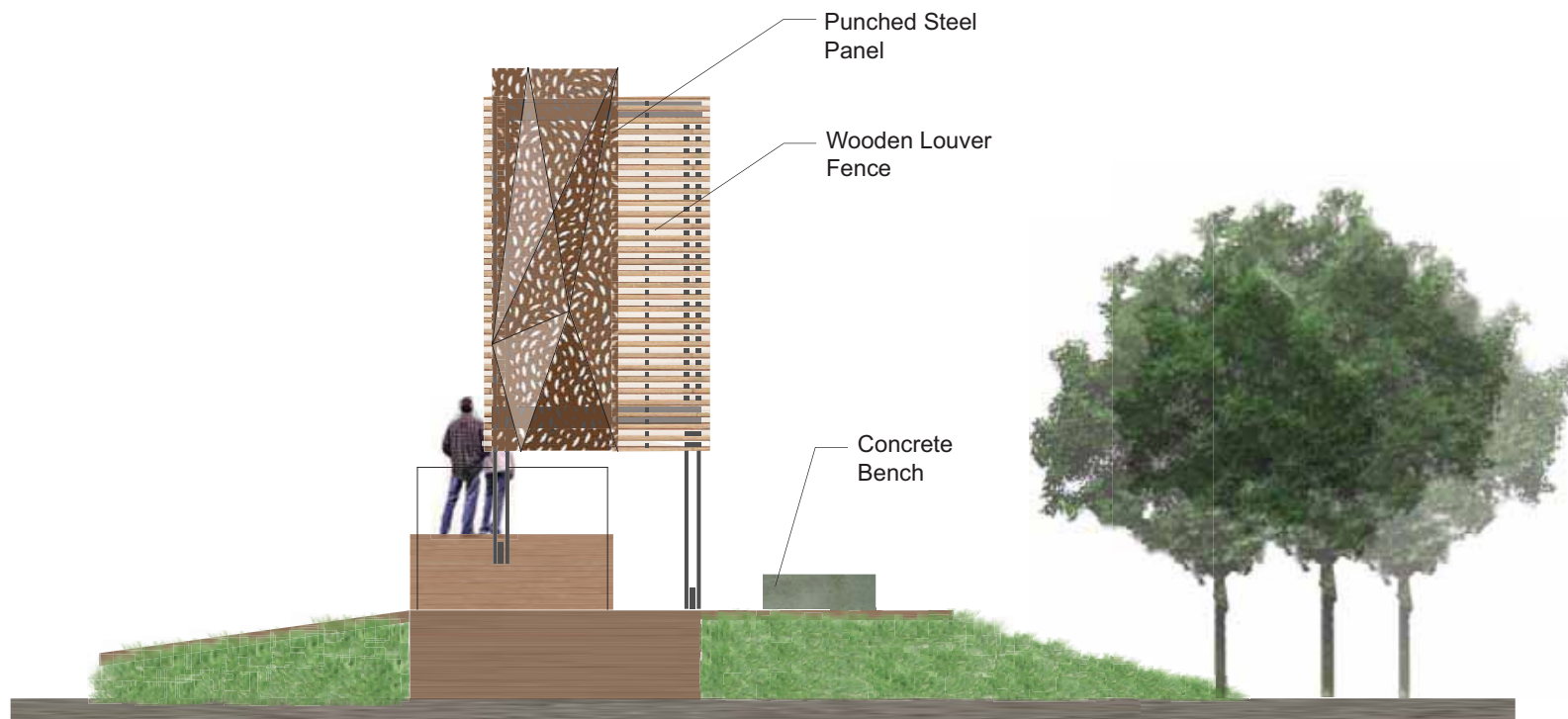


Demonstration Design - Possible Plan: Comfort building Lakeside Park, ground floor



Demonstration Design - Elevations: Comfort building Lakeside Park.





Demonstration Design - Elevations: Promontory Lakeside Park.



Demonstration Design: Promontory feature - Fusion Property, section through Promontory with Lake Ontario to the Left and the established forest to the right.

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**CITY OF MISSISSAUGA  
WATERFRONT PARKS STRATEGY  
COASTAL ENGINEERING OVERVIEW**



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**MARCH 2006**

**CITY OF MISSISSAUGA  
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COASTAL ENGINEERING OVERVIEW**

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## **1 INTRODUCTION**

The City of Mississauga has initiated a four-stage Waterfront Parks Strategy process to ensure that the waterfront is protected and remains the rich resource that it is. The objective of the strategy is to ensure the enhancement, creation and maintenance of sustainable, desirable and unique waterfront recreational opportunities and public spaces in recognition of Lake Ontario as an invaluable natural and cultural resource.

The first phase of the Strategy involved the completion of a public survey to ascertain and understanding of users, non-users, neighbours in regards to issues, opinions and use information and to assess park programming perceived deficits and surpluses. This phase was completed by the City.

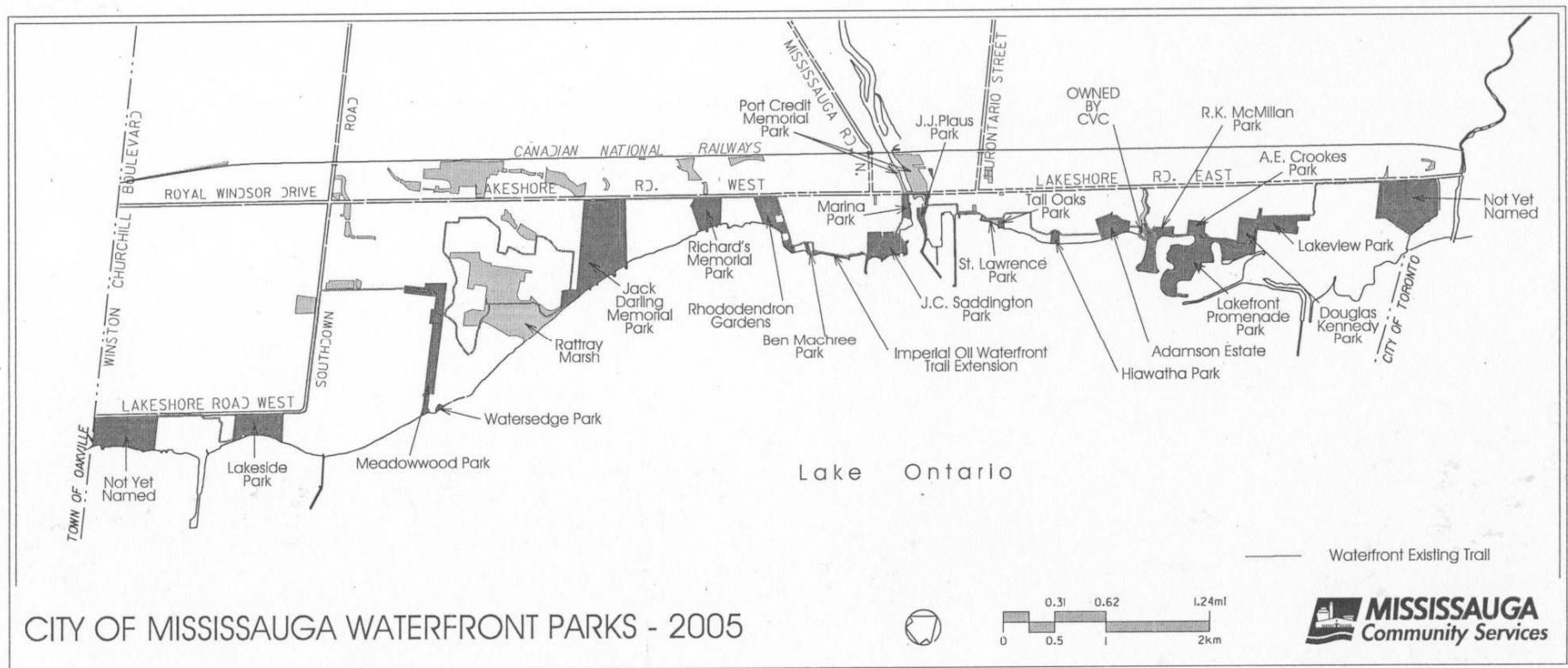
The next three stages of the Strategy include: preparation of a draft overall plan for the waterfront parks; site specific analyses and preparation of draft concepts; and preparation of a final overall plan, site specific concepts and an implementation plan.

In January 2006, the team led by Brook McIlroy Inc./Pace Architects was retained by the City to complete the final three phases of the Strategy. This report prepared by Baird & Associates for Brook McIlroy Inc./Pace Architects provides a coastal engineering overview of the parks and identifies issues and opportunities as input to the Strategy.

## **2 SITE LOCATION AND OVERVIEW**

The City of Mississauga is located at the western end of Lake Ontario. There are 24 waterfront parks included in the Waterfront Parks Strategy. Of these, four do not include actual waterfront and they are therefore not discussed in this report. There is a variety of shoreline types including eroding till banks, shingle and sand beaches, natural creeks and significant lengths of protected shoreline. The project shoreline and park locations are shown in Figure 2.1. A coastal review of each of the parks is provided in Section 4, with photographs in Appendices A1 and A2.





**Figure 2.1 Locations of City of Mississauga Waterfront Parks**

### **3 COASTAL CONDITIONS**

#### **3.1 Waves**

Wind-generated waves are the dominant driving force for erosion on lakes and larger rivers. Breaking waves result in unsteady flow (orbital velocities), steady flows (cross-shore and longshore currents) and turbulence, particularly under plunging breakers. Wave heights approaching from deepwater will change direction and will be reduced in height as the waves undergo the processes of refraction, diffraction and shoaling before reaching the shoreline.

Along exposed shorelines, wave conditions are typically depth-limited (i.e., wave heights are limited by the depth of water). Depth-limited waves can vary from approximately 0.6 to 1.5 times the depth, depending on the wave period and the nearshore slope.

#### **3.2 Water Levels**

Water levels on Lake Ontario vary in the long-term and seasonally in response to climatic conditions over the Great Lakes drainage basin (principally precipitation, runoff and evaporation), and in the short term due to the passage of individual storm events (localized surge or set-down due to wind stress and pressure effects). The typical seasonal variation in lake levels is approximately 0.5 m, with the average low occurring in December and the average high occurring in June (see Figure 3.1). The maximum recorded monthly-mean water level on Lake Ontario is 75.8 m and the minimum is 73.7 m, a range of almost 2 m. All elevations in this report are referenced to International Great Lakes Datum (IGLD 1985). Lake Ontario Chart Datum is 74.2 m IGLD 1985.

Extreme water levels must be considered for design of coastal works such as marinas and shore protection. The 100-year peak instantaneous water level (combined monthly mean and surge) at Toronto, the nearest site for which predictions are available is 75.72 m IGLD 1985, (MNR, 1989). A site-specific analysis would be required to define the storm surge at each individual site.

### 3.3 Great Lakes Shoreline Processes

Shorelines of the Great Lakes including the Mississauga shoreline are highly diverse and include:

- eroding cohesive bluffs and banks;
- bedrock shorelines (eroding and erosion resistant);
- dynamic sand and gravel beaches; and
- low-lying “muddy” shores (e.g., wetlands).

The nature of the impacts caused by shoreline protection structures depends very much on the characteristics of the local and regional shoreline morphology. Controlling substrate, surficial sediments, shoreline exposure and the modifying influence of the backshore deposit all play a role in the shoreline conditions. A general overview of shoreline morphology and the processes that help shape the shoreline is provided in Appendix B.

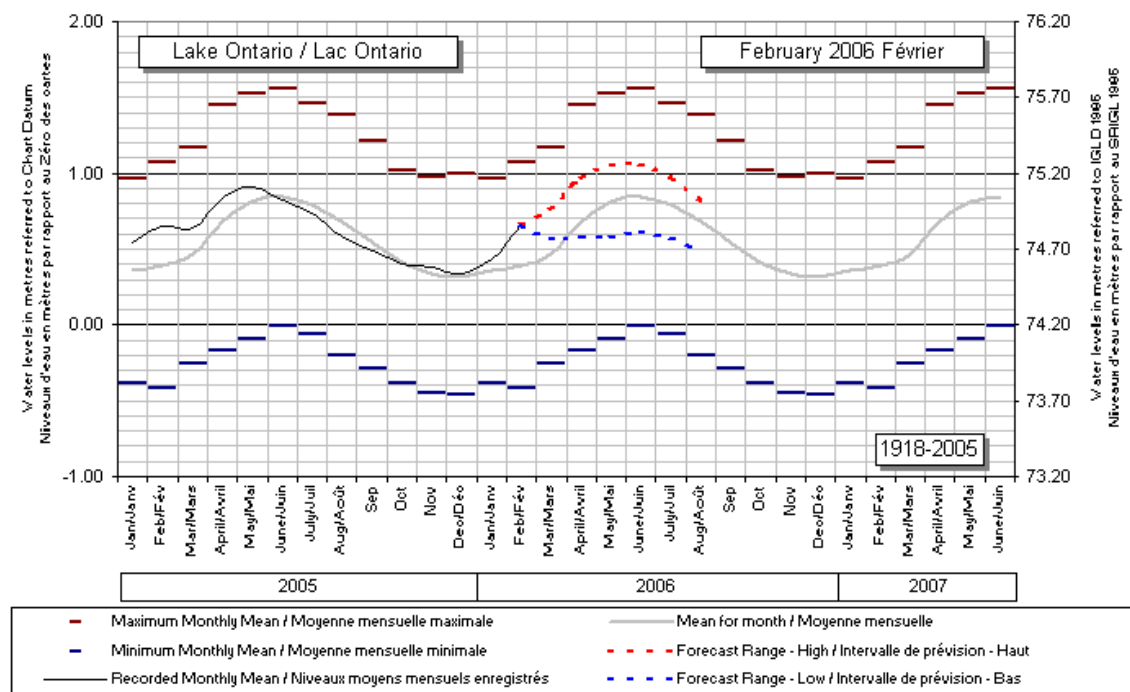


Figure 3.1 Monthly Mean Water Levels on Lake Ontario

## **4 PARK DESCRIPTIONS, ISSUES AND OPPORTUNITIES**

This section provides a description of the shoreline at each park, and identifies coastal issues and opportunities. The overview is based on site visits undertaken on Jan. 25, Feb. 8 and Mar. 16, 2006, a review of background reports and our knowledge of the shoreline. It is not intended to provide a fully comprehensive review of all coastal conditions, as this would be undertaken at a final design stage.

The waterfront parks discussed in this section are shown in Figure 2.1. The five priority parks identified by the City are discussed in Section 4.1, proceeding from west to east. The remaining parks are discussed in Section 4.2, again in order from west to east. A summary of shoreline type, shore protection, issues and opportunities is provided in the table at the end of this section.

### **4.1 Priority Parks**

The following parks were identified by the City as priority parks, to meet the immediate priority for redevelopment or improvements and are discussed in this section:

- Park 389 (Fusion);
- Lakeside Park;
- J.C. Saddington Park
- Marina Park; and
- Port Credit Memorial Park.

#### ***4.1.1 Park 389 (Fusion)***

This park is located at the western Mississauga boundary with the Town of Oakville. The park is currently not accessible to the public and includes an estimated 600 m of shoreline. The shoreline is largely low lying with vegetated, undeveloped backshore fronted by a cobble beach (see Photos A1.1 and A1.2). Joshua Creek empties into Lake Ontario at the west end of the park (see Photos A1.3 A1.4 and A1.5). The creek meanders in the property and is natural and unrestricted. The mouth of the creek is protected with armourstone breakwaters on both sides.

The shoreline on the west side of the creek is eroding till bluff (see Photo A1.6). There is significant accretion of cobble on the updrift (east side) of the breakwater as shown in Photo A1.2, indicating that the net direction of sediment transport is from east to west. The shoreline on the west side of the creek is starved of sediment and this has likely contributed to the erosion problem, though erosion of till bluffs along the Great Lakes shorelines is a natural process.

The shoreline at the east end of the site is also eroding till bluff. This shoreline has been partially protected with dumped armourstone. Although the armourstone has decreased

---

the rate of erosion, it is not an engineered structure and remediation is required to protect the shoreline. If left unprotected, the eroding shorelines at the east and west ends of the park will naturally continue to erode.

#### ***4.1.2 Lakeside Park***

The shoreline at the west end of the park is a low bank with a clay and shale shingle beach (see Photos A1.7 and A1.8). The beach composition is fairly unique in this area, and is the product of clay pipes dumped along the shoreline, that have broken down under wave action, forming a shingle beach. Sections of the bank are eroding as seen in Photo A1.9. Lakeside Creek empties into the lake near the western limit of the park.

The shoreline at the east end of the park is an eroding bank, approximately 6 m in height (see A1.9). The clay pipes that become exposed as the shoreline erodes, are visible in the photo. Photo A1.10 shows the makeup of the shingle beach.

If the shoreline is not protected, it will continue to erode. It is important however to recognize, that the clay shingle that makes up a large proportion of the beach, is a product of the eroding shoreline. If the shoreline is protected, this source of beach material will no longer exist and the composition of the beach will change as the clay is abraded by wave action.

#### ***4.1.3 J.C. Saddington Park***

This park, located on the west side of the mouth of the Credit River, includes an estimated 800 m of shoreline, divided between Lake Ontario and the mouth of the Credit River. The Lake Ontario shoreline is protected by dumped armourstone and scrap concrete (see Photo A1.11). Near the western limit of the park, two culverts empty into the lake. The culverts are protected by a gabion enclosure and armourstone has been placed on the lake side, presumably to protect the culverts from wave action. On the day of the site visit, the culverts were blocked with debris (see Photo A1.12).

Erosion of the backshore due to leaching of fines through the structure was observed where concrete and armourstone have been dumped on the shoreline and no geotextile or riprap have been placed as a filter (see Photo A1.13). There was also evidence of overtopping during a recent storm event (see runup line indicated by debris in Photo A1.14). Emergency repairs to the shore protection have been undertaken by the City (see riprap dumped in eroded areas inshore of armourstone in Photo A1.15 and A1.16). A longer term solution is required.

The Credit River marks the east end of the park. The shoreline is protected by a stacked armourstone wall (see Photos A1.17 and A1.18). A boardwalk runs along the top of sections of the wall. Geotextile was visible along the back of the wall.



The shoreline extends into an embayment (see Photo A1.19). Irregularly placed armourstone protects this stretch of shoreline, extending in front of the apartment building shown in Photos A1.19 and A1.20. Although the armourstone provides some level of protection, an engineered solution is required for longer term protection.

