

2476-2482 Confederation Parkway  
City of Mississauga

# PLANNING JUSTIFICATION REPORT

11/2020



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Sgt David Yakichuk  
Park

Hurontario St

Confederation Pkwy

Dundas St W

Brickyard Park



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# 1.0 INTRODUCTION

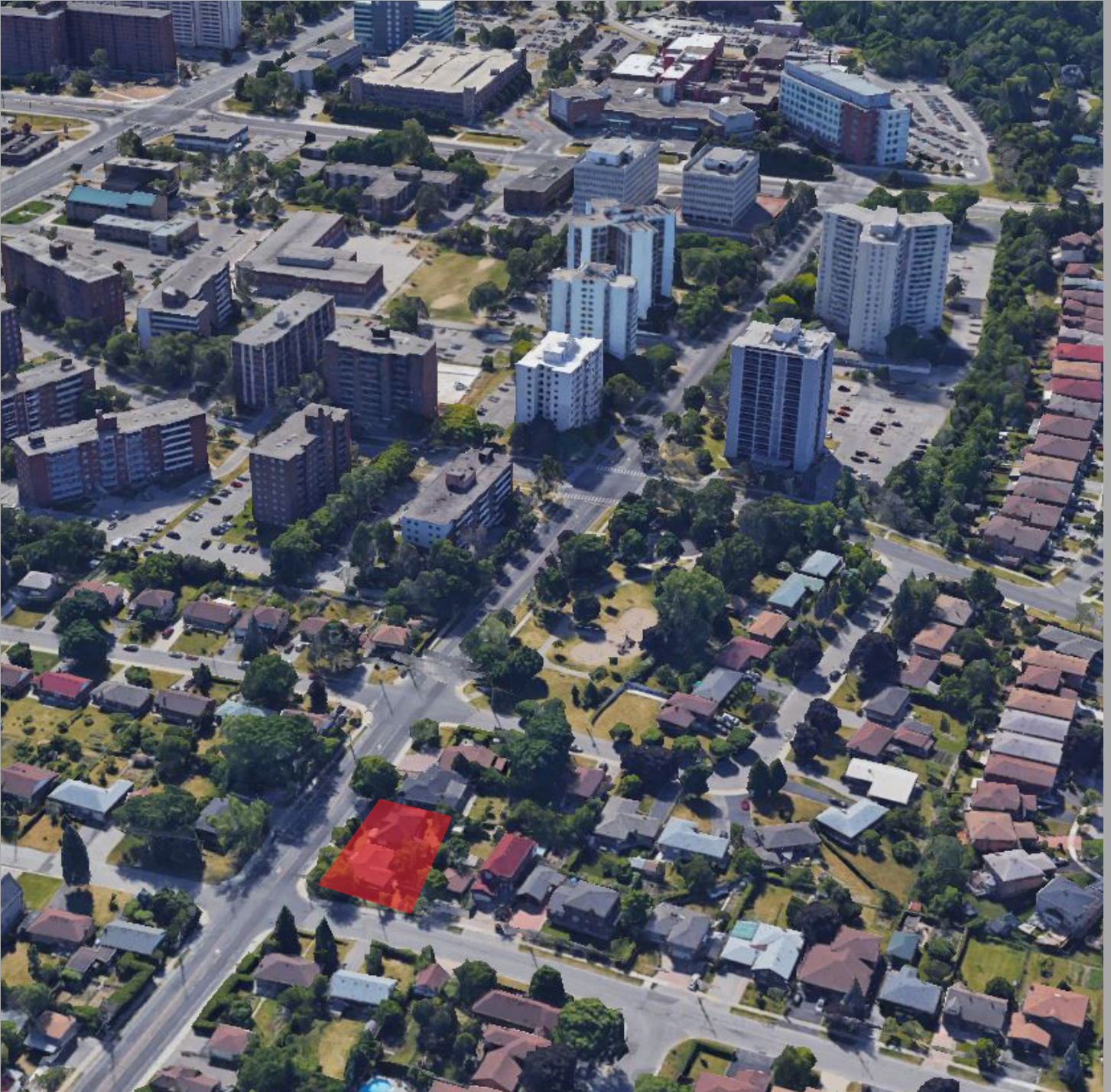


Figure 1 - Oblique Aerial Image Looking South towards Queensway W



Sajecki Planning Inc. has been retained by Preeminent Construction Management Inc. to assist in obtaining planning approvals to support the redevelopment of the properties municipally known as 2476 and 2482 Confederation Parkway in the City of Mississauga (the “subject properties”).

This Planning Justification Report (the “report”) has been prepared in support of an application to amend the City of Mississauga Zoning By-law 0225-2007, in order to permit two three-storey semi-detached dwellings on the subject properties.

The subject properties are located along the west side of Confederation Parkway, south of Dundas Street West and north of Queensway West.

The purpose of this report is to provide:

- An overview of the subject properties and surrounding context;
- A review of the applicable Provincial, Regional and Municipal planning policy and regulatory frameworks;
- A description of the proposed semi-detached dwellings;
- A summary of the technical reports prepared in support of the proposal; and,
- A description of proposed amendments to the City of Mississauga Zoning By-law (0225-2007).

The required pre-consultation meeting with the Development Application Review Committee (DARC) to discuss the proposal was held on October 17, 2018. A record of the submission requirements checklist is attached to this report, as **Appendix A**.

The following reports and materials were identified by the City and commenting Agencies as required for a complete application, and have thus been prepared as part of the Zoning By-law Amendment submission:

1. Complete Application Form;
2. Planning Justification Report;
3. Draft Zoning By-law Amendment;
4. Architectural Plans;
5. Engineering Plans;
6. Acoustical Feasibility Study;
7. Arborist Report;
8. Tree Preservation Plan;
9. Stormwater Management Plan;
10. Functional Servicing Report; and,
11. Phase 1 Environmental Site Assessment.



## 2.0 SITE AND SURROUNDINGS



Figure 2 - Aerial Photo



## 2.1 The Subject Properties

The subject properties are located on the west side of Confederation Parkway, between Dunbar Road (north) and Floradale Drive (south) in the City of Mississauga (See **Figure 2**).

They are located within the Cooksville neighbourhood, 1.3 km south of Cooksville GO Station, and 1.5 km north of the Queen Elizabeth Way (QEW). In terms of major streets, Dundas Street West is located to the north, Queensway West to the south, and Hurontario Street is located approximately 500 metres to the east.

The legal descriptions for the subject properties are:

- 2476 Confederation Pkwy: PLAN 500 LOT 30
- 2482 Confederation Pkwy: PLAN 500 LOT 29

The subject properties are rectangular in shape and have lot areas of approximately 725 sq.m. (2476) and 717 sq.m. (2482), for a total combined area of 1442 sq.m. Each lot is currently occupied by a single storey detached residential dwelling.



*Subject properties, 2476 and 2482 Confederation Parkway*





2476 and 2482 Confederation Parkway, looking north



2470 Confederation Parkway



2482 Confederation Parkway



2464 Confederation Parkway



2476 Confederation Parkway



2464 and 2470 Confederation Parkway



## 2.2 Surrounding Land Uses

### 2.2.1 Immediate Surroundings

The immediate land uses surrounding the subject properties along Confederation Pkwy are residential, with a mix of single detached dwellings as well as rental apartment buildings to the south, adjacent to Queensway West.

The subject properties back onto a stable residential neighbourhood to the **immediate west**, consisting of single detached and semi-detached dwellings.

Single detached dwellings are located to the **immediate north and east** of the subject properties, along Confederation Pkwy.

A small neighbourhood park (Floradale Park) is located **immediately south** of the subject properties, along the west side of Confederation Pkwy.



123 Dunbar Rd (northwest corner of Dunbar Rd and Confederation Pkwy), north of the subject properties



123 and 127 Dunbar Rd, north of the subject properties



Floradale Park, south of the subject properties



2487 Confederation Pkwy, east of the subject properties



## 2.2.2 General Surroundings

### North (east & west)

The areas **north** of the subject properties generally consists of stable residential neighbourhoods, several apartment building complexes, community amenities (parks, schools, and community centres), as well as commercial uses and plazas along Dundas St W and Hurontario St. There is also a large townhouse development (140 townhomes) in preconstruction along Confederation Pkwy and Dundas St W, approximately 300 metres north of the subject properties.

The Cooksville GO Station is located **further north**, between Confederation Pkwy and Hurontario St. West of the GO Station, along Hillcrest Ave and Confederation Pkwy, is an apartment complex consisting of four (4) twenty-two storey towers. Adjacent along the east side of Confederation Pkwy is a townhome development.

The Fairview neighbourhood is located north of the railway, and consists of single detached dwellings and various neighbourhood amenities.



99 Dunbar Road, east of the subject properties



King St W facing east, northeast of the subject properties



Looking north on Confederation Pkwy towards Dundas St W (from the subject properties)



Development under construction on the southeast corner of Confederation Pkwy and Dundas St W (90, 100, 110 Dundas St W)



## South (east & west)

The area **south** of the subject properties consist of single detached dwellings and rental apartment buildings with heights ranging between 6 and 20 storeys, along Confederation Pkwy and Paisley Blvd W.

Stable residential neighbourhoods are located **southwest** of the subject properties, comprised of semi-detached dwellings, single detached dwellings, newer townhome developments, as well as community amenities such as the Floradale Public School and Lummis Park.

Numerous medical facilities are located **further south** along Confederation Pkwy and the Queensway W, including the Mississauga Hospital (Trillium Health Partners) on the block southwest of the Queensway and Hurontario intersection.

The blocks between Queensway W and the Queen Elizabeth Way (QEW) are characterized by stable residential neighbourhoods consisting of single detached dwellings and mature tree canopies.



*Townhome development on Paisley Blvd W, southwest of the subject properties*



*Semi-detached dwellings on Antigua Rd, south of the subject properties*



*Mississauga Hospital, Trillium Health Partners (100 Queensway W), south of the subject properties*



*Apartment buildings along Confederation Pkwy, south of the subject properties*



## 2.3 Development Trends

Various planned development projects are located in close proximity to the subject properties and are at varying stages of construction and approval. These include high density mixed use and residential developments and a townhouse development.

The following section provides a high level overview of these projects and their updated status as per the City of Mississauga Development Applications Map (see **Figure 3**).



**Figure 3 - Nearby Development Applications**

### 2.3.1 Surrounding Proposals

#### **85-95 Dundas St W and 98 Agnes Street** (*in process*)

The proposed development is located on the north side of Dundas St W, east of Confederation Pkwy. A rezoning application has been submitted to permit an 18-storey mixed use building, consisting of 419 residential units and 385 m<sup>2</sup> of ground floor commercial.

#### **2444 Hurontario Street** (*in process*)

The proposed development is located on the west side of Hurontario St, south of Floradale Dr. The application proposes a 31-storey apartment building with 215 residential units and 3 live/work units.

#### **150 Paisley Boulevard West** (*withheld*)

The proposed development is located south of the subject properties, on the south west corner of Confederation Pkwy and Paisley Blvd W. The application proposes a 10-storey rental apartment building with 82 units and a 3-storey parking garage.

#### **2512, 2522, and 2532 Argyle Road** (*OMB appeal*)

The proposed development is located west of the subject properties, south of Dundas St W. A rezoning application is under appeal to permit 101 back-to-back stacked townhouses within 4 blocks on a private road.

#### **2560 & 2564 Confederation Parkway** (*approved*)

The proposed development is located north of the subject properties, on the east side of Confederation Pkwy, between Dundas St W and Dunbar Rd. A rezoning application has been submitted to permit the construction of four (4) three-storey live/work townhouses.

## 2.4 Transportation Network

As identified in the City of Mississauga Official Plan, the subject properties are located within an Intensification Area (500 metre radius of the Hurontario Intensification Corridor). The following sub-sections provide a thorough analysis of the subject properties' well-connected road and transit network.

### 2.4.1 Road Network

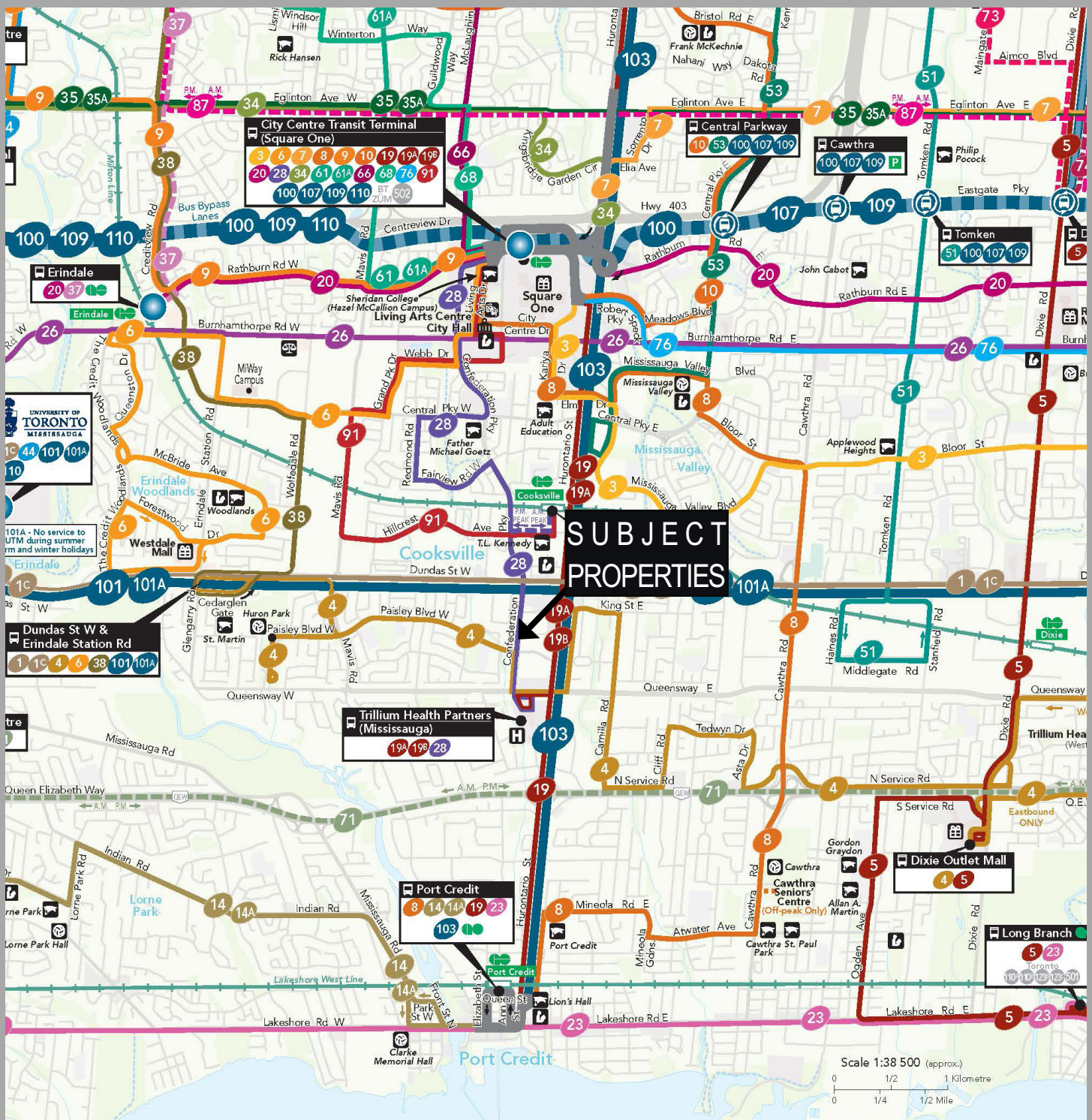
**Confederation Parkway** is classified as a Major Collector in the City's Official Plan (*Schedule 5 – Long Term Road Network*). It is a two-way street, transitioning from four lanes down to two lanes towards Queensway West with sidewalks on both sides. Confederation Parkway has a planned right-of-way width of 26 metres (*Schedule 8 – Designated Right-of-Way Widths*).