#### **4.1.4 Marina Park**

Marina Park is located on the west side of the Credit River, south of the Lakeshore Road bridge. The park is undeveloped and presently consists of two parking lots. The northerly lot is protected by a steel sheet pile wall (see Photo A1.21). Timber docks extend from the wall and the docks are used by charter fishing boats (see Photo A1.22). There is a public boat ramp midway along the shoreline. South of the boat ramp, the shoreline is protected by a mix of shore protection in varying states of disrepair (see Photo A1.23). Remedial work is required to protect the shoreline, resolve safety hazards and improve its appearance.

#### **4.1.5 Port Credit Memorial Park (West Side)**

This park is located on the west side of the Credit River, between Lakeshore Road and the railway bridge. The canoe and rowing clubs are located on the shoreline, in the park. Although work has recently been undertaken on the east side of the river (Memorial Park East), very little work has been done on the west side in recent years. The river bank has been protected using a number of methods including concrete slabs (A1.24), rip rap (A1.25) and gabion baskets (A1.26). The gabion baskets were recently installed. Shore protection along the other sections of shoreline are in a state of disrepair.

### **4.2 Other Waterfront Parks**

This section describes the following waterfront parks, listed in order from west to east:

- Meadowwood Park
- Watersedge Park
- Rattray Marsh
- Jack Darling Park
- Richard's Memorial Park
- Rhododendron Park
- Ben Machree Park
- Imperial Oil Waterfront Extension
- J.J. Plaus Park
- St. Lawrence Park
- Tall Oaks Park
- Hiawatha Park

- Adamson Estate
- R.K. McMillan Park
- Lakefront Promenade
- A.E. Crookes Park
- Douglas Kennedy Park
- Lakeview Park
- Arsenal Lands (not yet named)

#### **4.2.1 Meadowwood Park**

The shoreline is a low bank, estimated 2 to 3 m in height, protected by a stacked armourstone wall (see Photos A2.1 and A2.2). The wall appears to be in reasonably good condition and a filter layer of rip rap was visible behind the armourstone (Photo A2.3). There is a predominantly sand beach with some shingle along the toe of the armourstone wall. The beach was an estimated 8 m in width on the day of the site visit.

#### **4.2.2 Watersedge Park**

The shoreline is a low bank, estimated 2 to 3 m in height, protected by a stacked armourstone wall (see Photos A2.4 and A2.5). The wall appears to be in reasonably good condition and a filter layer of rip rap was visible behind the armourstone. An armourstone groyne is located midway along the park shoreline. A narrow sand and shingle beach has formed along the toe of the armourstone wall on the east side of the groyne. There is scattered rip rap and scrap concrete in the nearshore.

#### **4.2.3 Rattray Marsh**

This shoreline is a classic barrier beach separating Rattray Marsh from Lake Ontario. The beach is comprised of sand with some cobble and appears to have been relatively stable in recent years, based on the presence of significant tree growth along the back of the beach (see Photo A2.6). The beach is unprotected.

#### **4.2.4 Jack Darling Memorial Park**

Jack Darling Park is located on low lying land fronted by a mixed sand and cobble beach. The redevelopment of Jack Darling Park was completed in 1994 including construction of armourstone headlands (Photo A2.7), rip rap revetments (Photo A2.8), and beaches retained between headlands (Photo A2.9). In some areas protected by beaches, wave runup reaches the pathways during storm events as seen by the debris line (see Photo A2.10). Concrete ramps were constructed for shoreline access (Photo A2.11). Over the years erosion of the substrate material under the ramps has caused damage to the ramps. Some remedial work was done in 1999 to provide temporary protection but has run its

lifespan. The City intends to undertake further remedial work on the western ramp, to provide a long term solution, and remove the easterly ramp in 2006.

#### **4.2.5     *Richard's Memorial Park***

The shoreline is a sandy beach, largely unprotected (see Photos A2.12 and A2.13 and A2.14). There is a headland located near the centre of the park, constructed of scrap concrete. A relatively large box culvert is located on the west side of the headland (see Photo A2.15). There is an armourstone and riprap groyne toward the east end of the park. The shoreline rises east of the park and the adjacent property is protected by a stacked armourstone wall, visible in Photo A2.14. The shore protection and shoreline appearance could be improved by upgrading the shore protection, which is largely constructed of scrap material.

#### **4.2.6     *Rhododendron Park***

The shoreline at the west end of the park is a till bank, approximately 5 m in height. The shoreline elevation decreases in an easterly direction (see Photo A2.16). There is intermittent protection along the shoreline including irregularly placed armourstone (see Photo A2.17) and three sections of old stone and concrete walls (see Photo A2.18), however much of the shoreline is unprotected. No riprap or filter cloth was observed behind the armourstone. A small stream empties into the lake near the centre of the park shoreline. The shoreline does not appear to be actively eroding at present, however it would be prone to erosion when higher water levels combine with storm events. There is opportunity to improve the shoreline in this park.

#### **4.2.7     *Ben Machree Park***

The shoreline is a low bank protected by a stepped armourstone wall, backfilled with scrap concrete curb (see Photo A2.19). No geotextile was visible. At the west end of the site there is a small sand and cobble beach (see Photo A2.20). The beach shoreline is unprotected and is retained by the adjacent shorelines on either side, which are protected.

#### **4.2.8     *Imperial Oil Waterfront Extension***

The park occupies a narrow band of shoreline extending from Ben Machree Park (west end) to J.C. Saddington Park (east end). The west end of the park is protected by irregularly placed armourstone (see Photo A2.21). No filter cloth was visible and there is evidence of loss of fines from the backshore. The east end of the site was protected last fall with riprap (see Photos A2.22, A2.23 and A2.24). Again, filter cloth is not visible and it is not clear whether or not it was placed. The backshore is currently unvegetated and susceptible to erosion from runoff.

#### **4.2.9 J.J. Plaus Park**

The park is located on the east side of the Credit River, south of Lakeshore Road. It is a landscaped urban park. The riverbank is protected by a concrete capped steel sheet pile wall (see Photos A2.25). There is a timber curb along the lakeward edge of the wall. Safety ladders are recessed in the steel sheet piling. There is a marina located at the south end of the park (see Photo A2.26). A detailed inspection of the seawall was beyond the scope of this project, however, the wall appeared to be in relatively good repair (Photo A2.27).

#### **4.2.10 St. Lawrence Park**

The west section of the park is protected by a steel sheet pile wall (see Photo A2.28). A small beach has accreted at the west end of the park, adjacent to the pier. The remainder of the park is protected by a relatively new stepped armourstone wall, 4 tiers of stone were visible. The wall has been backfilled with riprap (see Photos A2.29 and A2.30). A toe stone has been placed to reduce the risk of undermining of the seawall. The crest elevation was estimated to be 79 m IGLD 1985. There is a headland with lookout located roughly midway along the park.

#### **4.2.11 Tall Oaks Park**

Tall Oaks Park is located adjacent to St. Lawrence Park, on its east side. There is significant contrast between the shore protection at the two parks. Whereas St. Lawrence Park is protected by a substantial armourstone wall, much of Tall Oaks Park is protected by a shingle beach, retained by a fieldstone headland and a concrete and armourstone groyne (see Photos A2.31 and A2.32). The headland creates a point and although it is likely overtopped during storm events, the trees on the point indicate some level of stability. There is an armourstone curb along the back of the beach.

At the east end of the park, there is a road allowance with a culvert (see Photo A2.33). The culvert is protected by concrete blocks. An old capped iron pipe is also exposed on the beach. The shoreline is protected by gabions and the wire baskets have corroded. The shore protection at the road allowance and the culvert protection should be replaced.

#### **4.2.12 Hiawatha Park**

The shoreline at this local park is protected by armourstone and dumped concrete (see Photos A2.34 and A2.35). The lakebed is shallow, estimated 74.5 m IGLD 1985 at the toe of the armourstone. Based on the irregular stone placement, it is likely that stone and concrete were dumped over a period of time, to protect the eroding shoreline, and that it is not an engineered design with a filter. Monitoring is recommended.

#### **4.2.13 Adamson Estate**

The shoreline is protected by an armourstone revetment at the west end (see Photo A2.36) and a stacked armourstone wall at the east end (see Photo A2.37). A small shingle beach has accreted in front of the armourstone wall. The shore protection is in a good state of repair and no additional work is anticipated in the near future.

#### **4.2.14 R.K. McMillan Park**

This park includes a section with no shoreline, south of Lakeside Avenue, and sections on either side of Cooksville Creek. The view from the land locked section of the park is shown in Photo A2.38. The shoreline is a pea gravel beach, sheltered by the armourstone headlands of Lakefront Promenade, though it is our understanding that this is not part of R.K. MacMillan Park and no comment is made on that shoreline in this report.

The creek shoreline is shown in Photos A2.39 and A2.40. The east side of the creek is protected by an armourstone revetment. The revetment is older construction and no geotextile was visible, though it may exist. The west side of the river is largely unprotected and a sandy spit has formed at the river mouth (see Photo A2.39).

#### **4.2.15 Lakefront Promenade**

This park is a large lakefill project, constructed in the 1980's and it includes several kilometres of shoreline. A detailed assessment of the shoreline is beyond the scope of this report. There are a range of shoreline types and structures including beaches in sheltered areas, riprap revetments, armourstone headlands and boat ramps (see Photos A2.41 to A2.44). The City monitors the shoreline and has undertaken repairs, i.e. ongoing repairs for erosion north of boat ramp.

#### **4.2.16 A.E. Crookes Park**

This park does not include any waterfront and therefore no comment is provided.

#### **4.2.17 Douglas Kennedy Park**

This park does not include any waterfront and therefore no comment is provided.

#### **4.2.18 Lakeview Park**

This park does not include any waterfront and therefore no comment is provided.



#### **4.2.19    *Arsenal Lands (not yet named)***

### **4.3        Summary of Coastal Conditions**

The table below provides a summary of shoreline type, shore protection, issues and opportunities for each of the parks discussed in the previous sections.

<b>Park</b>	<b>Shoreline Type</b>	<b>Shore Protection</b>	<b>Issues</b>	<b>Opportunities</b>
Park 389 - Fusion (not yet named)	Low bank and eroding bluff fronted by cobble beach.	Largely unprotected with the exception of armourstone breakwaters at mouth of Joshua Creek, armourstone groyne, and dumped armourstone at the east end of the site.	Erosion of unprotected till bluff shoreline sections. Potential for flooding due to creek.	Natural meandering creek. Natural cobble beach with no development in backshore. Potential to retain natural shoreline.
Lakeside Park	Low bank at west end of property, erosion along some sections observed. Higher eroding bank at east end of property.	Unprotected.	Shoreline erosion. Consider protecting some sections of shoreline.	Unique clay and shale shingle beach. Clay shingle supply is a consideration if existing beach composition is to be maintained.
J.C. Saddington Park	Protected eroding bank.	Eroding bank protected on Lake Ontario shoreline by dumped armourstone and scrap concrete. Protection in Credit River includes stepped armourstone wall and dumped armourstone and concrete.	Shore protection along the Lake Ontario shoreline is not engineered and is failing. Sections of the protection along the Credit River are also failing.	Potential to improve the entire park appearance by upgrading the shore protection.
Marina Park	Protected river bank.	Steel sheet piling, irregularly placed armourstone, timber piles, concrete blocks.	Shore protection has fallen into disrepair and is now a safety and erosion hazard. Finger piers also require repairs.	Improvements to shore protection will result in improved park amenity.

<b>Park</b>	<b>Shoreline Type</b>	<b>Shore Protection</b>	<b>Issues</b>	<b>Opportunities</b>
Port Credit Memorial Park (west side)	Protected river bank.	Concrete slabs, riprap, gabion baskets.	Shore protection has fallen into disrepair and is now an erosion hazard.	Improvements to shore protection will result in improved park amenity.
Meadowwood Park	Low bank.	Armourstone wall.	No specific issues identified at this time.	No specific opportunities identified at this time.
Watersedge Park	Low bank.	Armourstone wall.	No specific issues identified at this time.	No specific opportunities identified at this time.
Rattray Marsh	Barrier beach.	Unprotected.	No specific issues identified at this time.	No specific opportunities identified at this time.
Jack Darling Park	Beach	Armourstone headlands, revetments, unprotected beach.	Repairs to ramps being undertaken by City.	No specific opportunities identified at this time.
Richard's Memorial Park	Beach	Scrap concrete headland, armourstone and riprap groyne.	Shoreline structures not engineered.	Improvements to shoreline works would improve waterfront.
Rhododendron Park	Estimated 5 m bank at west end of park, low bank and beach at east end.	Intermittent including irregularly placed armourstone and old stone walls.	Protection is intermittent, not engineered.	Improvements to shoreline works would improve waterfront.
Ben Machree Park	Low bank, small pocket beach at west end of site.	Low bank protected by armourstone and scrap concrete, unprotected beach at west end of site.	Not possible to determine if armourstone and scrap concrete wall includes geotextile. If not, life of structure may be relatively limited.	Improvements to shoreline works would improve waterfront.

<b>Park</b>	<b>Shoreline Type</b>	<b>Shore Protection</b>	<b>Issues</b>	<b>Opportunities</b>
Imperial Oil Waterfront Extension	Natural shoreline is eroding till bank.	Irregularly placed armourstone at west end of park, no geotextile visible. Dumped riprap at east end of site (City should know if filter cloth was placed).	Shoreline structures do not appear to be engineered (i.e. no filter cloth). In the long term, improvements to shore protection will be required.	
J.J. Plaus Park	Natural shoreline is river bank.	Steel Sheet pile wall with concrete cap.	No coastal issues identified at this time.	No coastal opportunities identified at this time.
St. Lawrence Park	Natural shoreline is till bank.	Armourstone wall through most of park, steel sheet pile wall at west end.	Recently upgraded park, no coastal issues identified.	Recently upgraded park, no new coastal opportunities identified.
Tall Oaks Park	Natural shoreline is till bank.	Headland, shingle beach. Gabions.	Gabion baskets protecting shoreline at road allowance have deteriorated. Gabions are generally not suitable shore protection on exposed shorelines. Culvert requires attention.	Improvements to shore protection at road allowance will improve park amenity. Remove pipe on beach.
Hiawatha Park	Natural shoreline is eroding till bank.	Irregularly placed armourstone.	Shore protection does not appear to be engineered. Monitoring recommended.	No coastal opportunities identified at this time.
Adamson Estate	Natural shoreline is eroding till bank.	Armourstone seawall and revetment.	No coastal issues identified at this time.	No coastal opportunities identified at this time.

<b>Park</b>	<b>Shoreline Type</b>	<b>Shore Protection</b>	<b>Issues</b>	<b>Opportunities</b>
R.K. McMillan Park	River bank.	Armourstone revetment on east side, west side unprotected.	No coastal issues identified at this time. Monitor shore protection on east river bank.	Natural creek mouth – leave in natural state.
Lakefront Promenade	Lakefill.	Various including riprap, armourstone headlands, sheltered embayments.	No coastal issues identified based on cursory review. City undertaking ongoing maintenance.	No coastal opportunities identified at this time.
A.E. Crookes Park	No shoreline.			
Douglas Kennedy Park	No shoreline.			
Lakeview Park	No shoreline.			
Arsenal Lands (not yet named)	No shoreline.			



## **5 SUMMARY AND CONCLUSIONS**

This report provides preliminary definition of coastal conditions at the project site. A general discuss about waves, water levels and general shoreline types is provided. An overview of shoreline conditions at each of the parks is presented, including a discussion of shoreline type, the general condition of the shoreline and shore protection where applicable, identification of issues and opportunities. This information is intended to provide a general overview. Specific areas may be further investigated in the design phase if additional work related to shore protection is undertaken.

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## **APPENDIX A1**

### **PHOTOS – PRIORITY PARKS**



**Photo A1.1 Fusion Park – View east along cobble beach**



**Photo A1.2 Fusion – View west toward breakwater at mouth of creek**



**Photo A1.3 Fusion Park – View south from estate house**



**Photo A1.4 Fusion – View south toward mouth of creek**





**Photo A1.5 Fusion – View north from mouth of creek**



**Photo A1.6 Fusion – Eroding bluff west of creek**





**Photo A1.7 Lakeside Park - View east from western end of park**



**Photo A1.8 Lakeside Park - View west from eastern end of park**



**Photo A1.9 Lakeside Park – View west show eroding bank at east end of site**



**Photo A1.10 Lakeside Park – Natural shale and clay shingle beach**





**Photo A1.11 J.C. Saddington – Dumped armourstone at western limit of park**



**Photo A1.12 J.C. Saddington – Debris blocking culverts at west end of park**



**Photo A1.13 J.C. Saddington – Shoreline erosion and deteriorated shore protection**



**Photo A1.14 J.C. Saddington – Debris indicates runup elevation, overtopping**





**Photo A1.15 J.C. Saddington – Short term repairs include dumping riprap**



**Photo A1.16 J.C. Saddington – Dumped concrete protection**



**Photo A1.17 J.C. Saddington – Armourstone wall along east side of park showing exposed geotextile**



**Photo A1.18 J.C. Saddington – Armourstone wall and boardwalk along credit river**





**Photo A1.19 J.C. Saddington – Dumped armourstone at east end of park**



**Photo A1.20 J.C. Saddington – Dumped armourstone at park extension fronting apartment**



**Photo A1.21 Marina Park – View northwest toward lakeshore road**



**Photo A1.22 Marina Park – View south toward boat ramp**





**Photo A1.23 Marina Park – Shoreline at South end of Park**



**Photo A1.24 Port Credit Memorial Park (West Side) – Southern limit, near bridge**



**Photo A1.25 Port Credit Memorial Park (West Side) – Concrete slabs protecting canoe club**



**Photo A1.26 Port Credit Memorial Park (West Side) – Rip rap and gabion baskets**

## **APPENDIX A2 – OTHER PARKS**





**Photo A2.1 Meadowwood Park – View east showing armourstone wall and sand beach**



**Photo A2.2 Meadowwood Park – View west along armourstone wall**



**Photo A2.3 Meadowwood Park – View east showing stacked armourstone wall**



**Photo A2.4 Watersedge Park – View east showing stacked armourstone wall with groyne in foreground**





**Photo A2.5 Watersedge Park – View west showing stacked armourstone wall**



**Photo A2.6 Rattray Marsh – View west showing tree growth along back of barrier beach**



**Photo A2.7 Jack Darling Memorial Park – View east from west end of park with unprotected beach in foreground and headland in background**