**Dundas Street West** is classified as an Arterial Road in the City's Official Plan (*Schedule 5 – Long Term Road Network*), as well as a Higher Order Transit Corridor and Intensification Corridor (*Schedule 6 – Long Term Transit Network*). Dundas Street is a two-way, four-lane street with sidewalks on both sides and a planned right-of-way width of 35 metres.

**Queensway West**, located south of the subject properties, is classified as a Regional Arterial in the City's Official Plan (*Schedule 5 – Long Term Road Network*). Queensway West has a planned right-of-way width of 35 metres.

**Hurontario Street**, located east of the subject properties, is classified as an Arterial in the City's Official Plan (*Schedule 5 – Long Term Road Network*). Hurontario Street has a planned right-of-way width of 35 metres.





**Figure 4 - Transit Map**

## 2.4.2 Transit Network

### Bus Services

The subject properties are well connected to existing and planned transit services (see **Figure 4**).

Bus service is provided along Confederation Pkwy by the **28 Confederation** route, as well as the **4 Sherway Gardens** route south of the subject properties. Other nearby routes include the **101** and **101A Dundas Express**, the **1** and **1C Dundas-Collegeway**, as well as the **103 Hurontario Express** and **19 Hurontario**.

**Route 28** is a local route with all week service which runs north-south along Confederation Pkwy from Sheridan College to the Hospital (Trillium Health Centre).

**Route 4** is also a local route, running east-west from Dundas St W and Erindale Station Rd (west), to Sherway Gardens and Dixie Outlet Mall (east).

**Routes 101** and **101A** are express routes providing all day service, operating east-west along Dundas St, from South Common Centre to the Islington and Kipling Subway Stations. In addition, **Routes 1** and **1C** provide local service along Dundas St.

All day bus express services are also provided by **Route 103**, running north-south along Hurontario St from the Brampton Gateway Terminal to the Port Credit GO Station. In addition, **Routes 19 (A and B)** provide local service along Hurontario.

### Train and Bus Rapid Transit Services

The **Cooksville GO Station** is located approximately 1.3 km north of the subject properties. The Cooksville GO Station is on the Milton line between Erindale Station and Dixie Station. The Milton line operates between Milton (Milton GO Station) and Downtown Toronto (Union Station). The line provides weekday rush-hour train service between Milton and Toronto and all stops along the way.

The Cooksville GO is connected to MiWay's 28 Confederation and 91 Hillcrest bus services. It will also include connections to the future **Hurontario Light Rail Transit (LRT) Line** (anticipated completion for the end of 2022).

The Cooksville GO Station is currently undergoing significant improvements, including a new station with a large public plaza, additional parking, new bus bays, and upgraded railway access via pedestrian tunnels (anticipated completion for the end of 2020).

The City of Mississauga and Metrolinx are currently planning the Dundas Street **Bus Rapid Transit (BRT)** through Metrolinx's regional transportation plan "The Big Move", providing an important east-west link to current and future transit corridors in Mississauga. Furthermore, the recently approved Dundas Connects Master Plan recommends the BRT as the transit solution to the busiest east-west corridors across the City. The proposed BRT will have 20 stops including three terminals (Kipling Mobility Hub, UTM, and Ridgeway Drive).



## 3.0 POLICY AND REGULATORY CONTEXT



### 3.1 Overview

The following provides an overview of the ways in which the proposed development is supportive of provincial, regional and local policy directions, as set out in the Provincial Policy Statement, the Growth Plan for the Greater Golden Horseshoe, the Region of Peel Official Plan and the City of Mississauga Official Plan.

### 3.2 Provincial Policy Statement (2020)

The Provincial Policy Statement (PPS) provides long-term policy direction on matters of provincial interest related to land use planning and development decisions. As required under Section 3(5) of the Planning Act, all land use planning decisions shall be consistent with the PPS.

The PPS provides direction for establishing healthy, liveable and safe communities through efficient development and land use patterns, and the provision of appropriate ranges of land uses and densities (Policy 1.1.1). The PPS contains several policies that support intensification in built-up urban areas.

Policy 1.1.3.2 promotes densities and a mix of land uses that make efficient use of land resources, land, infrastructure and public service facilities, which are supported by existing and future transit.

The PPS directs planning authorities to identify appropriate locations for intensification and redevelopment that can support existing or planned infrastructure and public service facilities (Policy 1.1.3.3). Policy 1.1.3.4 further states that appropriate development standards facilitating intensification and redevelopment are promoted, while avoiding or mitigating risks to public health and safety.

With respect to housing, Section 1.4.3 promotes a range of housing types and appropriate densities to meet the needs

of current and future residents by, among other elements, promoting *“densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;”* (Policy 1.4.3.d).

The PPS promotes the efficient use of infrastructure with a significant emphasis on public transit (Sections 1.6.1, 1.6.3 and 1.6.7). Specifically, Policy 1.6.7.4 provides that development should be in line with appropriate densities and mixed uses, while minimizing the length and number of vehicular trips, in support of current and future transit and active transportation networks.

The PPS identifies energy conservation, air quality and climate change as important topics to address in protecting the environment and building healthy, resilient communities. Policy 1.8.1 supports energy conservation and efficiency, reduced greenhouse gas emissions, and climate change adaptation through land use and development patterns that promote compact urban form and a structure of nodes and corridors; active forms of transportation and public transit; and improve the mix of housing and employment uses to mitigate traffic congestion and commute lengths.



### 3.3 A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2020)

Revisions to the Growth Plan for the Greater Golden Horseshoe (the “Growth Plan”) were prepared and approved in 2020. As required under Section 3(5) of the Planning Act, all decisions relating to planning shall conform to the Growth Plan.

The subject properties are located within a designated Urban Growth Centre (Mississauga City Centre Urban Growth Centre) on Schedule 2 - Places to Grow Concept and in Section 3 of the Growth Plan (see **Figure 5**). Urban Growth Centres are intended to accommodate a significant share of population and employment growth; to accommodate and support major transit infrastructure; to serve as high-density major employment centres; and to act as focal areas for investment in institutional and region-wide public services (Policy 2.2.3).



**Figure 5** - Mississauga City Centre Urban Growth Centre - Growth Plan for the Greater Golden Horseshoe

The subject properties are in close proximity to several existing and future major transit lines (e.g. Dundas St BRT, Hurontario St LRT, Cooksville GO Station). Section 2.1 of the Growth Plan promotes mixed use intensification located within built-up urban areas in proximity to transit:

*“To support the achievement of complete communities that are healthier, safer, and more equitable, choices about where and how growth occurs in the GGH need to be made carefully. Better use of land and infrastructure can be made by directing growth to settlement areas and prioritizing intensification, with a focus on strategic growth areas, including urban growth centres and major transit station areas, as well as brownfield sites and greyfields. Concentrating new development in these areas provides a focus for investments in transit as well as other types of infrastructure and public service facilities to support forecasted growth, while also supporting a more diverse range and mix of housing options.”*

Section 2.1 of the Growth Plan further emphasizes transit as a first priority for major transportation investments:

*“...seeks to align transit with growth by directing growth to major transit station areas and other strategic growth areas, including urban growth centres, and promoting transit investments in these areas. To optimize provincial investments in higher order transit, this Plan also identifies priority transit corridors and the Province expects municipalities to complete detailed planning for major transit station areas on these corridors to support planned service levels.”*



The subject properties are located within a delineated built-up settlement area immediately adjacent to an Intensification Corridor (Dundas St W and Hurontario St) and a Major Transit Station Area (Cooksville GO Station). Intensification corridors are designated areas of intensification along arterials (such as Dundas St and Hurontario St), regional arterials, major roads, or higher order transit corridors. Higher order transit operates in dedicated rights-of-way, allowing for higher levels of speed and reliability (i.e. light rail, inter-city rail, subways, and streetcars).

Section 2.2.2 of the Growth Plan seeks to accommodate projected population and employment growth within delineated built-up areas by the year 2051. Schedule 3 of the Growth Plan forecasts a population of 2,280,000 residents and 1,070,000 jobs for the Region of Peel by 2051. Municipalities are required to develop strategies to meet their respective intensification targets by identifying strategic growth areas and recognizing them as a key focus for development (Policy 2.2.2(3)).

For the reasons set out above, it is our opinion that the proposal conforms with the goals and objectives of the Growth Plan. In particular, the proposal is aligned with the policies supportive of growth and intensification within built-up areas and Urban Growth Centres, adjacent to Intensification Corridors, and in proximity to existing and planned major transit lines.

### 3.4 Region of Peel Official Plan (2018 Consolidation)

The subject properties are located within the **Conceptual Urban Growth Centre** (*Schedule D – Regional Structure*). As per Section 5.3.3, the Urban Growth Centres designation outlines where development and intensification in the Region of Peel should occur, supporting sustainable development with intensification through compact form and mixed uses. The plan states the following as an objective (5.3.3.1.1):

*“To achieve Urban Growth Centres that are linked by public transit, and include a range and mix of high intensity compact forms and activities while taking into account the characteristics of existing communities and services.”*

Both Dundas St and Hurontario St are designated as **Major Roads** in the Region's road network (*Schedule E - Major Road Network*). Furthermore, Dundas St and Hurontario St are designated as Other Rapid Transit Corridor (*Schedule G - Rapid Transit Corridors*), intended to accommodate transit-supportive development densities and patterns.

The subject properties are also located in proximity to the Cooksville GO mobility hub (Gateway) identified at the Dundas and Hurontario intersection (*Schedule G*). Gateway hubs are major transit station areas located at intersections between two or more current or planned rapid transit lines.

In reference to the inter and intra-regional transit network, the Plan states the following as an objective (5.9.5.1.4):

*“To support and encourage transit-supportive development densities and patterns, particularly along rapid transit corridors and at designated nodes such as transit terminals, urban growth centres, GO stations and mobility hubs.”*

Policy 5.9.5.2.10 further provides that planning should encourage the *“intensification of residential and non-residential development at nodes and mobility hubs and along corridors to support a higher level of transit service and other sustainable modes.”*

Figure 3 in the Region of Peel's Official Plan identifies the subject properties within the Credit River watershed boundaries. As per Section 2.2.4, the natural boundaries of watersheds contribute to an ecosystem approach for the management of natural resources. The policies outlined in subsection 2.2.4.1 highlight the Peel Region's mandate to work jointly with the conservation authorities and the neighbouring municipalities to promote and participate in watershed and subwatershed plans.



### 3.5 City of Mississauga Official Plan (2019 Consolidation)

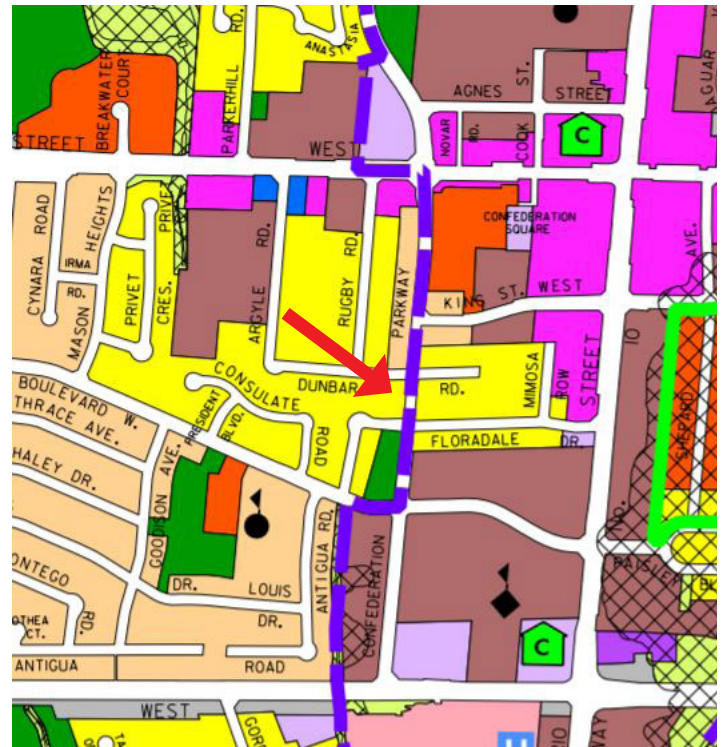
The subject properties are designated as **Neighbourhood** in the City of Mississauga Official Plan (*Schedule 1 – Urban System*).

Chapter 5 outlines the City’s growth management strategy. In the context of Mississauga’s anticipated growth, Section 5.1 of the Plan recognizes that:

*“Encouraging compact, mixed use development in appropriate locations will provide greater opportunities to live and work in Mississauga and reduce the need for extensive travel to fulfill the needs of day-to-day living. Directing growth to locations with existing or planned higher order or express transit service and enhancing opportunities for walking and cycling will allow for competitive alternatives to vehicular travel, which will minimize impacts on our environment and promote public health.”*

Furthermore, Schedule 10 designates the subject properties as **Residential Low Density I**. The subject properties are on the boundary of the **Downtown** (designated area is to the east of Confederation Parkway) (See **Figure 6**). Section 11.2.5.3 of the Plan provides that lands designated Residential Low Density I permit detached, semi-detached, and duplex dwellings.

Furthermore, Map 16-1 (*City Structure – Neighbourhoods*) identifies the subject properties within the **Cooksville Neighbourhood Character Area**, immediately adjacent to the Downtown Cooksville and Downtown Hospital Character Areas on the east side of Confederation Parkway (see **Figure 7**).



**Figure 6 - Schedule 10 - Land Use Designations, City of Mississauga Official Plan**

#### LAND USE DESIGNATIONS

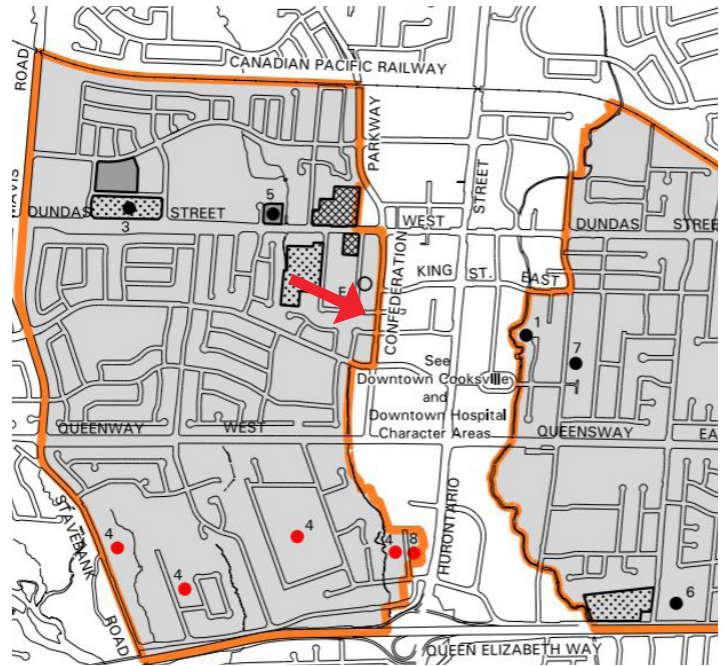
<span style="display:inline-block; width:15px; height:15px; background-color:yellow; border:1px solid black;"></span> Residential Low Density I	<span style="display:inline-block; width:15px; height:15px; background-color:lightblue; border:1px solid black;"></span> Airport
<span style="display:inline-block; width:15px; height:15px; background-color:orange; border:1px solid black;"></span> Residential Low Density II	<span style="display:inline-block; width:15px; height:15px; background-color:lightpink; border:1px solid black;"></span> Institutional
<span style="display:inline-block; width:15px; height:15px; background-color:red; border:1px solid black;"></span> Residential Medium Density	<span style="display:inline-block; width:15px; height:15px; background-color:green; border:1px solid black;"></span> Public Open Space
<span style="display:inline-block; width:15px; height:15px; background-color:brown; border:1px solid black;"></span> Residential High Density	<span style="display:inline-block; width:15px; height:15px; background-color:darkgreen; border:1px solid black;"></span> Private Open Space
<span style="display:inline-block; width:15px; height:15px; background-color:pink; border:1px solid black;"></span> Mixed Use	<span style="display:inline-block; width:15px; height:15px; background-color:lightgreen; border:1px solid black;"></span> Greenlands
<span style="display:inline-block; width:15px; height:15px; background-color:purple; border:1px solid black;"></span> Convenience Commercial	<span style="display:inline-block; width:15px; height:15px; background-color:lightcyan; border:1px solid black;"></span> Parkway Belt West
<span style="display:inline-block; width:15px; height:15px; background-color:blue; border:1px solid black;"></span> Motor Vehicle Commercial	<span style="display:inline-block; width:15px; height:15px; background-color:grey; border:1px solid black;"></span> Utility
<span style="display:inline-block; width:15px; height:15px; background-color:lightpurple; border:1px solid black;"></span> Office	<span style="display:inline-block; width:15px; height:15px; background: repeating-linear-gradient(45deg, transparent, transparent 2px, black 2px, black 4px); border:1px solid black;"></span> Special Waterfront
<span style="display:inline-block; width:15px; height:15px; background-color:cyan; border:1px solid black;"></span> Business Employment	<span style="display:inline-block; width:15px; height:15px; border:2px dashed black;"></span> Partial Approval Area
<span style="display:inline-block; width:15px; height:15px; background-color:darkblue; border:1px solid black;"></span> Industrial	