**Photo A2.8 Jack Darling Memorial Park – View east from central headland**



**Photo A2.9 Jack Darling Memorial Park – View east along sandy beach**



**Photo A2.10 Jack Darling Memorial Park – View west showing headland**





**Photo A2.11 Jack Darling Memorial Park – View east with concrete ramp in foreground**

**Photo A2.12 Richards Memorial Park – View south toward water**





**Photo A2.13 Richards Memorial Park – View east from centre of park toward groyne**



**Photo A2.14 Richards Memorial Park – View east toward eastern limit of park**





**Photo A2.15 Richard's Memorial Park – Scrap concrete protecting headland in foreground, ox culvert located on west side of headland**



**Photo A2.16 Rhododendron Park – View east along cobble beach**



**Photo A2.17 Rhododendron Park – Irregularly placed armourstone on shoreline**



**Photo A2.18 Rhododendron Park – Mixed sand and cobble beach at east end of site with old stone wall in background**





**Photo A2.19 Ben Machree – Scrap concrete revetment**



**Photo A2.20 Ben Machree – Pocket beach at west end of park**



**Photo A2.21 Imperial Oil Extension – View west along armoustone revetment**



**Photo A2.22 Imperial Oil Extension – View west along armoustone revetment**





**Photo A2.23 Imperial Oil Extension – View west along dumped armourstone**



**Photo A2.24 Imperial Oil Extension – View east**



**Photo A2.25 J.J. Plaus Park – View south along SSP wall**



**Photo A2.26 J.J. Plaus Park – View toward marina at south end of park**





**Photo A2.27 J.J. Plaus Park – View north along SSP wall**



**Photo A2.28 St. Lawrence Park – View west toward steel sheet pile wall**



**Photo A2.29 St. Lawrence Park – View west along armourstone wall**



**Photo A2.30 St. Lawrence Park – View west along armourstone wall**





**Photo A2.31 Tall Oaks – View west toward headland**



**Photo A2.32 Tall Oaks – View east with cobble beach in foreground**



**Photo A2.33 Tall Oaks – Culvert at road allowance and gabion shore protection**



**Photo A2.34 Hiawatha Park – View west along armourstone shore protection**





**Photo A2.35 Hiawatha Park – View east along armourstone shore protection**



**Photo A2.36 Adamson Estate – Armourstone revetment at west end of park**



**Photo A2.37 Adamson Estate – Stepped armourstone wall at east end of park**



**Photo A2.38 R.K. MacMillan – View south toward headland from beach**





**Photo A2.39 R.K. MacMillan – Armourstone protection at creek on west side of park**



**Photo A2.40 R.K. MacMillan – Armourstone protection at creek on west side of park**



**Photo A2.41 Lakefront Promenade as seen from Adamson Estate**



**Photo A2.42 Lakefront Promenade - Riprap protection in sheltered embayment**





**Photo A2.43 Lakefront Promenade – Boat ramp**



**Photo A2.44 Lakefront Promenade – Armourstone headland**

**APPENDIX B**  
**GREAT LAKES SHORELINES**

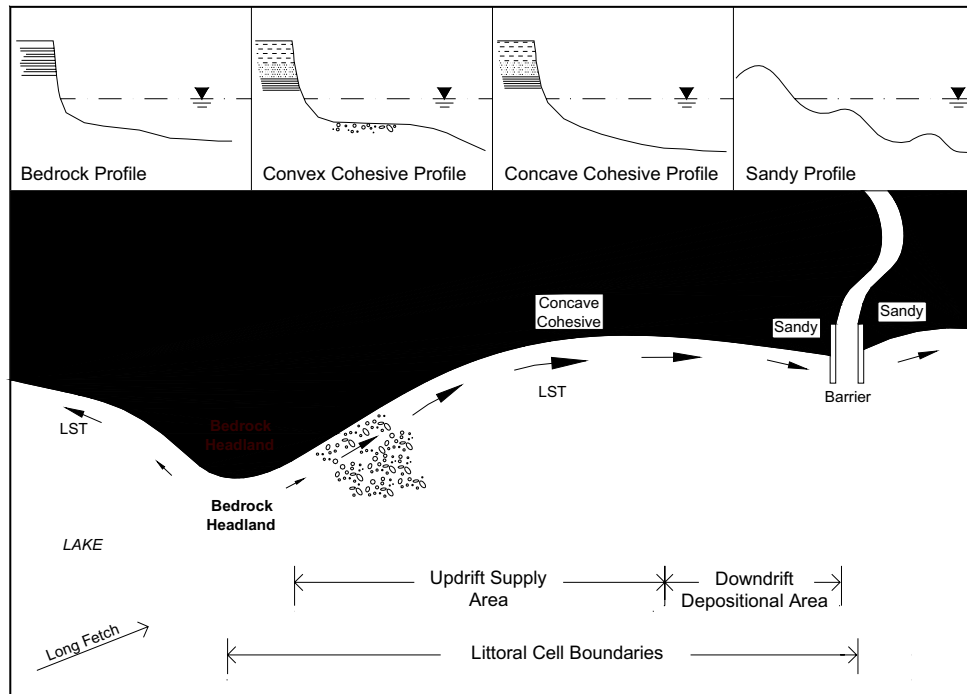
## **Introduction to Great Lakes' Shoreline Types**

There are three basic shore types on the Great Lakes: sandy, cohesive and bedrock. The shoreline processes for these three main shore types are discussed briefly to provide an understanding of the processes at Oshawa. The role of erosion, sedimentation, and longshore sediment transport in the long-term evolution of the three shore types will be presented within the context of a hypothetical littoral cell, presented in Figure B.1.

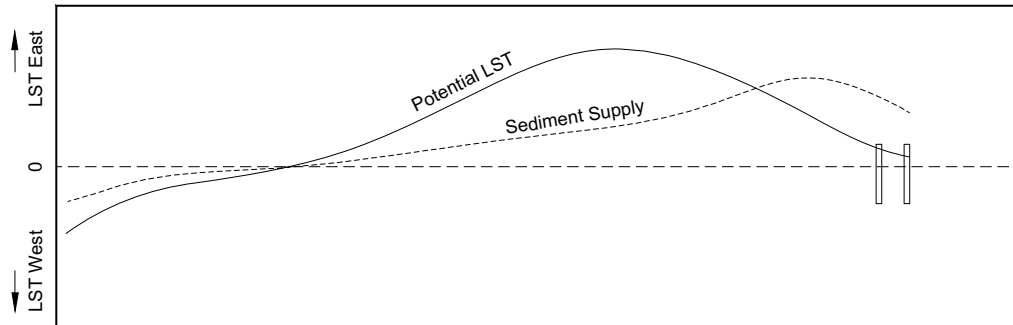
A littoral cell is a concept utilized to identify shoreline compartments or sediment boundaries based on the supply, transport and re-distribution of sand and gravel sized littoral material along the shore. Within a littoral cell, there is generally a net direction of longshore sediment transport (LST) due to the incident wave climate and there are no (or only minimal) losses or gains of sediment at the cell boundaries to adjacent cells. Therefore, a littoral cell is a self-contained unit and changes to the supply and transport of littoral sediment within the cell are limited to that cell and do not impact adjacent littoral cells. Littoral sub-cells are contained by promontories or features, which may not intercept all of the longshore transport in the surf zone, particularly under extreme conditions.

In the case of the hypothetical littoral cell in Panel A of Figure B.1, a bedrock headland defines the updrift boundary, while the harbor jetties create a littoral barrier and represents the limits of the downdrift depositional area. The littoral cell model in Figure B.1 is discussed further in the following sections on bedrock, cohesive and sandy shorelines.

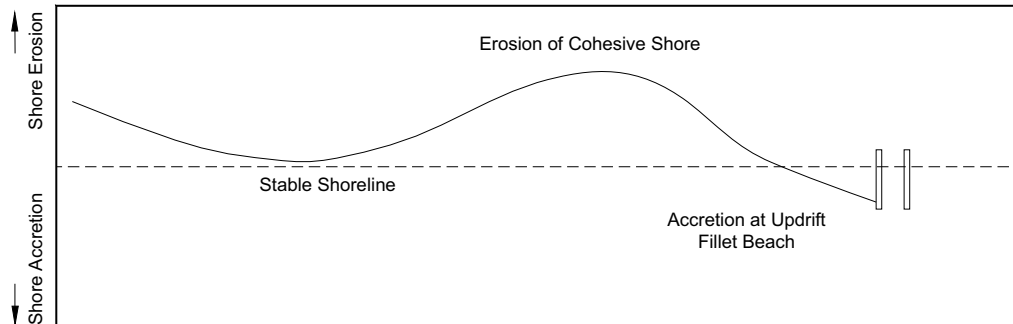
### PANEL A: Conceptual Littoral Cell



### PANEL B: Longshore Sediment Transport



### PANEL C: Long Term Shoreline Trend



Adopted from Davidson-Arnott, 1990

**Figure B.1 Shoreline Processes for a Conceptual Littoral Cell**



### *Bedrock Shorelines*

For a typical bedrock shoreline on the Great Lakes, the nearshore lakebed and bluff toe have developed in weak shale and limestone. The shale is capped with glacial till, clay and sand. The mechanical forces of wave action in the nearshore and wave attack at the base of rocky bluffs, frost action, expansion due to freezing and temperature-dependant wetting and drying are the primary mechanism leading to shore platform development and bluff erosion for bedrock coasts.

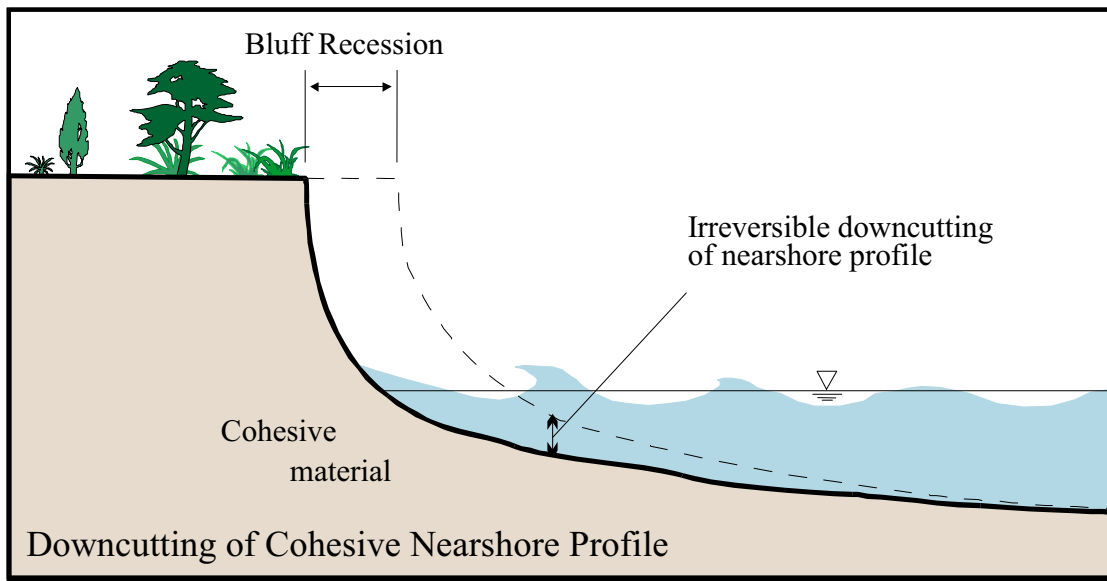
Although bedrock shores are erodible under direct wave attack and other physical/chemical processes, they are generally more erosion resistant than cohesive and sandy shorelines. Therefore, as seen in Panel A of the littoral cell model in Figure B.1, the bedrock outcrop results in the development of a prominent headland feature that forms the updrift littoral cell boundary. The direction and magnitude of the net LST rates are represented by the arrows in Panel A to highlight the influence of the headland on transport directions (i.e. creation of a divergent node). The direction and magnitude of LST is governed by the orientation of the shoreline relative to the direction of the incoming wave energy, the amount of wave energy, the available supply of littoral material and the size of the material.

### *Cohesive Shorelines*

The shoreline in the area is classified as a “cohesive shoreline”. It is comprised of consolidated mixture of silts, clays, sand and gravel. The controlling process for the recession of a cohesive shoreline bluff is the downcutting, or downwards erosion, of the nearshore cohesive profile by the wave induced forces (see Figure B.2). The ongoing downcutting of the nearshore profile, or lakebed, eventually results in the toe of the shoreline bluff being undercut. When the bluff is sufficiently over-steepened it will collapse resulting in recession of the crest. If the backshore composition of the shoreline remains the same, cohesive shorelines recede over time without change in the shape of their profile. Even though the process is governed by the downward cutting of the nearshore bottom by wave action, the effect is a horizontal translation of the entire shoreline profile at the long-term average bluff recession rate (see Figure B.2).

The erosion or downcutting of the cohesive material is irreversible and ongoing. The fine particles of silt and clay are carried off to the deep water where they settle to the bottom. The remaining small volume of coarser materials, or beach materials (i.e., sand, gravel and cobbles which are large enough to remain in the breaking wave zone), is moved alongshore and on- and offshore by the wave action.

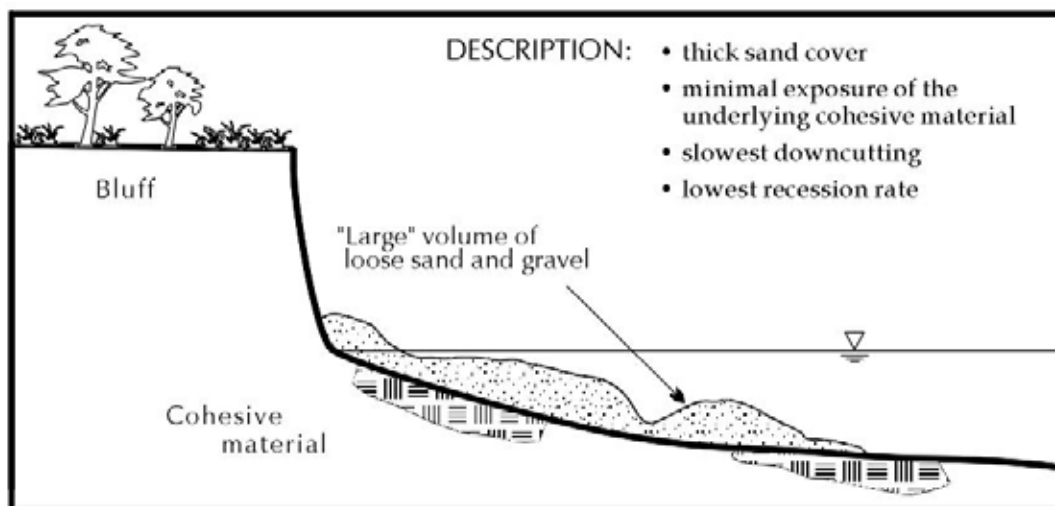
Cohesive shoreline profiles in predominantly fine-grained material (i.e., high percentage of silts and clays with relatively low percentages of sand and gravel or cobbles) are characterized as distinctively concave (see Figure B.2). The constant concave profile shape over time indicates that the downcutting rate is the greatest at the shoreline and gets less towards the offshore.



**Figure B.2 Cohesive Shoreline**

Cohesive shorelines differ from sandy shorelines. Along typical cohesive shorelines, the wave energy available to move beach materials exceeds the supply of beach materials from erosion of the shoreline. Narrow beaches may form during the calmer summer months. However, the beach material cannot accumulate in sufficient volumes to form a protective cover for the underlying cohesive substrate during storms.

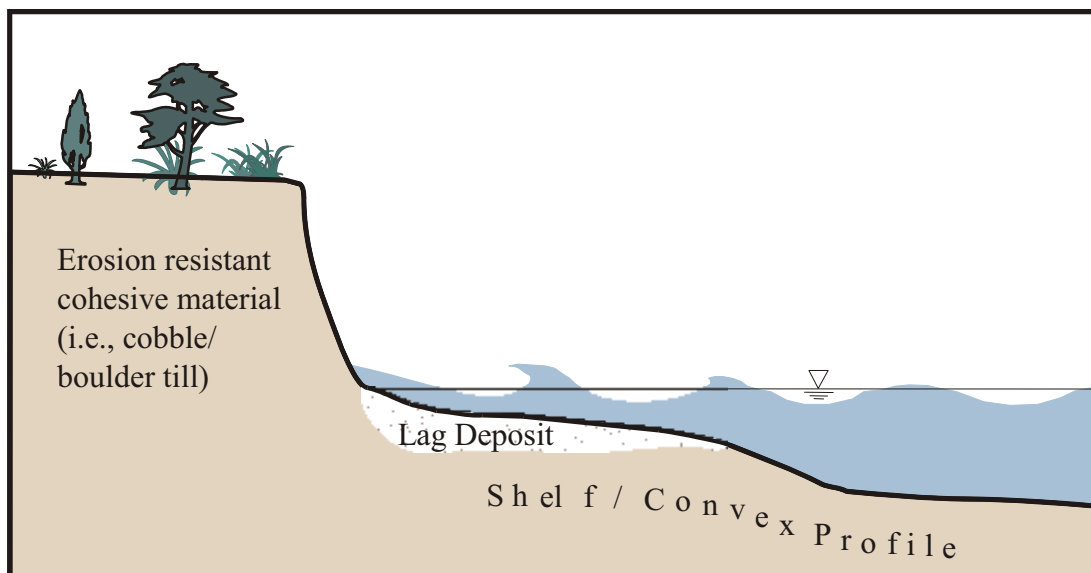
When sand volumes in the nearshore exceed the thickness of active sediment motion during storm events, the sand cover can protect the underlying cohesive substratum from downcutting. At Oshawa, adjacent to the harbour jetty, accretion is occurring. If there is a sufficient thickness of sand, the underlying cohesive material is protected from erosion (see Figure B.3).



**Figure B.3 Sand Cover Over Cohesive Material**

With reference to the conceptual littoral cell in Panel A of Figure B.1, the eroding cohesive shores represent an updrift supply area for new sand and gravel. However, due to the small fraction of sand and gravel in the eroded bluffs, and thus in the nearshore zone, the potential longshore sediment transport rate exceeds the available sediment supply. This process is illustrated graphically in Panel B of Figure B.1 and is one of the primary reasons cohesive shorelines generally feature narrow beaches and only limited sediment in the nearshore above the underlying cohesive substratum. In Panel B, the potential LST to the right of the headland is towards the east, while to the left of the headland it is to the west due to the different shoreline orientation. The headland, as previously explained, represents a divergent node with “zero” LST. The direction and magnitude of the arrows in panel A correspond to the vertical axis of the graph in Panel B. The curve of sediment supply represents to actual volume of material that is moved based on the sediment supply.