The subject properties are immediately adjacent to the **Downtown Cooksville** and **Downtown Hospital Character Areas**.

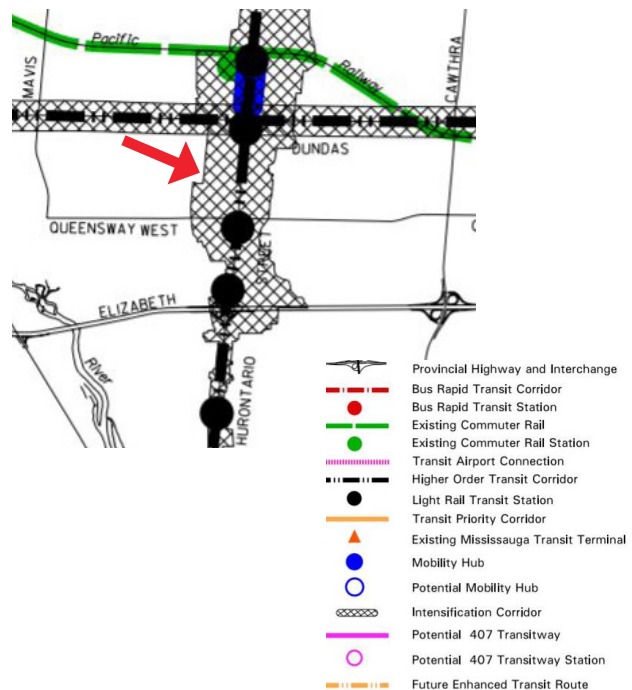
In regards to the overall character of the adjacent Downtown Cooksville Character Area, Policy 12.4.1.2 highlights that sections of Hurontario Street and Dundas Street should function as the neighbourhood's focal points, *"having a strong sense of place and main street character with active mixed use building frontages and highly pedestrianized nature. These street frontages should reinforce a distinctive, quality image with high standards in built form, landscaping and related pedestrian amenities."*

The subject properties are adjacent to the Hurontario Street Intensification Corridor to the east, and are also in close proximity to the Dundas Street Intensification Corridor to the north. Both Hurontario and Dundas Street are designated as Higher Order Transit Corridors on Schedule 6 - Long Term Transit Network, in the City's Official Plan (see **Figure 8**). The Dundas and Hurontario intersection northeast of the subject properties is identified as a Light Rail Transit Station (in regards to the planned LRT on Hurontario Street) and is designated as a Mobility Hub.

Chapter 8 of the City of Mississauga Official Plan outlines objectives and policies in relation to the creation of a multi-modal city. The policies speak to the evolving character of Mississauga as it transitions from a more suburban, vehicle oriented built form to an urban municipality, promoting a range of transportation modes (e.g., transit, cycling, walking). The Plan emphasizes that the successful implementation of a multi-modal transportation system will go hand-in-hand with the creation of compact, complete communities well served by transit.



**Figure 7 - Map 16-6: Cooksville Neighbourhood Character Area, City of Mississauga Official Plan**



**Figure 8 - Schedule 6 - Long Term Transit Network, City of Mississauga Official Plan**



### 3.6 City of Mississauga Zoning By-law 0225-2007

The subject properties are subject to the City of Mississauga Zoning By-law 0225-2007. This By-law was enacted and passed on June 20th 2007, and approved by the Ontario Municipal Board Order No. 2452 on September 10 2007. As provided on Map 15 of the City-wide Zoning By-law, the subject properties are zoned Residential - Detached Dwellings R3 (See **Figure 9**).

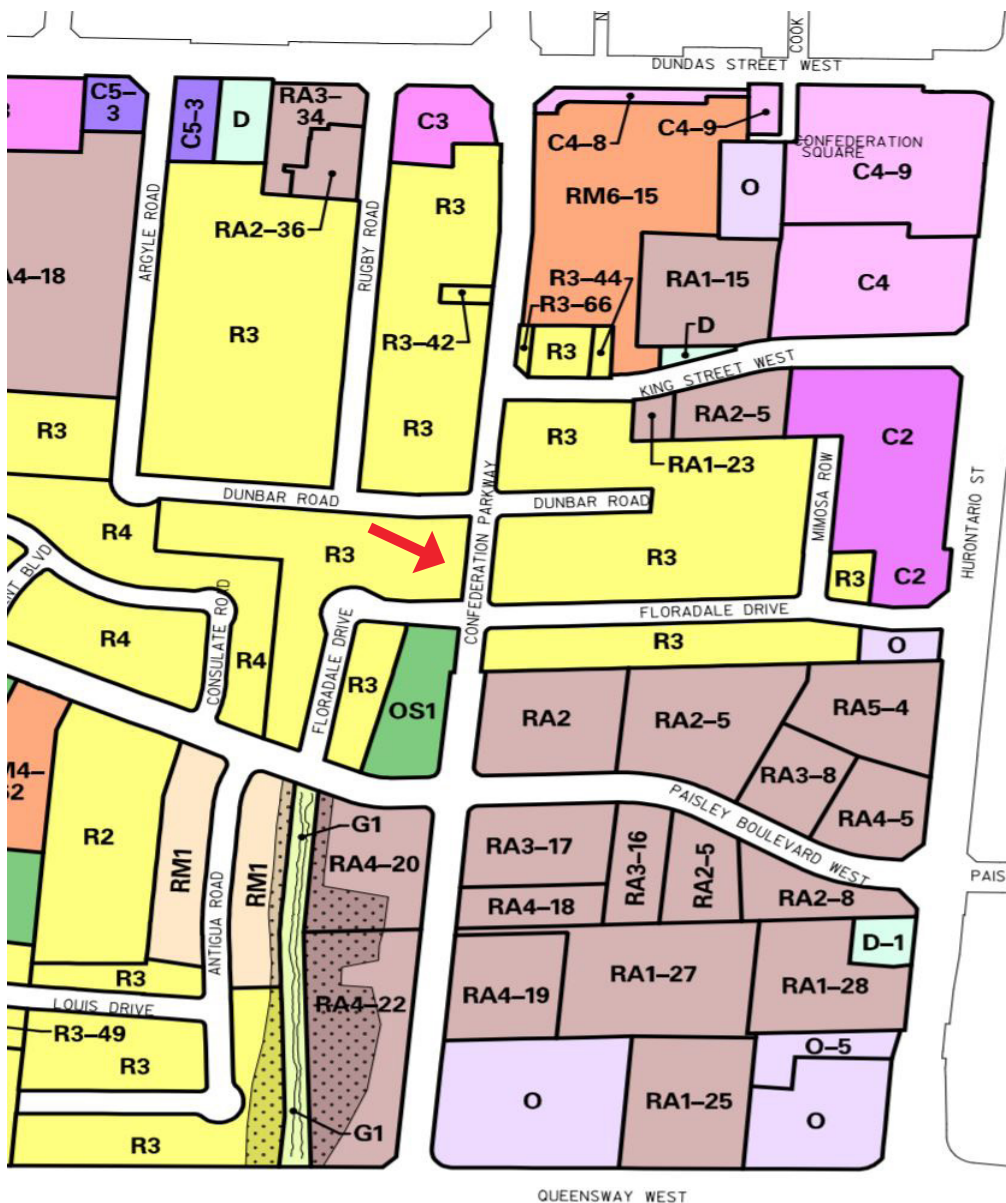


Figure 9 - City of Mississauga Zoning By-law No. 0225-2007 (Revised July 31, 2017)

As stated under Section 4.2 of the Zoning By-law, the R3 zone is subject to the following permitted uses and regulations (in addition to the permitted accessory buildings and structures):

Column	A	B	C	D	E	F
Line 1.0	ZONES	R1	R2	R3	R4	R5
<b>PERMITTED USES</b>						
2.0	RESIDENTIAL					
2.1	Detached Dwelling	✓ <sup>(1)</sup>	✓ <sup>(1)</sup>	✓ <sup>(1)</sup>	✓ <sup>(1)</sup>	✓ <sup>(1)</sup>
<b>ZONE REGULATIONS•</b>						
3.0	MINIMUM LOT AREA					
3.1	Interior lot	750 m <sup>2</sup>	695 m <sup>2</sup>	550 m <sup>2</sup>	365 m <sup>2</sup>	295 m <sup>2</sup>
3.2	Corner lot	835 m <sup>2</sup>	810 m <sup>2</sup>	720 m <sup>2</sup>	500 m <sup>2</sup>	415 m <sup>2</sup>
4.0	MINIMUM LOT FRONTAGE					
4.1	Interior lot	22.5 m	18.0 m	15.0 m	12.0 m	9.75 m
4.2	Corner lot	22.5 m	21.0 m	19.5 m	16.5 m	13.5 m
5.0	MAXIMUM LOT COVERAGE	25%	30%	35%	40%	40%
6.0	MINIMUM FRONT YARD					
6.1	Interior lot	9.0 m <sup>(2)(7)</sup>	9.0 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	6.0 m <sup>(2)</sup>	4.5 m <sup>(2)</sup>
6.2	Corner lot	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	6.0 m <sup>(2)</sup>	6.0 m <sup>(2)</sup>	4.5 m <sup>(2)</sup>
6.3	Front garage face - interior lot (0379-2009)	<sup>(8)</sup>	<sup>(8)</sup>	<sup>(8)</sup>	<sup>(8)</sup>	6.0 m
6.4	Front garage face - corner lot (0379-2009)	<sup>(8)</sup>	<sup>(8)</sup>	<sup>(8)</sup>	<sup>(8)</sup>	6.0 m
7.0	MINIMUM EXTERIOR SIDE YARD	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	6.0 m <sup>(2)</sup>	4.5 m <sup>(2)</sup>	4.5 m <sup>(2)</sup>
7.1	Front garage face (0379-2009)	<sup>(9)</sup>	<sup>(9)</sup>	<sup>(9)</sup>	6.0 m <sup>(2)</sup>	6.0 m <sup>(2)</sup>
8.0	MINIMUM INTERIOR SIDE YARD					
8.1	Interior lot	1.8 m on one side of the lot and 4.2 m on the other side <sup>(2)</sup>	1.8 m + 0.61 m for each additional storey or portion thereof above one (1) storey <sup>(2)</sup>	1.2 m + 0.61 m for each additional storey or portion thereof above one (1) storey <sup>(2)</sup>	1.2 m <sup>(2)</sup>	1.2 m on one side of the lot and 0.61 m on the other side <sup>(2)</sup>

Table 1 - General Residential (R3) Zone Regulations



Column	A	B	C	D	E	F
Line 1.0	ZONES	R1	R2	R3	R4	R5
Table 4.2.1 continued from previous page						
8.2	Corner lot	3.0 m <sup>(2)</sup>	3.0 m <sup>(2)</sup>	1.2 m + 0.61 m for each additional storey above one (1) storey <sup>(2)</sup>	1.2 m <sup>(2)</sup>	1.2 m <sup>(2)</sup>
9.0	MINIMUM REAR YARD					
9.1	Interior lot	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>
9.2	Corner lot	3.0 m <sup>(2)</sup>	3.0 m <sup>(2)</sup>	3.0 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>	7.5 m <sup>(2)</sup>
10.0	MAXIMUM HEIGHT	10.7 m	10.7 m	10.7 m	10.7 m	10.7 m
11.0	ENCROACHMENTS, PROJECTIONS AND SETBACKS					
11.1	Maximum projection of a garage beyond either the main front entrance or beyond the main entry feature where provided	n/a	n/a	n/a	n/a	2.5 m
11.2	Where a main entry feature has been provided, the maximum projection of a garage beyond a main front entrance	n/a	n/a	n/a	n/a	5.0 m
11.3	For a detached dwelling more than one (1) storey in height, where the garage projects beyond the main front entrance, a minimum of 75% of the width of the garage, measured from the inside face of the garage walls, shall be covered by a second storey which may be set back a maximum of 2.5 m from the front garage face	n/a	n/a	n/a	n/a	✓
12.0	ATTACHED GARAGE, PARKING AND DRIVEWAY					
12.1	Attached garage	Permitted <sup>(3)</sup>	Permitted <sup>(3)</sup>	Permitted <sup>(3)</sup>	Permitted <sup>(3)</sup>	Required <sup>(3)</sup>
12.2	Minimum parking spaces	✓ <sup>(4) (5)</sup>	✓ <sup>(4) (5)</sup>	✓ <sup>(4) (5)</sup>	✓ <sup>(4) (5)</sup>	✓ <sup>(4) (5)</sup>
12.3	Maximum driveway width (0190-2014)	Width of garage door opening(s) plus 2.0 m up to a maximum of 8.5 m; if no garage door then maximum width of 6.0 m	Width of garage door opening(s) plus 2.0 m up to a maximum of 6.0 m; if no garage door then maximum width of 6.0 m	Width of garage door opening(s) plus 2.0 m up to a maximum of 6.0 m; if no garage door then maximum width of 6.0 m	Width of garage door opening(s) plus 2.0 m up to a maximum of 6.0 m; if no garage door then maximum width of 6.0 m	Width of garage door opening(s) plus 2.0 m up to a maximum of 6.0 m; if no garage door then maximum width of 6.0 m
12.4	Minimum landscaped soft area in the yard containing the driveway (0190-2014)	40% of the front yard and/or exterior side yard	40% of the front yard and/or exterior side yard	40% of the front yard and/or exterior side yard	40% of the front yard and/or exterior side yard	30% of the front yard and/or exterior side yard

Table 2 - General Residential (R3) Zone Regulations

### 3.6.1 Zoning By-law Amendment Justification

The proposed development for two semi-detached dwellings at 2476 and 2482 Confederation Parkway requires an amendment to the City of Mississauga Zoning By-law 0225-2007.

Based on the policy review and analysis of the area, the nature and scale of the proposed development is appropriate and consistent with the neighbourhood character and context and represents sound planning. As per the Regional Official Plan, the subject properties are located within the Conceptual Urban Growth Centre, where development and intensification is intended to occur. Furthermore, the subject properties are designated for residential uses (Residential Low Density I) in the City of Mississauga Official Plan, whereby semi-detached dwellings are listed as a permitted use.