Figure B.4 provides a conceptual sketch of a cobble lag profile, which features a wide distinctive shelf and convex form. The convex profile is a variation of the classic concave cohesive profile seen in Figure B.2. Winnowing away of the fines, by wave action in the nearshore, and erosion of onshore bluffs with a high content of cobbles and boulders produces a cobble/boulder lag deposit in the nearshore. The lag deposit will act to retard the downcutting process with a resultant profile, which tends to be convex with a shelf developed in the nearshore. The shelf generally forms between the 2 m to 3 m water depth contour. A convex or shelf profile may feature a significant beach deposit at the shore, especially during average to low lake level periods when much of the wave energy is dissipated on the shelf.



**Figure B.4 Cohesive Shore with Lag Deposit**

### *Sandy shores*

The morphology and evolution of sandy coastlines is influenced by incident wave energy and lake levels and more recently by human alterations to the coastal environment. Besides the obvious differences in the geologic properties between sandy and cohesive shorelines, a major distinction is the potential for sandy shorelines to recover from erosion events. As discussed previously, erosion of cohesive shores is irreversible.

On the Great Lakes, short-term cross-shore profile response to lake level trends and severe storms are well documented (e.g., Hands, 1979; Nairn et al., 1997). During rising lake levels, a cross-shore profile response occurs as the mean water level increases, and sand from the beach and dune is transported in an offshore direction. Cross-shore transport occurs at cohesive and sandy shores.

However, over temporal scales covering years to decades, the supply of new material, the rate of sediment movement along the shore, and natural and artificial barriers to longshore transport are the fundamental processes that shape the morphology of a sandy coastline. In other words, gradients in longshore sediment transport are the critical factor determining whether a sandy shoreline will be in a state of erosion or accretion. The concept is demonstrated in Figure B.1 for the sandy shores in the littoral cell.

The updrift supply for the littoral cell in Panel A includes the eroding cohesive shores east of the bedrock headland. The dominant incident waves are from the southwest and result in a net longshore sediment transport direction to the east towards the harbor jetties (arrows in Panel A). In the eastern third of the littoral cell, the sediment supply exceeds the potential longshore sediment transport rate and a sandy shoreline develops, as demonstrated in Panel B. The harbor jetties also represent a partial/complete littoral barrier, which leads to additional sediment accumulation in the form of fillet beaches and shoals offshore of the coastal structures. The physical processes affecting sediment transport, and bypassing, and shoreline evolution in the vicinity of large coastal structures are complex and require detailed investigations to quantify short and long term trends.

Panel C in Figure B.1 summarizes the long-term shoreline trend of the conceptual littoral cell. At the bedrock headland, the shoreline is relatively stable with minimal shoreline erosion. The lag deposit of the convex profile slows the erosion but recession still occurs. At the concave cohesive profile, the amount of shoreline recession is the highest. At the barrier, where the sediment accumulates, shoreline accretion occurs.



## **Memo**

**To:** Brook McIlroy Inc.  
Ms Jessica Hawes

**Date:** November 13, 2006

**From:** Nick Poulos and Norman Chung

**RE:** Mississauga Waterfront Parks

**Project #:** 06.203

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Transportation, Traffic, Transit and Parking input has been prepared for the Mississauga Waterfront Parks. The parks include Fusion, Lakeside, JC Saddington, Marina and Memorial Park West

To assist in the preparation of transportation input the City of Mississauga Transportation and Works Department Waterfront Parks Parking Utilization Study dated March 2004 has been reviewed.

The following sections present the transportation input.

### **1. Introduction**

The City of Mississauga operates numerous waterfront parks between Winston Churchill Boulevard and the Credit River. The geographical setting dictates that Lakeshore Road either provides direct access to a park because the park is bounded by the road or the park is connected to Lakeshore Road via a local road connection. As a result Lakeshore Road is the primary facility that serves the parks and accommodates all available modes of transportation.

The Waterfront Master Plan clearly identifies the uniqueness of these parks and the fact that patron experience is enhanced by the relationship to the waterfront. In keeping with this spirit it is imperative that park space be kept opened and not unnecessarily be given over to the provision of off-street parking. In some instances the parks can only physically provide a nominal amount of off-street parking. Other parks which have the ability to provide off-street parking are recommended to have a parking supply sufficient to meet similar demand needs as observed in a recent park parking utilization study. It is important not to provide an over-supply of parking. Not only for the sake of the park and enjoyment of facilities but to help guide and increase the use of non-automobile modes of transportation.

The approach taken therefore to serve the transportation needs of the parks is one that is balanced allowing all primary modes of transportation to easily access the parks. The primary modes include automobile, transit walking and bicycling. Lakeshore Road being the primary access route that links all of the parks should evolve over time the capability to incorporate the spatial needs of these modes.

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## 2. Accessibility

The parks have a varying degree of accessibility to the primary modes of transportation. The following table summarizes the existing accessibility conditions.

**Table 1**

**Park Accessibility**

<b>Waterfront Park</b>	<b>Automobile Accessibility</b>	<b>Transit Accessibility</b>	<b>Pedestrian Accessibility</b>
Fusion Park	Direct Access available from Winston Churchill Boulevard and Lakeshore Road	Limited available transit service	Direct access from Lakeshore Road
Lakeside Park	Direct access from Lakeshore Road	Limited available transit service	Direct access from Lakeshore Road
JC Saddington Park	Direct access from Mississauga Road and three north south local roads connecting to Lakeshore Road	Regular scheduled transit service on Lakeshore Road approximately 300 to 400 meters north of the park	Direct access form the local roads connecting to Lakeshore Road
Marina and Memorial Park West	Direct access to Front Street which connects to Lakeshore Road	Regular scheduled transit service on Lakeshore Road	Direct access from Front Street

The facility which serves all modes of transportation and provides the primary ability to access all waterfront parks is Lakeshore Road. Only one park JC Saddington does not directly abut Lakeshore Road. It is however, within a comfortable walking distance from Lakeshore Road.

The transit service appears to be a function of the surrounding land uses. JC Saddington, Marina and Memorial Parks located in close proximity to residential and retail commercial stores along Lakeshore Road have justified a regular scheduled transit service. Fusion and Lakeside Park located in an industrial area has reduced transit services reflecting the peak hour needs of employment demands.

The primary movement of pedestrians is to and from Lakeshore Road. This is the case whether

## **Memo**

the park abuts Lakeshore Road or is located in close proximity to the park.

### **3. Parking**

The City of Mississauga Parking Utilization Study surveyed Jack Darling Richard's Memorial and Lakefront Promenade Park. The surveys were conducted during the days of the Civic Holiday and Labour Day long weekend, two regular weekends and five weekdays during the months of August and September 2003. The results from this comprehensive and well documented study indicate that:

- The parks are well used on Holidays and long weekends with the parking demand virtually equaling the available on site parking supply;
- During the weekday and average weekend the parking demand was between 50 to approximately 85 percent of the available on-site parking supply.

It is concluded from these utilization results that the on-site parking supply provided at these parks is sufficient to satisfactorily serve average and peak parking demands. The study did not indicate that increases to the parking supply are necessary.

This study determined the parking supply ratio (number of parking spaces per hectare) available at these parks and applied it to the Fusion and Lakeside Parks. This ratio based on existing demands shaped by regular transit service should be sufficient to meet peak parking demands. The parking supply shown on the master plans for these parks is considered sufficient.

It is further concluded that off-street parking should be limited at JC Saddington Park and Marina Park West. Patron accessibility to these parks and Memorial Park West should be primarily served by transit and walk modes of transportation. Marina and Memorial Parks should have the ability to permit vehicles to launch canoes and kayaks and facilitate special events involving regattas and other water related functions. These parks are not big enough in size to provide both the necessary facilities as well as a parking supply to meet peak parking demands. In many instances the peak parking demand is driven by special events which should necessitate a comprehensive parking strategy including:

- The use of remote parking areas and shuttle bus services;
- Greater reliance on existing public transit services;
- Implementation of new transit services linking the parks (JC Saddington, Marina and Memorial Park West) to the other parks and the retail commercial areas of Long Branch, Port Credit and the GO Rail Stations.

It is concluded that the parking supply illustrated on each of the draft concept plans master plans is sufficient to meet peak parking demands. Fusion and Lakeside Parks will have initially a greater reliance on the automobile to access the facilities whereas JC Saddington, Marina and Memorial West Parks will have to place a greater emphasis on transit and other specialized transit services to serve peak patron demands.

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## 4. Park Patron Accessibility

Each of the parks is reviewed below from an operations perspective to ensure that the key modes of transportation can be properly accommodated. The transit component is given predominance immediately adjacent to the parks. Transit and other modes such as bicycles and walking should be considered as part of an overall network or trail system that links the parks.

### Fusion Park

Fusion Park directly abuts Lakeshore Road. Vehicle access is directly available to the proposed on site parking supply. The park entrances are sufficiently spaced on Lakeshore Road such that vehicle turning movements do not adversely impact the primary Lakeshore Road traffic flows. As well park generated traffic is fairly low and occurs outside of the normal roadway weekday peak hours. As a result it is not anticipated that that park generated vehicle traffic will adversely impact background traffic flows. Depending upon growth in Lakeshore Road traffic consideration can be given to introducing westbound park left turn storage lanes. However this should not be done at the expense of widening the road.

The existing transit services and any enhanced transit service should not enter the park. Instead bus stops with bus bays should be strategically located on Lakeshore Road at each of the two ends of the park. It is recommended that pedestrian activated traffic signals should be considered at each of the bus stop location areas. This would permit the safe crossing of pedestrians and with associated traffic control signs including reduced speed limit calm through vehicle movements on Lakeshore Road.

The master plan should ensure that the internal circulation system is sufficiently designed to permit the adequate circulation and free-flow movement of all emergency and service vehicles.

### Lakeside Park

Lakeside Park directly abuts Lakeshore Road. Vehicle access is directly available to the proposed on site parking supply. The park entrances are sufficiently spaced on Lakeshore Road such that vehicle turning movements do not adversely impact the primary Lakeshore Road traffic flows. As well park generated traffic is fairly low and occurs outside of the normal roadway weekday peak hours. As a result it is not anticipated that that park generated vehicle traffic will adversely impact background traffic flows. Depending upon growth in Lakeshore Road traffic consideration can be given to introducing westbound park left turn storage lanes. However this should not be done at the expense of widening the road.

The existing transit services and any enhanced transit service should not enter the park. Instead a bus stop with bus bays should be located in the central entrance area of the park. It is recommended that a pedestrian activated traffic signal should be considered of the bus stop



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location area. This would permit the safe crossing of pedestrians and with associated traffic control signs including reduced speed limit calm through vehicle movements on Lakeshore Road.

The master plan should ensure that the internal circulation system is sufficiently designed to permit the adequate circulation and free-flow movement of all emergency and service vehicles.

The transit recommendation is consistent for both parks. Since both parks abut Lakeshore Road there is no patron advantage to turning the buses into and out of the park. Such a turning movement condition would add unnecessary travel time and costs to the transit service.

Park parking on Lakeshore Road has not been recommended. Although on street parking could provide a measure of traffic calming along the frontage of the park it is felt that the roadway can be calmed through the introduction of transit measures, pedestrian signal activated crossing and related traffic control devices. In stead of parking on Lakeshore Road consideration should be given to introducing bike lanes. Further depending upon transit and high occupancy vehicle demands consideration could be given to turning the curb lane into a combined bike and high occupancy vehicle lane.

### JC Saddlington Park

Mississauga Road South will provide the primary access route for automobile traffic. Since the park is the last use on this road background traffic will be very low and it is not anticipated that any special traffic control measures will be required.

Emergency and service vehicle should be able to freely access the small on-site buildings.

The primary emphasis will be to encourage pedestrian connections from Lakeshore Road to the park.

### Marina Park and Memorial Park West

The primary mode of access for Marina Park and Memorial Park West should be walking and / or bicycling. As a result the connections from Lakeshore Road including the connection beneath Lakeshore Road are of primary importance. Consideration should be given to directly integrate this pedestrian connection system with strategic bus stops on Lakeshore Road. This could include the introduction of new traffic signals to integrate the bus stop with the pedestrian connection system.

The limited parking in Memorial Park West is satisfactorily accessed via Front Street North. Similarly Front Street North provides access to the internal loop road of Marina Park. This loop

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road should be considered a multipurpose access route designed to provide emergency access and service capability to the Village Market Place. It should not be considered as an operational facility when events or activities are occurring within the park or park is active for typical daily activities.

Discussion has occurred about introducing a below grade parking facility in Memorial Park West. Although a detailed impact assessment has not been completed through this study it is evident that much more analysis has to be done to prove the feasibility of such a consideration. As an example there are several operating considerations that may prevent the feasibility consideration including:

- Front Street North may likely not have sufficient reserve capacity to accommodate the increased traffic flows associated with the number of parking spaces and the daily turnover of parking spaces;
- The intersection of Front Street North and Lakeshore Road may require upgrading including the installation of traffic signals. Such signals would be fully integrated into the Lakeshore flow operation as opposed to traffic signals activated by pedestrians or transit users;
- Careful assessment must be made with respect to topographical and soils conditions to acknowledge the proximity of the Credit River and the potential need to provide flood prevention measures.

### **5. Internal Roads and Parking Layout**

The “greening” of parking lots can be considered. Designs which are environmentally sensitive should be considered. The parking demand spread out during the summer months with low turnover rates is conducive to designs that incorporate innovative drainage and surface conditions. This would include parking aisles. The main circulation roads for Fusion and Lakeside Parks however, should have traditional surface pavement treatments structurally supporting emergency and service vehicle loads.

The loop road within Marina Park should be of sufficient weight bearing capability to support service and emergency vehicles. Consideration however, should be given to using non-asphalt surface materials to denote that this loop is not an active road to circulate within the park area.

### **6. Transit Opportunity**

It is recognized that transit must provide a role to serve patron demand usage of these parks. In the immediate term JC Saddington, Marina and Memorial Parks must rely on transit to serve as many demand trips as possible. In the medium to long term time frames transit can begin to play an important role serving the travel demand needs of Fusion and Lakeside Parks.

In the immediate term regularly scheduled transit services properly promoted could attract more and more people away from automobiles usage.

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Since it is readily understood that JC Saddington, Marina and Memorial Parks cannot provide adequate off street parking especially when planned events are held consideration should be given to implementing a new transit service. This new transit service operating in the summer months during specific hours could be an overlay of the Lakeshore Road services that links to each of the GO Rail stations and passes through the retail commercial areas of Long Branch and Port Credit. The service would serve typical transit demands as well as be promoted for park access and retail shopping purposes.

### **7. Bicycles**

Bicycles could be an attractive way to access and link the parks. It would be appropriate to review the City's trail and bike network to determine where appropriate connections can be made. Connections should be made to Lakeshore Road since it is the primary facility which links all of the parks.

A study may have to be initiated to investigate the feasibility of Lakeshore Road to incorporate bike lanes within its right-of-way. The study could also investigate existing and future travel demands within Lakeshore Road to determine any opportunity to rationalize existing traffic and parking lanes. The rationalization could also determine the need for a combined high occupancy vehicle and bike lane. Such a study could cost in the order of \$50,000 to \$60,000.

### **8. Other Potential Studies**

The transportation work completed for these parks indicates that other studies would be of importance to support not only the park objectives but the overall transportation plan for the municipality. Specifically these studies include:

- The above mentioned combined high occupancy vehicle and bike lane study;
- An operational study to determine the operating costs and benefits of introducing a new Lakeshore transit route. The study could investigate benefits to businesses and solicit their participation in offsetting costs. Such a study is estimated to cost in the order of \$30,000;
- A functional design study of Fusion and Lakeside Parks to finalize the bus stop locations, pedestrian crossings and traffic signal design plans. It is estimate that this work could cost in the order of \$30, 000 to \$40,000;
- A functional design study to determine the location of bus stops, bus bays, possible traffic signal installation and other road/transit features to support and encourage related pedestrian and transit activities at Marina and Memorial Parks. It is estimated that such a study could cost in the order of \$30,000;
- An information and signage study could be initiated. This study would identify the information and directional signage to identify the parks along Lakeshore Road. This signage plan could also tie into the transit supportive initiatives, bike trail and lane network.

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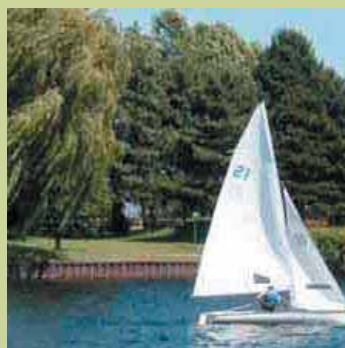
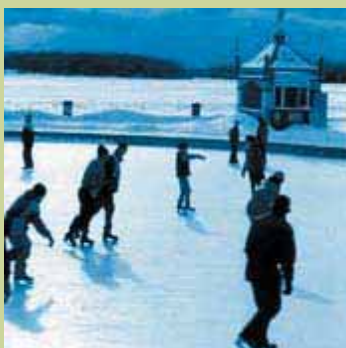
It is difficult to estimate the costs for the information and signage study initiative. The cost could range from between \$20,000 to \$30,000, depending upon the area to be covered and the extent of signage tie-ins. The other costs are given as preliminary estimates only and do not reflect field or operating data inputs.



# **Memo**

## PUBLIC CONSULTATION SUMMARY

# WHAT WE LEARNED FROM YOU



Prepared By:

**Brook McIlroy Inc. / Pace Architects**

in conjunction with:

Poulos and Chung Ltd.

Terraprobe

Baird and Associates

Philips Engineering Ltd.



## Public Consultation Summary

# Introduction

On February 21 and 23, 2006 The City of Mississauga, in conjunction with the Consulting Team of Brook McIlroy Inc. / Pace Architects *Urban Design and Landscape Architecture*, Baird and Associates *Coastal Engineers*, Poulos & Chung *Transportation Engineers*, Terraprobe Engineering *Soils Experts*, Philips Engineering *Infrastructure*, hosted a public open house and workshop to seek additional public input in regards to current use of parks and kick off phase 1 on the waterfront parks strategy.

## Who came to the public consultation sessions and workshops?

Poster, flyers and portable signs were distributed throughout the City and media advisory to invite anyone who was interested in participating in the open house. The following is a brief summary of who attended:

Residents from the park neighbourhoods  
Residents from other communities in the City  
Business owners  
Media  
Special interest groups: Environment, Leash Free, Trails etc.



## What was presented?

A series of boards, a slide presentation and workshop questionnaires were presented at the meetings to provide background information about the waterfront park strategy and garner participation from the public.







## What was the outcome?

The workshops began by asking participants to collect into discussion groups and to complete 'post-it' note exercises, which were designed to determine each participant's three most important uses or things that make a great waterfront park system and subsequently, what makes a great waterfront park. The following is a summary of the Post-it note exercises.

## February 21, 2006 Workshop

### Top Uses or Things for a Great Waterfront Park System:

1. Safe connectivity (15)
2. A balance of programming: retail and cultural events (12)
3. Accessibility to all: transit, barrier free and all seasons (10)
4. Access to the lake: views, fishing, boating swimming (10)
5. Environmental protection: wildlife friendly, education (9)
6. Cleaner parks: dogs, geese, water quality (7)
7. Improved trail conditions: separate bike and pedestrian trails (7)
8. Washrooms and comfort facilities (5)
9. All season access (3)
10. Trails through green space not a road (2)
11. Play facilities for kids (6)
12. Improved picnic areas
13. Special elements: water feature, boardwalk, splash pad, beaches, beach volleyball, meeting place, workout place (1 ea.)
14. Traditional to specific design and by land to link parks (1)
15. No high density or retail (1)
16. Financial commitment (1)





### **Top Uses or Things for a Great Waterfront Park:**

1. Landscaping: green all season, unique, natural, educational (14)
2. Access to the water: views, swimming, boating, water sports and kites, free access (14)
3. Balanced programming: leash free, festival space, restaurants and patios, sunbathing, trails, no sports fields (11)
4. Environment: trees, clean water, wildlife and unique (11)
5. Trails: well-organized, marked, separate for bikes and walking (7)
6. Safety and lighting: accessible to all (4)
7. Children's facilities (4)
8. Maintenance: garbage and cleanliness, geese (4)
9. All season appeal: skating (3)
10. Picnic areas (1)
11. Trail exercise stations (1)



### **Individual Group Comments:**

#### Group 1:

- Parks should be unique first with common elements like universal signage, posted maps, monitored leash free areas
- There should be more BBQ pits, picnic tables, landscaping, boat mooring opportunities, trails and trail connections, beach volleyball
- Improve maintenance, empty garbage bins more often
- JC Saddington – splash pad, new marina, restaurants, tennis courts, more trees, landscaping, lighting, all season uses, garbage cans and maintenance, ped. Bridge to connect to east boardwalk, create a loop, docking, boat launch with trailer parking, freeze pond and maintain rink, bike parking
- Small concert pit or amphitheatre, more programming for music
- Port Credit Memorial West – boat rental, canoes, benches, lighting
- Marina Park – boardwalk, trees connect to Memorial on east side, docking, boat launch, passive recreation
- Lakeside Park – create as destination park, have BBQ's, recreational areas, tennis, comfort stations and refreshments
- Fusion Park – passive recreation



#### Group 2:

"It's not what makes a great waterfront park system. It's not what makes a great waterfront park. It's who pays for this and how much are we, the residents of Port Credit, going to bear the consequences?"



#### Group 3

- Washrooms should be priority – need more, open for longer hours, cleaner, more frequent maintenance, keep open in the winter
- Restore both sides of the Credit River
- Parking is always an issue – should also be free
- Parks close too early

Group 4:

- Connectivity, use transportation hubs
- Mixed use and theme parks from natural to urban designed to facilitate a higher exchange of activity
- Landmark elements
- Activities for all ages
- Clean and safe washroom facilities, lighting at night
- More beachscapes + better maintenance to preserve
- Cycling right of way
- Maintenance and washrooms
- Seasonal opportunities
- Introduce public art program
- Music, festivals, organized activities
- Housing – higher density to sustain transportation system



Group 5:

- Park linkages, better connectivity between parks
- Cleared paths in the winter months
- Parks should be unique, diverse with some common elements like signs, maps, safety kiosks, parking lots
- Linkages through environmental education, enhance cultural and historical connections to park, neighbourhood provide educational opportunities
- Partnership with private development to provide canoe and bike rentals, one-way shuttles, coffee wagons, healthy food choices
- More variety for different sized events, different parks for different venues
- Clean water and water's edge, control the geese
- Protect the environment, approve only sustainable development, educate
- Improve transportation to parks
- By-law enforcement more involved



**Summary of Questionnaires (based on 17 questionnaires collected at the workshop):**

***Question: Do the parks need a unified vision or should each park be distinct and unique, or can they be both?***

Most participants thought that the parks should be distinct and unique but that they could contain common elements like signage, safety kiosks, parking lots and maps. Parks should be seamlessly tied together but allow for variability. Aspects to be unique: watersedge, children's facilities, vegetation, marinas, historic and cultural learning.

***Question: How do you use the waterfront park system today, what would enhance your park experience?***

Participants use the waterfront park system to cycle, walk, run, ice skate, enjoy nature and plants, walk dog, roller blade, picnic and BBQ, run businesses (charter boats). Full waterfront access, controlling the geese, improving cleanliness, separate bike/pedestrian trails, boardwalk, clearing trails in the winter and groomed trails for x-country skiing would enhance the park experience.

***Question: Which park is your favourite and what would make it more comfortable and beautiful?***

Lakefront Promenade, Arsenal, Rhododendron Park, Ben Machree were some of the favourite parks listed. Improved maintenance, better sand beaches, more picnic areas, concessions, lifeguards, more trails and beach volleyball would make them more comfortable and beautiful.

***Question: What opportunities do you see for the priority parks, Port Credit Memorial (west side of Credit River), Marina Park, J.C. Saddington Park, Lakeside Park and Not Yet Named Park (Fusion)?***

In general more community events, especially during the summer and around the Christmas holidays. People would like to see more gardens, seasonal displays, festivals, children's imagination/educational play areas, rock walls, a bandshell, naturalized areas and soccer fields

- Marina Park: Fishing, clean up area, link to Saddington Park, parking will be a challenge, boardwalk, trees and landscaping, connections to memorial docking, upgrade boat launch, passive recreation, new charter docks, art show, concerts, vendors, buskers
- Memorial Park: Narrow, small, opportunity to naturalize, provide some fishing areas
- Lakeside Park: Dirty air from industry, provide access to lake and boating - conflict with small craft boating and industrial docking uses? BBQ areas, tennis courts, comfort station and refreshments, buffer from industrial, leash free area
- Fusion Park: Boat launching, windsurfing and small boat sailing, improve public access, passive recreation, keep natural, buffer from industrial areas
- JC Saddington: All season use, generally well used but look dated, needs to be 'spruced' up, add tennis, splash pad, bike racks, skating, marina with launch facility and trailer parking, pedestrian bridge to east boardwalk, improve lighting and maintenance, beach

## February 23, 2006 Workshop

### Top Uses or Things for a Great Waterfront Park System:

1. Trails: for biking, rollerblading, walking; way-finding (signage); linkages and north-south connections (28)
2. Aesthetics: history and culture; unique identity; green and natural; improved maintenance (10)
3. Access to Lake: lake views; boating and swimming (9)
4. Environment: clean and healthy; control geese and dogs (8)
5. Natural Areas: wildlife and vegetation; heritage; passive (7)
6. Programming: seating areas; all season and all weather; balance of passive and active; outdoor arts; picnic areas; cultural heritage (7)
7. Children's Play: wave pool and play grounds (7)
8. Accessibility and Safety: sep. trail users; universal (6)
9. All Season Activities: skating – natural and artificial; skiing and tobogganing (5)
10. Parking: sep. from trail and water; over-use (2)
11. Access to Park: all forms of transportation (1)

#### Specific Elements:

Band shell (5)  
Ice cream (1)  
Boardwalk (3)  
Sports Facilities: volleyball and baseball (2)  
Party/BBQ (1)  
Lawn Bowling (1)  
Rentals: bikes and canoes (1)  
Restaurant (1)

**Note: In the interest of time and efficiency, the post it note exercise was not repeated for the second part of the questionnaire.**

### Individual Group Comments:

#### Group 1:

- Jack Darling., Arsenal, Lakefront Promenade Parks, right size to become a node
- Far west park required
- Water quality – boat pollution...?
- Dog Runs/Parks....where is there place...down to waters edge?
- Missing linkage between Jack Darling and Rattray Marsh
- Trail linkages: signage especially outside parks, more signs on paved streets – way-finding within parks and between parks
- Large map sign with legend – in certain parks, to list what facilities are where
- Better maintenance especially west of Credit River, north of Lakeshore
- Marina Park – no need for parking lot
- Saddington – no change of use, just “freshen’ up” existing
- Fusion and Lakeside – parking needed





Group 2:

- Most transportation to parks is by car
- Public Transportation needs north-south connections
- Restaurants allow all-season use and safe and secure night time use
- Public-private relationships
- Dogs are neat and fun to watch and engage with, consider separate areas
- Signage – keep unique parks but universal way-finding (km)
- Rentals – bikes, skates, canoes, rollerblades so people can visit from distances without having to carry equip. with them
- Fusion – cultural events and variety of service/restaurants: not exclusive
- Need places to rent canoes, kayaks, bikes, rollerblades, ice skates etc.
- Bike Lanes on Lakeshore Road because it is wide enough...make it the unifying link/access point for many of the parks



Group 3:

- Linkage between parks – add more paths and signs
- Cleanliness and washrooms – over-use and summer line-ups especially at Jack Darling Park
- Keep parks unique and design for specific use
- J.C. Saddington: pool, squash, tennis courts needed by community
- Memorial Park needs maintenance
- Jack Darling and Marina Park needs a walkway to link (JJ Plaus to Marina needs pedestrian link)
- “Pedestrian Only” bridge over Credit 1k north of Lakeshore
- Richards Memorial Park– approved pavilion for Trans Canada Trail



Group 4:

- Unified Signage to indicate history of park and community/neighbourhood
- Connect parks under banner of Mississauga
- Cleanliness
- Preserve natural areas
- Heritage buildings need to be preserved and kept
- Safety awareness through design
- Commercial possibilities: tea house, docks for fishing
- Already have 2 “leash free” in south side, maybe add to parks in north?
- Memorial Park West-side - watching the river – natural seating/berm framed with trees to watch rowing club activities



Suggestions from a resident:

A waterfront trail should be established along the southern strip of Petro-Canada's property, connecting a narrow strip from Lakeside Park “leash free” to Meadowwood Park, then linked to Rattray Marsh via city streets.



## **Summary of Questionnaires:**

### ***Question: Do the parks need a unified vision or should each park be distinct and unique, or can they be both?***

Each park should have a unique experience that contributes to the overall waterfront trail theme/visions –Water. Parks should be well planned to incorporate all facilities.

### ***Question: How do you use the waterfront park system today, what would enhance your park experience?***

Participants use the park system for walking, running, rollerblading, playgrounds and splash pads, biking, sitting and picnics, bird watching, dog walking, enjoying nature, solace.  
Rental opportunities for watercraft, safe night time uses, restaurants, safe bike travel on Lakeshore from Oakville to Etobicoke, good concession stands with water, ice cream, Gatorade, dinner cruises, km markings on trail system, better maintenance, accessible trails and washroom facilities, open air concert area, better wayfinding with signs and pavement markings would enhance the park experience.

### ***Question: Which park is your favourite and what would make it more comfortable and beautiful?***

Lakefront Promenade, Jack Darling Park, St. Lawrence Park, Rattray Marsh was listed as one of the more favourite parks.  
It should be left alone.

### ***Question: What opportunities do you see for the priority parks, Port Credit Memorial (west side of Credit River), Marina Park, J.C. Saddington Park, Lakeside Park and Not Yet Named Park (Fusion)?***

Public and private partnerships, boat cruises, exercise equipment rentals, bandshell area, Horseback riding?, cafes and restaurants, integrated water experiences on beach

- Fusion Park: Gateway Park, Wellness centre, outdoor 'Guild Inn' – display salvaged building materials from historic buildings – purchase farm on north side of Lakeshore and operate as working farm like Springdale in Milton, design of park should be completed by public competition.
- Lakeside Park: Paths through forest area on east side, paved trail from parking area to watersedge and trail, needs icon – 'Giant Salmon'
- Memorial Park: Pedestrian bridge joining Memorial Park to railway bridge, canoe rentals, walkway along water linking to JC Saddington and Marina
- Marina Park: Picnic tables, no parking, drop off areas for families and picnic supplies
- JC Saddington: Market Square, sports facility, cultural centre, special events venue,

## **How will this feedback guide the Waterfront Parks Strategy?**

**It has been used to determine the Key Guiding Directions outlined on the on the following pages.**

## Key Guiding Directions – Based on Consultation Input

### 1. Connecting the Parks

#### **North/South connections:**

- Rerouting bus routes to engage parks
- Propose waterfront shuttle services during peak months
- Improve signage from Lakeshore Road,
- Lakeshore/NS linking road intersection as gateway to park, provide signage, built element and planting to identify
- NS streets between Lakeshore and lake to be 'green' links to waterfront, make recommendations
- Improve bicycle route connections north south, dedicated lanes
- NS connection between Memorial West to Marina Park

#### **East/West connections**

- Lakeshore as overall linking east west corridor, recommend streetscape treatment
- Dedicated bike lane on Lakeshore
- Signage/gateways at north south streets leading to the waterfront, recommend treatment
- Propose east west shuttle system during peak months
- Waterfront trail east west connections, unify trail typologies to support cohesive, recognizable trail system
- Missing link trail connections – citizen's proposed new waterfront link on Petro Canada lands between Rattray Marsh and Lakeside Park
- Treatment of street linkages between parks, make recommendations, propose land acquisitions parks

#### **Park entry and edges**

- Treatment of north and south side of Lakeshore where parks meet blvd
- Create standard guidelines for sequence of entry, for people in all modes of travel
- Create welcoming, accommodating and safe entry points, identify places for rental equipment, provide adequate lighting, comfort facilities, directional mapping and information, introduce park identity
- Emergency service access, no duplication of surfaces for this use

#### **Access to the lake, its all about the water**

- Major EW connection to be explored
- All parks to access lake physically or visually, create view corridors, look-out piers, boardwalks, ramps, beaches
- Opportunities for motorized and non-motorized access to shore
- Provision for variety of water related activities, provide rental facilities for non-motorized water craft

#### **Signage and Wayfinding**

- Can be the common thread linking all of the unique parks
- Signage graphics and installation design to be distinct and recognizable, eg. sign posts become markers
- Use pavement markings Incorporate distance measures from EW and WE, include information about distances to parks
- Can include commemorative aspect – disks in pavement to identify waterfront trail

## 2. Ensuring Unique Park Identity

### **Cultural heritage interpretation and education**

- Combine with park entry kiosk, and entry feature
- Provide interactive ways of displaying the park story, graphic timelines, public art, artefact display

### **Park aesthetics**

- Distil the character of a park, passive vs. active, vegetated vs. open, urban vs. bucolic
- Enhance and promote its unique qualities through landscaping and park built elements such as shelters, nodes, play areas
- Standardization of site furnishings such as benches, trash cans etc.? Some unique elements

### **Park Location**

- Identify park location in grand scheme of system
- Look at park significance in terms of natural systems, regional significance – physical location and function in the waterfront landscape
- Location can begin to support the overall theme of waterfront, e.g. east-west references evolving into dawn-dusk, beginning-end, upstream-downstream theme.

### **Park program and facilities**

- Facilities are to be appropriate for the identity of each park e.g. a bandshell in a neighbourhood park is not a appropriate facility for the park
- Facilities need to be supported by park size and layout, needs to be a good physical fit

*Thank you for participating...*



## **Flip Sheet Opinions**

### **June 2006**

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#### **Guiding Principles**

- Trail-use separation – design for future transportation needs
- Trail conflict
- Avoid having to drive in order to exercise in parks – multi-modal access with soft surfacing for joggers
- Accessibility to the Lake shoreline
- Connect the parks to the rest of the City
- Parks are a regional destination
- Should be a continuous park system along the water's edge
- Preservation and celebration of our history
- Highlight community compatibility and context
- community compatibility
- Balance community needs with heritage
- Be responsive to immediate neighbourhood identity
- Public education – stewardship
- Restaurant destination on the waterfront
- Funding time frame
- Financial support
- Art as a principle
- Clean up goose droppings
- Ecological wildlife management
- Wildlife management
- Water quality and safety – algae problem
- Canopy cover and wind protection can contribute to a place being vibrant and year round
- First priority is the water's edge
- Balance

#### **Land Securement**

- Establish a Ranking System
- Organized and Unorganized is equal
- If the opportunity to buy land arises, the City should pursue acquiring it
- Waterfront parks are for passive uses not sports
- Imaginative play areas, not just structures
- Recreational activities include tennis to expand as priority
- Windows on the Lake – like Tall Oaks

- Tree planting programs on Port Street, north of Marina will serve to connect the waterfront trail – this streetscape is barren and foreboding as it is now
- Est. lights for a pedestrian crossing across Lakeshore Road
- Trail Contiguity
- Educational and school uses

### **J.C. Saddington Park**

- Separate parking from activities
- Never enough parking
- Parking garage
- Not enough parking available on Front Street
- Gravel parking lots preferred more than paving stones
- Village Green will decrease overall parking
- Moving parking is a good idea
- Metered parking on Lake Street
- Parking issues with restaurant
- Parking at Marina and interim at Imperial lands/underground parking
- Parking enforcement and education/information programs
- Limited parking forces on-street parking on residential street – conflicts, especially on Front St. and Bay St.
- Screen parking on street edge from on-looking residences
- Shuttle bus service
- Picnic facilities require drop off loops
- Trail link to Marina Park
- Boat access to the restaurant
- Public boat launching
- Locate trailer parking for boat launch (ESSO lands?)
- Locate boat launch facilities
- Better use for the Village Green – formal gardens?
- Maintain pond
- Manage pond area to keep wildlife out
- View on the Lake
- No buildings south of Port – have buildings to create a space of attraction
- Aviary
- Wildlife interpretation opportunities
- School children frequent this park
- Sheltered picnic area
- Access to shoreline
- Garbage and maintenance
- Litter from restaurant