The proposed redevelopment of the subject properties into two (2) three-storey semi-detached dwellings provide an appropriate level of intensification for the area, provide a sensitive transition to the neighbourhood to the west, and result in minimal shadow implications on adjacent properties. The proposed semis are in keeping with the neighbourhood character, noting that there are semi-detached dwellings in close proximity to the subject properties.

Additionally, provided its service as a Major Collector and proximity to two designated Arterials and Intensification Corridors (Dundas Street West and Hurontario Street), Confederation Parkway is an appropriate location for intensification.

In summary, the Zoning By-law Amendment proposes to change the existing R3 (Detached Dwellings) Zone to the RM2 (Semi-Detached Dwellings) Zone.

## 3.7 Additional Considerations

### 3.7.1 Dundas Connects Master Plan

The City of Mississauga has developed a Master Plan for the entire Dundas Street Corridor, measuring 19.5 km in length and 4 km in width, from Oakville in the west, to Etobicoke in the east. The Dundas Connects Master Plan was endorsed by the City's Planning and Development Committee on June 11th 2018. In February 2020, the City implemented an amendment to the Official Plan (MOPA 106) to implement the Dundas Connects recommendations for road widening

The Plan aims to integrate transportation and land use planning to address both current and future demands along the Corridor through the implementation of higher order transit and sustainable transit-oriented development.

Section 2.4.2.4 of the Master Plan identifies the Cooksville Focus Area as a Mobility Hub given the proximity to the Hurontario LRT, the planned Dundas BRT, as well as the Cooksville GO Station. The subject properties are in very close proximity to the Focus Area Boundary north of Dunbar Road.

### 3.7.2 Hurontario Master Plan (2010)

Completed in October of 2010, the Hurontario Master Plan provides a comprehensive analysis of the master plan for the corridor in preparation of the LRT, including an implementation plan.

### 3.7.3 Metrolinx 2041 Regional Transportation Plan (2018)

Aligned with the guiding principles in Ontario's Growth Plan (2020), the Metrolinx 2041 Regional Transportation Plan sets out the vision and strategy for accommodating the transportation needs of current and future populations and



goods in the Greater Toronto and Hamilton Area (GTHA). The Plan identifies Bus Rapid Transit on Dundas Street across the City of Mississauga as a key link in an integrated, multi-modal and regional transit system.

### **3.7.4 Making Room for the Middle: A Housing Strategy for Mississauga**

In addressing the Region's lack of affordable housing options, the City of Mississauga has released a plan entitled "Making Room for the Middle" (2017) to provide housing stock for middle-income residents. The built-form, heights, and densities proposed in the Dundas Connects Master Plan support the retention of existing affordable housing stock (publicly subsidized and private, market rental) as well as the creation of affordable housing within new higher density developments, as proposed for the subject site.

Furthermore, the strategy supports the Provincial Growth Plan and the Region of Peel Official Plan, which encourage the development of affordable housing near major transit station areas and priority transit corridors.

## 4.0 PROPOSAL



## 4.1 Overview

The proposal seeks to redevelop the two (2) existing single storey residential dwellings located at 2476 and 2482 Confederation Parkway into two (2) three-storey semi-detached dwellings. The lots will be reconfigured to front onto Dunbar Road.

The semi-detached dwellings will each be comprised of two (2) units, for a total of four (4) dwellings. Each unit will consist of three above-ground levels and one basement level with an integrated garage at the front of the house providing one (1) parking space per unit.

A brief summary of the proposed development configuration is outlined below. Detailed site plan statistics are provided in Table 3.

### Lot Area

#### *2476 Confederation Pkwy*

- UNIT A (Parts 2 & 7): 305.3 m<sup>2</sup>
- UNIT B (Parts 1 & 6): 304.8 m<sup>2</sup>

#### *2482 Confederation Pkwy*

- UNIT A (Parts 4 & 9): 418.1 m<sup>2</sup>
- UNIT B (Parts 3 & 8): 305.3 m<sup>2</sup>

### Gross Floor Area

#### *2476 Confederation Pkwy*

- UNIT A (Parts 2 & 7): 290.36 m<sup>2</sup>
- UNIT B (Parts 1 & 6): 290.36 m<sup>2</sup>

#### *2482 Confederation Pkwy*

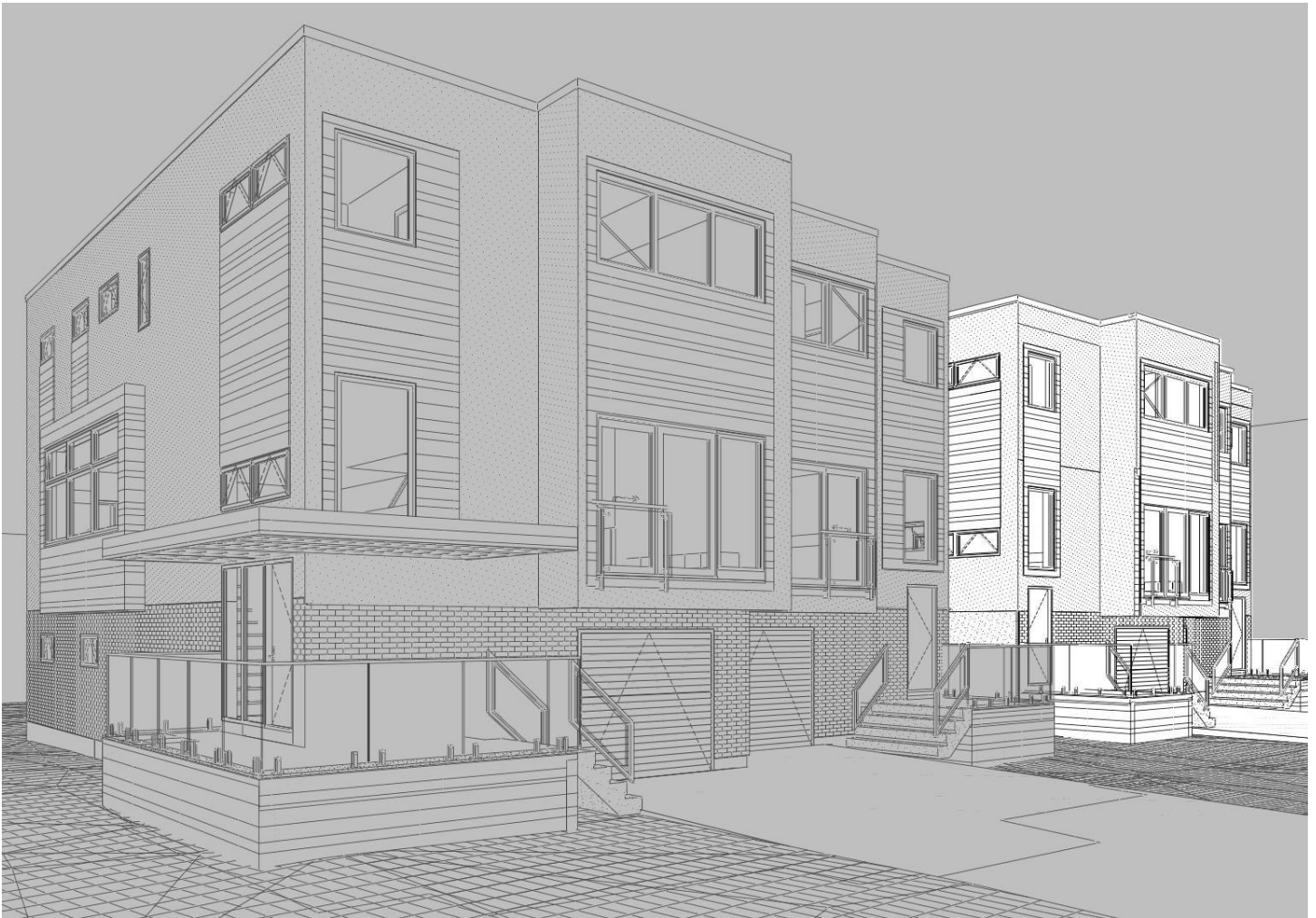
- UNIT A (Parts 4 & 9): 292.3 m<sup>2</sup>
- UNIT B (Parts 3 & 8): 290.36 m<sup>2</sup>

### Pedestrian and Vehicular Access

Fronting on Dunbar Road, a paved driveway will connect vehicles and front entrances to the public right-of-way.