- Wildlife naturalization
- Pond algae problem needs to be addressed before activities are encouraged
- Natural skating pond
- Teahouse/Restaurant okay, not fast food
- Informal restaurant in the European style
- No restaurant
- Restaurant will compete with ex. downtown business
- Restaurant on the water's edge
- Refuge from urban lifestyle – commercial does not belong in the park
- Location of facilities adjacent the pond of year-round washrooms
- Replace armour-stone with cobble beach access
- Soft landscaping – no metal pavilion
- Amphitheatre as a quiet gathering place

### **Marina Park**

- Need to address potential conflict on trail along waters' edge
- Traffic and Parking issues
- Limit parking – Tiered system
- Underground parking is too costly
- Keep boat launch
- Public boat launch
- Marketplace/pavilion type structure limited to 1-level
- If Library is to be re-located through the FRAM development then perhaps the west side of the River in Marina Park will be a suitable site
- Double-sided buildings
- Buildings north of Port Street
- Riverfront views blocked by structures/construction
- Existing vacant commercial space in Port Credit so why add more buildings
- Farmers' Market doesn't require a building, just designated area
- Farmers' Market could take place in the proposed Village Green
- Heritage priority – no commercial
- Soft landscaping – plantings and grass
- Residents' neighbourhood park

### **Memorial Park**

- Public access is important and constructing buildings at this site doesn't support this
- Layout improvements in concept make parking less of a highlight
- Canoe and kayak office

- More fishing

## **Lakeside Park**

- Leash Free needs to be mowed more than once a month
- Do not decrease size of Leash Free
- Playground located adjacent to Leash Free is desirable and makes Leash Free family-oriented
- Leash Free needs water access
- Water quality safe for dogs
- If Leash Free area is restricted people will begin using other areas like Rattray Marsh
- Tree planting in Leash Free
- Tap for dog drinking
- Ex. location of Leash Free provides view to the rest of the park and is good for safety
- Keep Leash Free large
- Leash Free conflicts with other potential uses e.g. special events
- lots of commercial traffic
- Tractor trailers are backing into the regional facility
- Need to have a vehicle to access this outlying park
- Address safety of traffic and trucks
- Shuttle bus – address parking issue
- Speed limit and noise from the roadway
- Provide adequate parking facilities for large special events – present reality
- What is a Cultural Meadow? – regeneration of an ex. open field with native species
- Transit shuttle – train like at CNE
- Gathering for festivals appropriate here due to distance from surrounding residents' homes
- Have a permanent stage
- Advertise and promote the use of Lakeside at Fusion
- Industrial odours conflict with picnicking
- Noise and Industrial odours are buffered
- Odour issue
- Wind abatement and micro-climate
- Window on the Lake
- Pier opportunity for fishing
- Windbreaks around splash pad
- Vegetated parking lots preferred over asphalt
-



- Limit paving and paved paths – possibility of this Park being used as a test site for alternative softer surfacing materials
- Hold this area as a land bank for when surrounding area is ready for a park
- Make water's edge attractive
- Kinetic art (wind-water)
- Lit pathways

### **Fusion**

- Wedding ceremony/reception and photo location
- Permit building out for banquets and fund raising
- Restaurant/Catering
- Public-Private partnership
- Restaurant in manor house
- Office and Public use of the building
- Winters are not cold any longer-ice rink
- Amphitheatre is not cost effective/not programmed for
- Formal English garden/Floral garden
- Partnership in maintenance with Cloverleaf Garden Club
- Maintain the grounds naturally
- Keep noisy activities like picnicking at Lakeside
- Conflict between picnickers and wildlife
- Link Fusion to Lakeside – family train
- How do I visit the site without having to drive there?
- Way-finding
- Visitors are car-dependent – too far
- Industrial parks busy with visits from employees
- Boat launch? – or on west side of the park system
- Access to shoreline
- See the water
- Universal access
- Views from Lakeshore into the site
- Naturalizing shoreline – erosion is a natural episode and if erosion is slow – allow nature to take its course
- Preserve woodlot intact like Rattray
- Preserve natural areas
- Keep Fusion remote
- Pond algae concerns – meadow habitat more appropriate?
- Impact of development on deer habitat
- Joshua Creek watershed education
- Educational instalments need to be vandal proof – require on-going upkeep, cleaning and replacement must be budgeted for

- Advertise the park
- Park signature - promotion
- Program this park
- Make this park unique from other beaches
- Playground is needed
- Early morning use
- Cycling access – trail
- Fitness loop
- No concrete trails – use mulch and stone dust – environmentally sensitive
- Encourage year-round activity
- Petting Zoo

## ***Public Consultation Summary***

### **Introduction**

On November 29<sup>th</sup>, 2007 The City of Mississauga, in conjunction with the Consulting Team of Brook McIlroy Inc. / Pace Architects *Urban Design and Landscape Architecture*, Baird and Associates *Coastal Engineers*, Poulos & Chung *Transportation Engineers*, hosted a public open house to seek additional public input on the Waterfront Parks Strategy. Key elements for consideration at the meeting were the Overall Park Strategy, Guiding Principles, Park Implementation, and Priority Park Master Plans for Park 387 (Fusion), Lakeside Park, J.C. Saddington Park, Marina Park and Port Credit Memorial West Park.

### **Public Consultation Attendees**

The following is a brief summary of who attended:

- Residents from the park neighbourhoods
- Residents from other communities in the City
- Business owners
- Media
- Special interest groups: Environment, Leash Free, Trails, Real Estate etc.
- City Staff

### **Afternoon and Evening Event**

Held at the Port Credit Public Library at both an afternoon and evening session, a series of boards, a slide presentation and questionnaires were presented to provide information about the Waterfront Parks Strategy and the Priority Park Conceptual Master Plans. The draft final report was made available on-line prior to the open house for review and consideration.

The following pages provide a summary of the feedback collected during and after the public open house. Comments were collected in person at the event, by fax, by email and through the submission of the questionnaire. The deadline for comments was December 17<sup>th</sup>, 2007.

**To obtain up-to-date information regarding the Mississauga Waterfront Parks Strategy please visit the City's Website:**

[www.mississauga.ca/parks](http://www.mississauga.ca/parks)

## MARINA PARK

- From a macro level it's a great master plan, however, my concerns are many i.e., heavy summer use will put a big demand on parking, I'm not sure the commercial applications along Marina Park will be successful, it appears that the loss of the boat launching facilities in Marina Park can be replaced conveniently and the ultimate access and enjoyment of the water quality is not significantly addressed.
- West side Lakeshore Road, Front Street North you will need a stop light or another under the bridge walkway
- Trail links (especially under bridge). It is very difficult to cross the road there at the moment
- Farmers Market
- Like the idea of restaurants and shops
- Wonderful, especially Marina Park, but once again where will everyone park?
- Heritage Conservation - Native Mississaugas - Credit River
- I am strongly opposed to having buildings within this site and the proposed one way vehicle loop makes absolutely no sense to me.
- I'll like to see a nice boardwalk along the line in Front Street like the way it is in Toronto
- The area of "Hacienda Bay" is too far close to the mouth of the river and in my opinion would be unsafe for canoe access. The river gets extremely choppy anywhere south of the current launch.
- In your proposal you are getting rid of the boat launch ramps at Marina Park. This is taking away the small boaters' access to the Lake. The only other boat launch ramps are over past Cawthra at Promenade Park.
- Big concern with the Marina Park plan is the proposed one way road from Port Street around the proposed shopping area and ending at the bottom of Bay Street.
- We are now a designated heritage district what is the effect on our "Village by the Lake" designation.
- Would like to see the boat ramps remain even if scaled down. Boating/fisherman bring charm & character to 'Port Credit'.
- There is nothing in your proposal to create a new launch ramp/parking facility either at Marina Park or an alternate park. What is going to happen is people will be going to Oakville to spend their money, they have 3 launch facilities.
- Concerns about the proposal for Marina Park. I suspect that the plan for these parks is to turn them into designation parks. As such, I believe it is not only unworkable, but it's undesirable, for residential areas. Proposal not only takes away those lots from Marina Park but will encourage more traffic into this residential area.
- Buildings erected in Marina Park will ruin the serenity and the street-escape of the mouth of the Credit River.
- Like to see more green space and a nice wide board walk, and not more bricks, mortar and asphalt. The charter boats and the public boat launch are an integral part of Marina Park. They need to stay. Strongly opposed to having buildings within this site and the proposed one way vehicle loop makes absolutely no sense to me.
- Marina Park is not a large parcel of land so let's keep it void of the congestion of buildings and roads (one way loop)
- Buildings erected in Marina Park will ruin the serenity and the streetscape of the mouth of the Credit River. The view from Front St. will be typically what one would expect to see when one looks at the rear of buildings - trash bins. The charter boats and the public boat launch are an integral part of Marina Park, they need to stay.
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- Strongly object to buildings, structures of any type – no tents or pavilions
- No shops/stores – the existing shops in Port Credit struggle for business – let's support other shops and encourage green space, park, etc.
- Marina Park (south of Lakeshore) – concern that Charter Boat businesses will have "slips" but where do their clients park? In front of homes on Port and Bay streets? Permanent structures (market place) for what will be only seasonally operated.
- Marina Park buildings will limit access and bring more vehicles to the area – we have enough places to shop as it is – why do we need more – additional taxes! I think so. More cars, more traffic, less parking not good
- Marina parks removed all the parking? Added more retail? We have more than enough
- I am concerned about the fishing area at Marina Park. With moving the fishing area close to the charter boats you might have a problem with the boats being caught up in the fishing lines.
- The buildings at Front Street South (east side) have businesses on Lakeshore that depend on summer (spring & fall) trade generated by the people using the trail. Why not put up structures, that provide shelter and seating; umbrellas etc. require high maintenance.
- Very sterile. Remember we have winter here 6 months of the year. Seating – as I age, I need a back to my chair.



- No through way roads, No buildings, make all park with lots of trees.
- No structures of anything.
- If you can't park, you won't come, if you don't come, no money
- Where do we park! Why more retail, we are having a hard enough time keeping stores full.
- Under bridge connections good. Please do it.
- No access for launching personal craft, canoe/kayak
- People require parking
- No parking
- Control types of shops, commercial development. Port Credit is full of tattoo parlours and spas
- Less buildings; have more greenery; need more plants and less of paved areas; don't waste so much space; need parking lot instead of demonstration garden.
- Love the trees; I don't want to see any building; there is no need for parking; I only want to see green space; no need for a road
- Needs an Interim Plan (Marina Park)
- Must have parking
- Need parking allowed on streets as well, they are wide enough.
- Fish cleaning station; fish disposed. No services for charter operator.
- Additional vehicles in this area will create gridlock on weekends. Parking will impact local residents.
- Why bring vehicles down here. Pedestrian only
- Like the village market Place
- No buildings; more green space; Keep boat ramp; less traffic
- Absolutely no buildings; Green space only and maintain view
- Where do I launch my boat?
- Need parking! no commercial but perhaps a farmers market; need trees
- No Shops
- Where are umbrella patios? Seating to eat, eateries, booths, carts; Public dining areas where people can bring their lunch
- Coffee places/ ice cream
- Soft edge to river bank needed to absorb boat wake
- Marina & Saddington are embedded parks inside a residential area! They must be considered differently with the residents concerns; parking issues; no shops off Lakeshore; support Salmon Fisheries; Open Vistas for air; No Road Lane
- Keep boats
- Offering charter boat businesses without having on site (not street) parking for unloading customer's car is showing no consideration for neighbours on Port & Bay Streets.
- Low bushes parking issues; No buildings; more park & walk ways; Keep charters; Boat ramps remain.
- Village market place will be seasonal. What use are permanent structures that will only be viable 4-5 months, a year.
- No building, more garden space
- No parking unloading area for charter business
- This should be for residents not tourists
- Be sure who you are building for tourists/residents or new condo dwellers
- I like the creation of a park here...extension of path around building to JC Saddington Park, opportunity for a farmers market, fish monger, and connection under the bridge to north park.
- Like to see added: ensure shade trees are added for comfort and environmental reasons and narrowing of Front St S – current traffic is too fast – not sure if this is part of mandate
- Generally good. The reasons for two "Village Market Places" at Marina Park are not evident to me – these would be out of place and not harmonial with adjacent residential uses. (Also – very close to the existing commercial district along lakeshore)
- People friendly, Marina Park removing gravel parking for village market, pedestrian path facing waterfront.
- We **strongly support** the recommendations that maintain the connection with the water, and harmonize the park with the neighbouring Heritage District, namely: stabilization of the shoreline; walkway connections under the Credit River Bridge north to Memorial Park West, and along the water's edge south to Saddington Park; enhanced sightlines along Port St. W. and Bay St. to the Credit River, with primarily pedestrian use; improved streetscape along Front St. S.; eco-sensitive landscaping and encouragement of tree plantings; water's edge seating; use of Front St. N. for alternate parking facilities; provision of a dedicated fishing area and fish-cleaning facilities; continued presence of charter boat docking, which reinforces the traditional Harbour ambience and draws tourists to a central area to maximize the positive economic impact...
- We believe that this site needs to be the subject of a **Place-Making** exercise.

- In keeping with the public wharf concept, an interesting idea has been put forward....by building an historic replica of a boat from our distinct waterfront heritage – the stone hooker...We need strong tangible reference(s) to this epoch (Mississaugas), perhaps located in Marina Park. ... Before the public can decide whether it is appropriate to move these facilities, there needs to be an understanding of the alternatives proposed....the streets of OPCV were filled with car and trailer parking every weekend during the boating season, which caused much disruption to residents. We believe that a public boat launch in Port Credit is essential. The old Texaco lands, or more possibly the Harbour Marina lands, could potentially be utilized for this purpose....The proposal for commercial and small scale retail at Marina Park has proven to be controversial with some stakeholders. We support the recommendation of conducting a marketing study that could also encompass any proposed restaurant at Saddlington Park. In any event, the land at Marina Park should remain publicly-held.
- The plan calls for the existing boat launch and parking in Marina Park to be relocated but doesn't say where it will be relocated to. I hope the new location will still be in Port Credit.

### J.C. SADDINGTON

- I like the lookout
- Saddlington Park lookout shelter looks ugly – too modern – not in keeping with rest of structures in other parks which are modern Victorian types
- If the boat launch is removed from Marina Park, there is no facility to put a canoe into the Credit. "Hacienda Bay" is far too open/choppy to use.
- Like the idea of the promontory looking out on the lake, however as much as I would like it I have some doubts as to whether a restaurant would survive year round at this location. Perhaps a seasonal snack building would make more sense.
- Can the old pumping station buildings...be used for Art Gallery of Mississauga outreach gallery?
- The restaurant not a good move. I don't see it surviving. Parks should be for people maybe offering refreshment areas (vending?) - clean washrooms - Roofed open shelter areas, splash pads, music pit. BBQ pits.
- Concerns with the draft plan for J.C. Saddlington insofar as the addition of facilities at the same time as parking is removed. The parking is full on weekends at present and there is little doubt that an increase in facilities will encourage additional park users. Additional parking will be required, not less.
- Concerns with the proposed restaurant in J.C. Saddlington Park. While a small concession might be a good idea, a privately operated and potentially liquor licensed restaurant has no place in a public park.
- While there are low-lying areas of J.C. Saddlington Park which would require raising to promote proper drainage, paving of the park should be limited. As well, trees should not be removed to accommodate pathways, promontories or other features.
- I dislike the Restaurant in park, consider food cart vendor permits instead
- How does climate change lake level affect plans?
- There is no need for large boat launch areas at Saddlington
- Food shade should be good at restaurants
- Think about winter months, outdoor skating rink would be nice, no restaurants needed
- Good to know that there will be further Public Consultation on JCSaddlington Park
- Restaurant would be a great idea if done properly. Sit and relax, enjoy beer or wine (legally). Lake Puolinch had one and it was very enjoyable. Look at popularity of Snug Harbour. It overlooks the water, people walk and enjoy the surroundings.
- Naturalization is Great; CVC consultation ongoing Please.
- Good idea to get rid of the old parking lot on Lake street
- Need the parking here
- Need canoe, kayak launch
- Need parking
- Balance of people use / natural restoration. More shade trees, more bicycle routes, more seating, like the lookouts in J.C. Saddlington Park.
- Let's hope "naturalization" means more than just letting the weeds go! high Park has become a wilderness of golden. The same is already happening on the footpath below the Texaco lands. No more naturalization that just makes a mess
- Subsidized food services to keep it affordable for all ages, especially children i.e., hot chocolate/ ice cream
- The whole place is subsidized! Pack a lunch
- You need parking if you have a restaurant. What kind of restaurant? Small scale- affordable or families? Prefer options
- The vertical markers look UGLY!
- Make this a true Greek style amphitheatre with true natural acoustics

- Garbage is a huge concern. Signs should be put enforcing use or garbage bins; Also put more recycling bins for plastic bottles + green bins (left over)
- We don't want a restaurant (garbage) here. There are enough nice restaurants/coffee shops within 2min walk
- Lighting for safety
- Small craft launching facilities
- Doesn't need restaurants. There are enough close by, and it would just take up space
- Parking Issues; No restaurants! Any shops on west side of bridge will not survive and Main drag already suffering/struggling
- Imperial Oil 30 feet extension should be list (Coyote risk)
- I strongly suggest that the water conditions at Hacienda Bay be investigated under varying weather conditions....for non-motorized craft...and that an alternate location upriver be considered.
- I strongly disagree with restaurants at JC Saddington.
- I like the warming facilities promoting all season sue, heritage courtyard using the existing pump house buildings, community programs, amphitheatre, wetland and canoe/kayak access in Hacienda Bay, lookouts that connect park to the lake, river edge connection to Marina Park
- Not sure about the promontory at foot of Mississauga Road and not sure about restaurant since Port Credit has enough restaurants within walking distance.
- We **strongly support** the recommendations that open up the park to the water, and harmonize the park with the neighbouring Heritage District, namely: stabilization of the shoreline, walkway connection via water's edge boardwalk to Marina Park to the north, elimination or reshaping of the berms, improved sightlines to the lake from the interior of the park, and from the foot of both John and Peter Streets, improved streetscapes along Mississauga Rd. and Lake St., eco-sensitive landscaping and encouragement of diverse tree plantings, enhancement of the existing pond, stream, and wetlands, provision of dedicated fishing areas, updating and repair of physical features (e.g. wooden retaining walls), The park's design needs to be re-oriented to optimize its lakeside location. ... It is important to provide comforts, including sufficient washroom facilities, drinking fountains, picnic areas, numerous benches, at least one shelter, and other amenities such as a concession kiosk or café. Warming stations for the winter months, and continuation of the skating pond, but with better facilities, are good ideas. While we agree that the northern parking lot should be converted to a "village green" area, to provide better transition to the Heritage neighbourhood, and a more attractive approach to the park, provision for adequate parking needs to be maintained for the large number of visitors hauling coolers, chairs, BBQs, tents, and other equipment, in order to stay for the day.  
..Insufficient parking would result in cars parking along the residential streets of the adjacent Heritage District....The proposed new "promontory" feature unfortunately spoils the Harbour vista at the south-east point of the park. The visual and physical connection between the lake and river should not be "formalized as a park feature"...The designated area for fishing respects the existing patterns. ..Proposed "lookouts" should be minimal, low-profile, and a simple extension of the shoreline stabilization protocol...The possibility of a small-boat launch for kayaks and canoes in the "Hacienda Bay" location ties in beautifully with the recreational identity of the park, and should be in addition to the launch we have proposed for Memorial West.
- The ability to view the night sky from an area of darkness, coupled with a proposed astronomical lookout at the foot of Mississauga Rd., reinforces present use. Care should be taken not to destroy the sightline to the water, when one is traveling down Mississauga Rd. S. to the Park....Interpretive elements should be in keeping with the Heritage District, and not detract from the natural features of the park (pond, stream, wetlands, trees, wilderness areas, etc.) Wilderness areas are fine if care is taken to nurture native plantings and enable ecological understanding...Potential wetland restoration is a very exciting concept in the "Hacienda Bay" area, but again, subject to the same concerns.  
...We would promote a genuine amphitheatre, sunk into the ground (which may not be possible in a landfill site), with natural acoustics that will awe children and adults alike....Adaptive re-use of the existing pump house buildings, as suggested, and with a possible courtyard, is an excellent idea, and could also be the site for small theatre enterprises instead of, or in addition to, the amphitheatre concept. Note that there is a high berm located behind the pump house – perhaps this, or some other berm in the park, could be adapted for amphitheatre seating? ...There is controversy about the "restaurant" component, as indeed occurred during the previous public workshops....There is an economic concern by some stakeholders that this would compete with the restaurants on the Lakeshore Rd. business strip, and keep JCS Park visitors from walking into the Harbour area, although the new boardwalk north to Marina Park will surely encourage just that! There is also a concern that a restaurant, with the additional parking it would require, and the blockage of the lake view, is an unnecessary encroachment and an unwanted built form in what should be a pastoral environment. In any event, the land at Saddington Park should remain publicly-held. Simple food amenities at the Park are a must.
- The promontory feature proposed for Saddington Park looks like a sculpture and it seems out of place in a semi-natural environment...I do not see any need for a restaurant in the park. There may be a market for a seasonal snack bar next to the pond, but that function could also be served by mobile vendors. We do, however, see a potential for a park pavilion / small special events facility...If the building could be designed to allow people to experience the park environment - water view, natural surroundings, wildlife from the comfort of indoors, I could see

it becoming a popular place for weddings, conferences, group picnics that need insurance against inclement weather, etc.... There is a felt need for the proposed water's edge trail between Marina and Saddlington parks.

## PORT CREDIT MEMORIAL PARK WEST

- Memorial Park West – I like the layout
- Memorial Park West – shore treatment needs to be natural (soft) to allow boat wake to dissipate. East side treatment has created a problem which for novice rowers, paddlers and family canoe trips becomes a safety concern. Planting proposed just south of rowing club would get trampled by visiting teams and spectators watching events held regularly at the two clubs. Don't over plant – keep it simple
- Planting area to the south of the Don Rowing Club paved area should be moved further south to allow large boat trailers to turn to access our boat bays. About 20 meters should be okay (adjacent to proposed pedestrian walkway)
- The Don Rowing Club needs the area next to the Club – on the northern part of the Park – for Boat storage and transportation equipment.
- I am concerned about changes proposed to the riverbank along Front Street between Lakeshore and the Rowing Club. We need a soft-scape bank, not retaining wall to disrupt the wake caused by boats. The retaining wall on the east side of the river has created a lot of issues with boat wake. I would also like to see a plan that will encourage the geese to go elsewhere!
- Care and concern re Memorial Park west development that facilitates recreational use of Credit River – not only by Don Rowing club and Mississauga Canoe Club but by private users as well (that a proper and protected regatta course on the lake!)
- Memorial Park West: Shoreline treatment must consider boat wake from coach boats, pleasure craft; The east side treatment has created wake making it unsafe for novice rowers paddlers, and family canoe trips.
- Have suitable signed dock launch facilities for private canoers & kayaks so that they do not need to use the facilities of private member clubs such as Don Rowing
- Memorial Park West: The canoe club and the rowing club both large public regattas and spectators and visiting teams use this space. The planting area just south of rowing club cuts off usable space and would likely be trampled. Keep it simple and don't over plant.
- Existing totem pole requires painting and the base reinforced and the pole vertically straightened.
- With all the commercial space being built in Port Credit more temporary and seasonal buildings will have difficulty surviving.
- Need a place to put canoe in the river
- Provide for parking off waterfront
- Drinking fountain needed, public washrooms
- Like maintaining the green space....vegetated slope section to river, possibility of path under the Lakeshore bridge.
- Not sure about the vertical columns
- Would like to see added: canoe kayak access since the parking is a reasonable distance – a contoured shoreline would be nice;
- Reconfigure Lakeshore and Front St N to remove existing metal barriers between sidewalk and park..."push" pedestrian into traffic...
- We **strongly support** the following recommendations: stabilization of the shoreline, walkway connection under the Credit River Bridge to Marina Park to the south, continuation of the bike path up the Credit River as far as possible, maintenance of Front St. N. car parking, buffered from the park, open lawn concept, water's edge seating, eco-sensitive landscaping and respect for the existing tree canopy, potential expansion of the park northward as sites (e.g. the Legion) become available (i.e. no new built structures such as apartment buildings, should be permitted), provision of fishing areas. ...A further connection...is construction of a pedestrian/cycling bridge just south of the CNR tracks, which would streamline access to the Port Credit GO Station, and link Memorial West to Memorial East over the Credit River. Addition of a public canoe / kayak launching area would be compatible with the existing tradition of the private clubs, and reinforce the recreational use of the River (now an echo of the historic trading uses)... The "Olympic Walk of Fame" could mark the route paddlers would take to reach the launch at the water's edge...The proposed "interpretive column" features within the park would clutter the landscape and detract from the passive experience. The existing totem pole, while not historically correct, *has* become a local landmark and should be the prominent feature if it is retained in this location. Any lighting near the water would spoil the evening experience of darkness many enjoy along the riverbank.

## FUSION

- Maintaining a much naturalized approach to park design and management. This site should be managed to increase and maintain biological diversity; the outdoor ice rink is a great idea.

- Extremely pleased that the park will have cross country skiing as a use. How does the City intend to keep other people off the trail if they are snow covered? Overall, I think this park design is very good.
- Fusion Park (and any others) nice to see preserving historic buildings, incorporate and utilize
- Leave the Fusion Park as natural as possible (see above). This shoreline has one of the last remaining pebble beaches in Lake Ontario and are important (fish habitat). Leave the two parks in this area as "Adult Parks" retaining wildlife access to this lake
- Armour stone is NOT natural & takes away the natural lake edge much needed by shoreline.
- LEED design for structures
- Like Preservation of woodland walking trails, Outdoor skating rink, Preservation and improvement of area for bird migration, Bus access in east parking lot, Tree house & secret garden, Naturalization of Clearview creek, Shoreline recommendation

## LAKESIDE

- Lakeside Park – grass between cars parked – white, nice, is not practical for maintenance
- I do not like the fact that leash free park is reduced in sizes in some parts of the park
- If the leash free area at Lakeside is slated to "shrink", I am opposed. The park is well used year-round by dogs and their owners
- The air around Lakeside Park can be quite foul. Are we spending wisely developing a park space that may never attract a crowd?
- Move large leash free to Texaco land in Port Credit (reply to this: This is a non starter)
- The size of the existing leash free area must remain or increase; not be made smaller. This park is well used by dogs and their owners
- Size of leash free must stay the same or large
- Why develop a park where the air is so foul?
- Concern with environmental impact (pollution) caused by heavily used parks. Recommend to leave this section of waterfront "natural and undisturbed."
- Concern with public safety. This heavy industrial area with truck traffic 7 days per week. Recommend to leave this area as natural, undisturbed area with some trails for use by small number of people to observe natural wildlife and flora/fauna
- Like green paving idea, alternate energy source, demonstration gardens, splash pad, winter shelter
- I welcome the green technologies demonstration area planned for Lakeside Park, and I hope that green technologies will be used throughout the Waterfront Parks system where possible.
- Concern ...if the proposed park began to attract greater numbers of pleasure craft to the area, the owners of which may not be as aware of the dangers of navigation in commercial shipping lanes.

## OTHER COMMENTS

- Nowhere are my concerns about increased gang activity, drug dealing, traffic calming, parking controls, after hours security and park closing times addressed.
- Please do not overdevelop the parks
- If we are to move towards being environmentally friendly, we need green
- It feels a bit "contrived", we should be striving to create natural areas
- Emphasis on enhanced green space and waterfront access
- Acquisition of waterfront property as it becomes available
- Strategy shows that City is really trying to provide improved parkland for the public.
- We like the idea of linking as many parks as possible along the waterfront.
- Concept of a shuttle among parks and into parks, (village market and Marina Park), Greater winter-time activities proposal, Underground connection to Marina Park, Saddington Park. Water activities are always a great draw/use of space
- Lack of parking – it is needed in Port Credit, many visit the area
- No restaurants/commercial in the parks – there are enough in the village! Less mortar, more green!
- The concepts are excellent, what about the area presently used by the Post Office to park their vans between the Marina and C/Y yachts?
- Parking is going to be a major problem
- Parking, Parking, Parking



- Concern about parking – definite route buses to parks
- Bike lane on Lakeshore – already too busy
- No large boat launch off Imperial Oil property
- The 2 western parks Lakeside and Fusion are located in a heavy industrial area with plants operating 24/7. Development of the plants with splash pads etc. are inconsistent with this area. Splash pads/ice rinks should be where the people are
- Better access to parks at water from subdivision
- No recommendation to better connect “water colours”
- Need tunnel or bridge over to Port Credit or Clarkson
- Anyone in this community has to travel north before they can go south to Lakeshore. Causes traffic congestion at waterfront
- Light hearted music – semi classical – not blaring, affordable
- Like the idea of people first, not cars
- I like the idea of lighting, right now some of the trails are poorly lit
- Like the idea of outdoor skating rinks. Want to see a place to rent skates and buy hot chocolate
- Like the idea of theme gardens (e.g. Toronto Music Gardens)
- Like the “event” area
- Pedestrian and bicycle friendly spaces
- Access to the lake preserved for all
- Multi-use facilities
- Attractive and multi-purpose friendly
- Green space brings out community! Yeah
- Nice to see the improvements!
- I like the fact that we’re protecting the land from development and making it accessible to citizens and visitors
- Activities should be walking, etc. at Waterfront not organized sports i.e. A.E. Crookes
- Eliminating organized sports facilities in waterfront parks, i.e. baseball, softball
- That it’s a “draft” and it’s still open to input from the community
- I believe the beautification is essential for preservation. An attractive Waterfront will draw people. Parking though, becomes a bigger issue like the Farmer’s Open Market very much.
- Urgent Problems: Goose defecation is a very serious health and safety issue. Any modification of shoreline that would encourage Geese to access the shoreline via canoe and rowing docks will be a disaster. Slipping on Goose droppings can cause serious injury, when carrying rowing shells.
- The general concept is good.
- Aesthetically pleasing. More recreational opportunity for those who would not normally visit our parks.
- More park areas and trails, more benches and shade
- All parks good for people. More gardens
- Overall good
- Overall I think the strategy is good, encouraging more people to get out and enjoy the outdoors and community, this is why I like to live in Port Credit.
- Not much (to like)
- Possibility that transit will help connect/link the waterfront parks. What about smaller buses – “shuttling” people between sites? Is it less costly than running a full bus service?
- Great to see connecting water front parks and water front trail
- Not a whole lot (to like)
- (like) There is a strategy and input is being sent
- Would like move parking lot (short term) to allow canoes to be launched near water’s edge (beach areas) – now there is only Jack Darling and Canoe Club (when they aren’t there or by Salmon ramps) which according to plans will not be available (Richards Park might work)
- I would like to see the front open view to the lake
- I don’t like the fact of having buildings blocking the view of the lake
- Green space: need to add more
- Natural theatre seating – good, no need to port chairs
- Impressive
- Green space
- Lots of parkland
- I like everything pertaining to the revitalization of our parks
- First class art work but of no value unless each picture is shown in relation to location along the waterfront. Useless you are steeped in the progress, a “visitor” would never be able to recognize the areas or locations – the small overall map is at the end

- Well developed and cohesive strategy linking natural resource to use with blend of interests/uses in mind
- Comment regarding the stench emanating from the waterfront during the summer months, particularly after a rainfall. This is seems to be an on-going occurrence as I have had this malodorous experience on previous visits.
- Credit Village Marina has facilities for large boat docking and has, in fact, been used for this purpose in the past. The pier extending along the west side of CVM is sufficient to moor boats such as those pictured (Mariposa Belle) in the Strategy. There is no need to also provide this facility at J.C. Saddington Park.
- Winter use - there are no activities. Without dog walkers, the parks would be deserted. What about cross country skiing? Cafes along the waterfront are desperately required.
- Toilets - many more are needed. The few that do exist are closed for 6 months. Water fountains - the few that exist have not worked in several years. Garbage cans - 90% are removed during 6 months of winter. All cans need lids to stop raccoons from tipping them over and spreading garbage everywhere.
- Water conditions at "Hacienda Bay" are investigated under varying weather conditions before the City suggests that small non-motorized craft make use of the area for launching and that an alternate location upriver be considered.
- Applaud your ideas for maintaining the boat charters, providing a fishing area, as well as having a path between these 3 parks.
- These two parks, Marina and J C Saddington are 'embedded' in a residential area, should be addressed differently than the 'destination' parks and these 2 should not be lumped in with all 5 parks as the same. They are not the same. I believe Marina Park also has the added issues of environmental disposal with soil disturbance?
- I have recently started to discover Mississauga's parks and they are lovely as they are (and large and plentiful)! Talk of things like "blurring" the lines between parking and parkland with little squares of concrete with greenery growing in between is lovely - but absolutely unnecessary and extremely wasteful of taxpayer dollars.
- Whereas Port Credit and Clarkson have lost the opportunity, 100% of Lakeview (except 9 homes on Richey Crescent) MUST be a waterfront trail and park - from the Adamson Estate to the Arsenal Lands.
- Priority list for Arsenal lands would be to make it a destination that can be all year around and use it for exercise. For this it would be great if it were connected to a Waterfront trail. A boardwalk would be fantastic. The off leash area looks too small to be useful. What about using the long narrow strip on the east side of the creek as the off leash area. It would give the dogs a longer run and keep it completely separate from any kids' activities. I currently have to drive my dog all the way over to The Jack Darling Park for a decent off leash area. I would love to be able to just walk to this one. What is the next step in the process for this particular park?
- Parking accommodation is a big concern
- Too Urban Park scheme. Parking will be a problem. If you add attraction, tourists will come. Create parking north of Lakeshore Road.
- Too much infrastructure. A park should offer an opportunity for picnics, exploration, solitude for children to explore. I find that the City constantly attempts to build roads, restaurants and other unnecessary structures. People will find their own way of enjoying the beautiful waterfront parks
- I think that P389 and Lakeside Park should be left undisturbed in natural condition, undisturbed. Have you asked local industries for their comments and input about plans for these parks.
- Do not eliminate the natural water's edge environment by putting armour-stone. It's unwelcoming for migratory birds. Natural, natural, natural for the little remaining, waterfront, please. There seems to be too much organization.
- Expanding the usable waterfront public space. Long term direction of preserving and building for generations to come.
- We want the waterfront attraction to bring people from the rest of the City (& beyond) so need to make parking more of a priority, not eliminate it.
- Although we need to increase non-vehicle access to parks, the more popular parks will require more parking. The reality is that we're a city set-up for drivers.
- Promote boating/fishing
- Create amenities that will create income to help pay for facilities amphitheatre rental, Thai Chi in summer
- Some realistic thought concerning how people will get to the parks and park their car
- Like recommendation about food and drink on waterfront. Europe example - walks and enjoy "market atmosphere". Need a place to sit, enjoy, coffee, ice cream or hot chocolate as a family
- Saving Harbour nice but too expensive for family outings
- Jack Darling Toboggan Hill - not hay bale and fences at the bottom but berms to slow and stop sleds at bottom. Present solution is dangerous, maybe some sculpting of winter runs for excitement and safety
- No buildings, tents, pavilions
- No metal structures
- No pavilions
- No buildings
- No shops
- More green space, parks!
- Bike trails have to be off the road (Lakeshore) not part of lanes

- Want to bike along lake all the way to Oakville
- Boat ramps should be maintained
- Boat rentals should be maintained
- Keep parking and more.....underground parking?
- I hope there is plenty of restaurant/pub/coffee shops on the lake included in the plan – we want to attract visitors – people like to eat, drink, chat with a lake view. There is nothing available for these activities between Burlington and Toronto – we have the best bit of waterfront with which to add this kind of “big city” (e.g. Baltimore, San Francisco, San Diego, etc.) approach
- I wish the City will implement strict selection as to the kind of shops/store they will allow to put in our park
- Include some areas for skateboarders and cyclists to keep them off the sidewalks! Wetlands excellent; education, drainage and lake improvement
- Parking, Parking, Parking
- Really need dual (or more) use facilities to draw people to parks even in winter. Ponds - fishing, canoe lessons, kayaking, all “summer” – skating “winter” – areas where planned activities can occur in winter (e.g. a Winterfest; Iditarod(dog sled race/rally); orienteering trails of various lengths, 1km, 2km...)
- More “youth” focussed areas – if not on the water’s edge at least spread across an area such as Douglas Kennedy Park or Lakefront Promenade
- Benches adaptable for seniors and grandchildren both of us fall through some wide backs and too tall seats
- Skate board area – more basketball courts
- Outdoor swimming pool somewhere along the Lakeshore trail looking out to the lake. There is a lack of outdoor swimming facilities for adults in Port Credit
- Fire pits near the proposed seating rinks
- Like to see a bonfire pit area that can be reserved. Allows you to use the area at night
- Transit Strategy need to be aligned
- Want ability to rent kayaks and canoe, want a designated canoe pond
- Less development, put more money into maintaining the existing park
- More natural spaces
- Focus on foot and bike traffic
- Outdoor activities for the family – year round i.e. outdoor skating rink at the Marina Park (by lighthouse)
- No mention of a LRT or other Transit Plan. Why do we need a restaurant in JC Saddington? Another tax consideration? We don’t need another restaurant in Port Credit. What about Family Picnic Area?
- An outdoor skating rink would be nice – such as Gage Park Brampton
- A side note – the Derry House (Adamson Estate) is in need of maintenance i.e. paint, replace boards, broken window
- 2<sup>nd</sup> level parking in library parking lot – Port Credit needs more parking!
- Existing utilities companies should be responsible for buffering residents via landscaping
- Addition of trees (mixed evergreen and deciduous) in existing parks
- More benches, a boardwalk with solar lighting that would stretch far enough for a safe form of exercise even in the middle of winter when it’s dark by 5 p.m.
- A very specific type of Commerce, ice-cream shops, outside vendors, reasons for people to come down from north Mississauga and Brampton. More playgrounds for small children
- Shoreline should not be like east side of river, which bounces back wake from the power (coach) boats as safety issue to rowing shells
- The new water’s edge should allow waves to disappear – considering rowing traffic.
- Water trail walkways should have walking/cycling/roller blading lanes to organize traffic along the trail. Some ideas seem to make the parks system to commercialized; it can take away from the quiet atmosphere of the park.
- Leave park areas as natural as possible; more police; washrooms open during daytime hours; trail use in winter; snow removal
- By-law for pet control. Higher fines for non-leashed animals and poop and scoop
- Location for community gardens, more education/demonstration for natural/sustainable living, eco/struc. i.e. homes, natural gardens, organic gardens, more/larger market places for local foods / farmers (i.e. local farmers markets)
- Signage needed for the Waterfront Trail – in these parks and others – it is somewhat “hit and miss” at present
- Generally – good!! Looking forward to seeing this come to fruition.
- Green parking at vacant Imperial Oil site – free parking. Add bench Volleyball to Saddington – Add “do not feed birds” sign.
- More emphasis on water quality improvement and attention to the algae problem that plagues the Ben Machree Park and Rhododendron Park.
- Connect Ben Machree with Rhododendron Park asap.
- Parks receive year round use. Why not have washroom facilities available for 12 months of the year, not just the summer months? Encourage access to parks by means other than automobile (i.e. walking, cycling, bus, etc.)

- OPG land should not be used in the future for industrial use.
- Leave P389 and Lakeside Park as "natural" parks for nature lover to hike and observe local wildlife. Do not develop this area. It will cause more pollution (litter, cars, etc.) this is a heavy industrial area used 24/7 and bringing large numbers of public may cause safety issues.
- Winter washrooms in Jack Darling to accommodate children sliding, cross county skiers and walkers. Plant native species for wildlife which depend on it.
- Create more places to congregate in small numbers, benches, places to sit etc. Connect to existing Lakeshore Road shops etc., especially west of the river, rather than compete by adding more marginal commercial space. More "comfort stations" where concentration of people are expected.
- Parking vital at Memorial Park West and Marina Park. Like to see washrooms open earlier (6a.m.) and remain open off season. Marina Park needs a place for public to launch a canoe. Would love some snow ploughing on pedestrian / cycle paths for most popular waterfront parks. It would allow much better footing and greater use.
- Who is going to pay for all this?
- "year round destination" None of your photos on this board relates to the 6 month of cold wet slush & ice which are the ingredients of a Mississauga winter. Are you dreaming!
- Link Marie Curtis Park in front of sewage treatment plant to Lakeview generating site & Lakefront Promenade.
- Open waterfront from Etobicoke creek to Port Credit walkway along shoreline.
- Celebrate Aboriginal heritage. Build in First Nations into Planning and Economic Development
- Keep "Art" displays away from natural areas. Orange banners (for example) would alarm bird, especially during migration. Waterfront (a rare commodity) should be first and foremost welcoming to migratory birds.
- What about a shuttle bus between parks- especially along the lake in the summer time?
- Bike paths & walking path along trails – "designated paths"
- Markings On walkways should be kept to a minimum, as they do not enhance a walk in the park
- Seems like a lot of construction & money spent to create "natural" park system. Why not just nudge nature a bit?
- Need to acknowledge and be flexible to accommodate rowing paddling regattas, lots of people, boats and need for viewing; Canoe & Rowing clubs are wonderful resources truly unique to Mississauga and need to celebrate these.
- Need benches to sit and relax and view the waterfront
- Considering our aging populating, would there be ramps for wheel chair access? Please think about this.
- More trees, not just 2 small strips.
- We need year around washroom in parks, not comfort stations that are open for only 4 months.
- give more budget towards landscaping in existing parks include mixture of evergreens as well as deciduous trees
- why not have water fountains in all City parks?
- parks within residential areas should say "Parks close as sun-down". This is being done in Oakville.
- why can't we add a skating area beside the Credit River or when cold enough, on the river? Why not bring people down in the winter?
- skateboards area and lots of basketball, teenage stuff
- Jack Darling Park has lots of winter activity, sledding, skiing, but no washroom facilities in winter
- snow removal of trail will improve winter use
- some facilities are needed to draw people to lakefront parks in fall and winter e.g. skating pond, actual canoe/boat pond in summer
- Recognize the waterfront as Mississauga's most vital natural resource. It should be formally recognized as a significant cost centre and not as an opportunity to spend the bare minimum. This is an important distinction....100% of Lakeview (except 9 homes on Richey Crescent) MUST be a waterfront trail and park - from the Adamson Estate to the Arsenal Lands....Toilets - many more are needed . The few that do exist are closed for 6 months.
- Water fountains, Garbage cans, Garbage collection - over concentration on sports areas and not on the parks....While I agree fewer cars and parking spaces is an ideal concept, it is not reality. Until bus routes are expanded, bike and roller blade paths from Lakeshore are created and expanded, parking at the parks is a huge issue....
- Existing natural areas with "Let it be" concept are nice but often are regarded as the lowest cost option. More variety and vision is required. The introduction of more wild life especially deer would be most welcome....
- Access streets such as Hampton Crescent and Montbeck Crescent need traffic calming. This area has no sidewalks and no cycle paths.
- Bus routes access right to park entrances are needed, not just along Lakeshore Road.
- Signage from Lakeshore needs huge improvement E.G. Lake Promenade signage from Lakeshore is tiny.
- Winter use - there are no activities. Without dog walkers, the parks would be deserted. What about cross country skiing? Cafes along the waterfront are desperately required. Events - Mississauga Marathon is about it. Many more are needed.
- Better separation of cycle and roller bladers from pedestrians along trails. Introduction of sandy beaches everywhere. ...More leash free zones...



- Ice removal - all park trails not just priority trails. Improved after hours security to circumvent any increased gang activity or drug dealing.
- The City needs a full time marketing department to promote the waterfront as a destination point, a tourist attraction, develop new ideas and create excitement. I agree that organized recreational activities should be phased out. They swallow up all available parking and cause a constant strain with neighborhood with illegal street parking and garbage.
- I do not agree that existing sports facilities should stay. Other locations should be actively investigated. Arsenal Lands - a large leash free zone with water access should be identified. Create a Heritage walk for students and a war memorial, identifying the women that worked there in the ammunition factories. Lakeview Park should be abandoned and relocated on the Lakeview Generating Grounds. Douglas Kennedy Park baseball field should be relocated and re-identified as alternate parking and/or boat storage area away from the lake. ...A.E. Crookes Park softball field should be relocated. Immediate ban on expanding existing administration building facilities. ... R.K. McMillan Park - already has a North/South trail connection to Lakeshore. Make the cleaning of Cooksville creek a priority. Abandon parks - E.G. just east of Richey Crescent. Land owned by the Credit Valley Conservation at the mouth of Cooksville Creek, west side. ...
- The future development of the Lakeview Generating Grounds must be a priority for waterfront and park development. The idea of placing a new Gas plant there is unthinkable.
- All parks, and the entire water's edge, should ideally be in public ownership, and accessible year round
- We recognize that our present urban society, and Mississauga's physical structure, is car-based, and that
- many visitors will continue to travel to our parks by car, **YET ...**
- Port Credit needs to move towards a walking and cycling community model, and encourage Active
- Transportation, to reduce traffic gridlock and parking congestion
- Port Credit welcomes visitors from all of Mississauga and well beyond – our local residents, and local
- movement patterns, should be protected from the impact
- In addition to logical public transit within the district, feeder transit lines need to be developed in the rest
- of Mississauga to bring residents down to all the Waterfront Parks
- Maintain a recreational setting and do not over-engineer the "jewel on the lake"
- No development of the parks along our waterfront should trump the waterfront itself. If primary attention is paid to preserving and reinforcing the shorelines (river and lake), to enable public access without damaging the delicate edge where water meets land, then a tremendous responsibility will have been met for generations to come. ... Parking needs to be looked at holistically, not just at site-specific park locations, but as part of the overall destination of Port Credit. Parking issues go well beyond the parks to encompass both the Lakeshore corridor and the rest of the Village, including the neighbourhoods. ...Surface parking is at present inadequate in Port Credit, and will be much worse over time, despite a hoped-for increase in transit and non-automobile forms of transportation. We need BOTH better parking and better pedestrian / bicycle pathways. ...It is believed that better public transit will result in increased park usage with lower impact on "gridlock". The suggestion of a "potential Waterfront [Shuttle] Bus route" (p57) is excellent, and could be tried on a pilot basis....Part of the Waterfront Parks Strategy should encourage usage in the winter months, by making parks and pathways accessible for pedestrians, cyclists, joggers, and pets... and along the Waterfront Trail in winter would promote Port Credit as a year-round destination for all of Mississauga, and for winter tourists, and would provide more flexible and pleasant access to the West Village shopping district. ..The Waterfront Parks Strategy is a comprehensive and visionary framework for Mississauga's finest and most enduring asset: the waterfront. The City has recognized the social, economic, and environmental return on enhancing this asset, and has put the appropriate resources into researching and developing this plan. The degree of public access to the waterfront that is currently available, and planned for the future, reflects an enlightened principle that has been followed since the City's inception, and has now brought us to this exciting new phase. Speaking more specifically in terms of Port Credit, the strategy to bring uniformity, as well as cultural identity, to each waterfront park is well-conceived. Just as there is a need for a street-furnishings policy across Port Credit's business district, our waterfront parks (which are all highly visible) need a coordinated policy on signage, drinking fountains, lighting, heritage markers, garbage cans, bike racks, benches, etc. Furnishings throughout the Village and the parks should work together, with materials, colours and styles providing an overall Port Credit "signature" that reflects our rich heritage resources, Harbour location, and natural environment. ...Given the diverse socio-economic mix of the population, the parks are a great equalizer where all can freely benefit from the cooling Lake breezes; the walking, biking & small-boating culture; fishing; and "getting away from it all." We would like to see these parks evolve in response to usage patterns, rather than be over-engineered initially. Do not "program", but rather allow visitors to make their own choices and create their own sense of place. An increasing number of seniors living in South Mississauga will also be shaping how the parks are used, and what facilities are needed. The "place" should be the destination, not the program, and each "place" should be affordable and comfortable for all. That said, our overall impression is that "programming" is not the intent and the focus of the Draft Final Report, and we strongly endorse the emphasis on sustainability and flexibility in the Parks Strategy...Ambient sound is also a factor in park enjoyment, and people should be able to hear the water lapping against the shore, the wind in the trees from AE Crookes Park west to Richard's Park, no 'sport' centred place for teens



- Hampton Crescent identified for an increased access point to encourage more people to use facilities. I want you to perform a traffic study on a long weekend in the summer before you make any such recommendations.
- Remove organized recreational use in park and allow more use by public for spontaneous sports cricket, soccer, baseball etc.
- AE Crookes baseball park needs to have late games moved to another park re: nuisance to local residents, parking and park users. The park should be for everyone to use not just baseball.
- Before we focus on expansion, perhaps we could maintain the existing system. For example, snow removal and year round washroom access.
- Where are the outdoor musical shells that teen band/orchestra etc groups could perform from?
- Where are the wind farm elements to be located that make these outdoor power needing elements cheap and self – sufficient? Will there be solar paneled sitting areas to attract the temperature conscious elderly?
- Will there be astronomy oriented teaching learning centres for youth teen adults to enjoy attending/participating? Where will scuba diving etc displays/learning centres be located? Will glaziers/potters/jewelers have a centre to call home and to teach youth from?
- Has the impact of our greasy geese been planned for?
- a fishermen's museum here?
- Signage pointing to the nearest washroom facility. Especially needed at waterfront trail between JC Saddington and Ben Machree Park. Upon entering a park have directions to the various points of interest.
- Like having a strategy that looks at all the parks, consideration of improving public transportation access to parks
- How will artistic structures defining spaces and architectural canopies age in their design and the materials that are used in their construction.
- There needs to be a public canoe launch into the Credit River, with parking nearby. Adequate parking is required wherever there are group picnic sites.
- Make Arsenal Park a destination that I can go to all year round and use it for exercise. Another essential component is lighting...solar? Off leash area looks too small to be useful.
- Dredging of the Credit River will need to be dearly addressed sometime very soon...the silt level is so high...this should be addressed along with upgrading the shoreline parks! It goes hand in hand and I understand will require a lot of money coupled with environmental/disposing issues.
- I feel that with impending tax increases, we do not need to be spending 30 million dollars on parks. ...I strongly object to these projects at a time when we should be belt-tightening.
- I'd like to see the planners identify at least some residential streets in Port Credit, Lorne Park and Clarkson for access too.

## Memorial Park West: 1 hectare - 10,000 sq m (2.47 acres)

### Preliminary Park Budgeting

Prepared By: BMI in Conjunction with City of Mississauga

October, 2007

ITEM NO	DESCRIPTION OF WORK	EST. QTY.	UNIT TYPE	ITEM COST
<b>Memorial Park Development</b>				
1.0	Site Preparation	1	budget	\$289,000.00
2.0	Hard Landscaping	1	budget	\$282,170.00
3.0	Soft Landscaping	1	budget	\$180,000.00
4.0	Site Furnishings	1	budget	\$165,600.00
5.0	Miscellaneous	1	budget	\$116,000.00
6.0	Streetscape Work	1	budget	\$49,500.00
7.0	Shoreline	1	budget	\$464,400.00
<b>Sub Total =</b>				<b>\$1,546,670.00</b>
8.0	Contractor's Cost			\$15,646.00
<b>Total =</b>				<b>\$1,562,316.00</b>

**\*NOTE- No contingencies, costs for design and GST are included in the above figures**

## **Marina Park: (1.27 Hectares, 12,700 sq m (3.13 acres))**

### **Preliminary Park Budgeting**

**Prepared By: BMI in Conjunction with City of Mississauga**

**October, 2007**

<b>ITEM NO</b>	<b>DESCRIPTION OF WORK</b>	<b>EST. QTY.</b>	<b>UNIT TYPE</b>	<b>ITEM COST</b>
<b>Marina Park Development</b>				
1.0	Site Preparation	1	budget	\$706,500.00
2.0	Hard Landscaping	1	budget	\$1,630,250.00
3.0	Soft Landscaping	1	budget	\$179,000.00
4.0	Site Furnishings	1	budget	\$387,900.00
5.0	Miscellaneous	1	budget	\$178,500.00
6.0	Streetscape Work	1	budget	\$387,000.00
7.0	Shoreline	1	budget	\$880,000.00
<b>Sub Total =</b>				<b>\$4,349,150.00</b>
8.0	Contractor's Cost			\$48,917.00
<b>Total =</b>				<b>\$4,398,067.00</b>

#### **EXCLUDED & ASSUMED ITEMS:**

Cost of underbridge connection or Rivergate Walk not included

Cost of retail/commercial buildings including utilities, water and sanitary not included.

Site service connection off Front Street not confirmed

Servicing is for outdoor vending/fishing/event stations

**\*NOTE- No contingencies, costs for design and GST are included in the above figures**

## JC Saddington Park: (10.11 Hectares (25 acres))

(101,100 square metres)

### Preliminary Park Budgeting

Prepared By: BMI in Conjunction with City of Mississauga

October, 2007

ITEM NO	DESCRIPTION OF WORK	EST. QTY.	UNIT TYPE	ITEM COST
<b>JC Saddington Park Development</b>				
1.0	Site Preparation	1	budget	\$1,026,000.00
2.0	Hard Landscaping	1	budget	\$1,568,305.00
3.0	Soft Landscaping	1	budget	\$995,000.00
4.0	Site Furnishings	1	budget	\$597,100.00
5.0	Miscellaneous	1	budget	\$901,000.00
6.0	Streetscape Work	1	budget	\$292,500.00
7.0	Pond	1	budget	\$43,500.00
8.0	Shoreline	1	budget	\$2,025,000.00
<b>Sub Total =</b>				<b>\$7,448,405.00</b>
9.0	Contractor's Cost			\$54,234.00
<b>Total =</b>				<b>\$7,502,639.00</b>

**\*NOTE- No contingencies, costs for design and GST are included in the above figures**

excludes pond renovation, restaurants, tea house

The cost of shoreline may increase subject to further review

## **Fusion Park: (19.43 Hectares (48.01 acres))**

### **Preliminary Park Budgeting**

**Prepared By: BMI in Conjunction with City of Mississauga**

**October, 2007**

<b>ITEM NO</b>	<b>DESCRIPTION OF WORK</b>	<b>EST. QTY.</b>	<b>UNIT TYPE</b>	<b>ITEM COST</b>
<b>Fusion Park Development</b>				
1.0	Site Preparation	1	budget	\$790,000.00
2.0	Hard Landscaping	1	budget	\$1,359,900.00
3.0	Soft Landscaping	1	budget	\$1,097,000.00
4.0	Site Furnishings	1	budget	\$1,064,000.00
5.0	Miscellaneous	1	budget	\$1,271,500.00
6.0	Streetscape Work	1	budget	\$295,000.00
7.0	Shoreline	1	budget	\$1,811,000.00
<b>Sub Total =</b>				<b>\$7,688,400.00</b>
8.0	Contractor's Cost			\$77,184.00
<b>Total =</b>				<b><u>\$7,765,584.00</u></b>

**\*NOTE- No contingencies, costs for design and GST are included in the above figures**

excludes major renovation or development of building

assumed BMI costs for Shoreline - inconclusive until further study