### 4.1.1 Development Renderings

The development renderings have been prepared to meet the City of Mississauga's RM2 Zone specifications for semi-detached dwellings.



*Figure 10 - Development Rendering - Semi-Detached Dwellings with 2746 highlighted*





Figure 11 - Development Rendering - Context (for illustrative purposes)

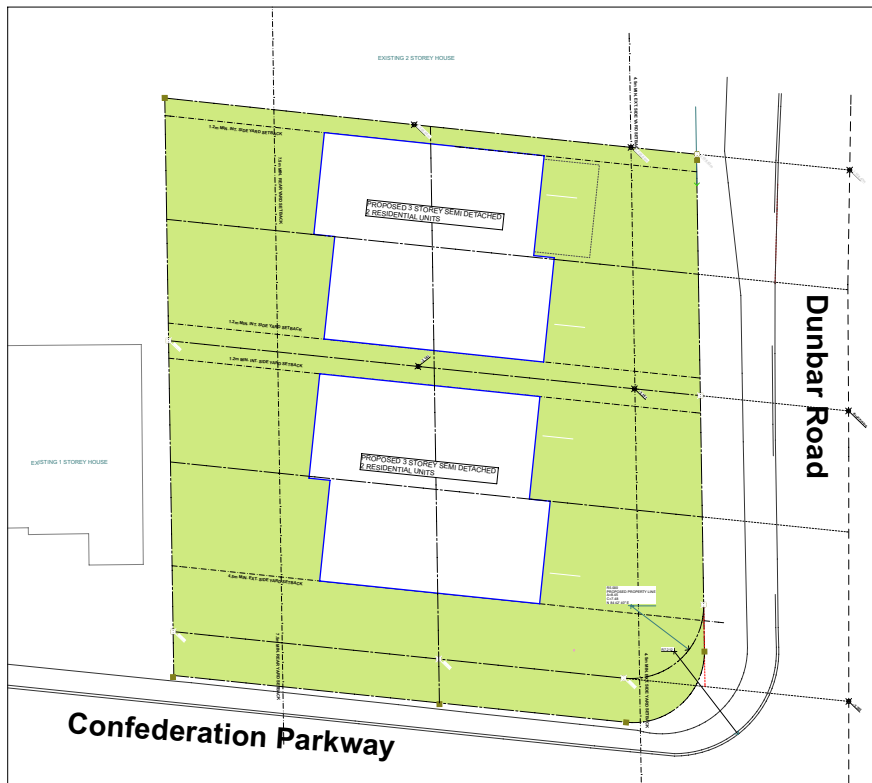


Figure 12 - Building Footprint (for illustrative purposes)

## 4.1.2 Site Statistics

PROJECT STATISTICS			
LOT ( 29 AND 30 )		PARTS 2 & 7 (UNIT A)	PARTS 1 & 6 (UNIT B)
ADDRESS:		2476 Confederation	2476 Confederation
ZONING:		RM2	RM2
LOT AREA ( PARTS 1,2,6,7)		305.3 m²	304.8 m²
LOT FRONTAGE:		8.35m	8.35m
BUILDING HEIGHT			
AVERAGE GRADE:		108.57m	108.57m
HEIGHT TO HIGHEST RIDGE:		9.57m	9.57m
GROSS FLOOR AREA			
	EXISTING	PROPOSED	PROPOSED
GROUND FLOOR:	0m²	75.23m²	75.23m²
SECOND FLOOR:	0m²	109.60m²	109.60m²
THIRD FLOOR:	0m²	105.53m²	105.53m²
TOTAL GFA:		290.36m²	290.36m²
LOT COVERAGE			
	EXISTING	PROPOSED	PROPOSED
DWELLING FOOTPRINT:	0m²	111.86m²	111.86m²
PORCH:	0m²	8.44m²	8.44m²
DECK: (>0.6m HEIGHT AND 10m²)	0m²	0m²	0m²
OTHER: (BALCONY PROJECTIONS BEYOND DWELLING FOOTPRINT)	0m²	8.76m²	8.76m²
TOTAL LOT COVERAGE:		129.01m	129.01m
TOTAL LOT COVERAGE (%)		42%	42%
AVERAGE GRADE CALCULATIONS			
PROPERTY SIDE LINES POINTS		E	108.45m
		F	108.65m
		G	108.55m
		H	108.54m
		I	108.47m
		J	108.62m
		K	108.66m
		L	108.64m
AVERAGE GRADE:		108.57m	
LANDSCAPED OPEN SPACE			
FRONT YARD AREA:		86.6m²	89.68m²
HARD SURFACES AREA - PORCH:		9.33m²	9.33m²
HARD SURFACES AREA - DRIVEWAY:		35.23m²	39.28m²
TOTAL LANDSCAPED OPEN SPACE (FRONT YARD):		42.04m² (49%)	41.07m² (46%)
SIDE YARD AREA:		18.16m²	18.00m²
HARD SURFACES AREA:		0m²	0m²
TOTAL LANDSCAPED OPEN SPACE (SIDE YARD):		18.16m² (100%)	18.00m² (100%)
REAR YARD AREA:		92.38m²	87.58m²
HARD SURFACES AREA:		0m²	0m²
TOTAL LANDSCAPED OPEN SPACE (REAR YARD):		92.38m² (100%)	87.58m² (100%)

PROJECT STATISTICS					
LOT:		PARTS 4,9 (UNIT A)		PARTS 3,8 (UNIT B)	
ADDRESS:		2482 Confederation		2482 Confederation	
ZONING:		RM2		RM2	
LOT AREA:		418.1 m <sup>2</sup>		305.3m <sup>2</sup>	
LOT FRONTAGE:		14.41m		8.35m	
BUILDING HEIGHT					
AVERAGE GRADE:		108.57m		108.57m	
HEIGHT TO HIGHEST RIDGE:		9.57m		9.57m	
GROSS FLOOR AREA					
		EXISTING	PROPOSED	PROPOSED	
GROUND FLOOR:		0m <sup>2</sup>	75.23m <sup>2</sup>	75.23m <sup>2</sup>	
SECOND FLOOR:		0m <sup>2</sup>	111.54m <sup>2</sup>	109.60m <sup>2</sup>	
THIRD FLOOR:		0m <sup>2</sup>	105.53m <sup>2</sup>	105.53m <sup>2</sup>	
TOTAL GFA:			292.3m <sup>2</sup>	290.36m <sup>2</sup>	
LOT COVERAGE					
		EXISTING	PROPOSED	PROPOSED	
DWELLING FOOTPRINT:		0m <sup>2</sup>	113.86m <sup>2</sup>	111.86m <sup>2</sup>	
PORCH:		0m <sup>2</sup>	16.09m <sup>2</sup>	8.44m <sup>2</sup>	
DECK: (>0.6m HEIGHT AND 10m <sup>2</sup> )		0m <sup>2</sup>	0m <sup>2</sup>	0m <sup>2</sup>	
OTHER: (BALCONY PROJECTIONS BEYOND DWELLING FOOTPRINT)		0m <sup>2</sup>	8.76m <sup>2</sup>	8.76m <sup>2</sup>	
TOTAL LOT COVERAGE:			138.71m	129.01m	
TOTAL LOT COVERAGE (%)			33%	42%	
AVERAGE GRADE CALCULATIONS					
PROPERTY SIDE LINES POINTS			A	108.42m	
			B	108.62m	
			C	108.70m	
			D	108.62m	
			E	108.45m	
			F	108.65m	
			G	108.55m	
			H	108.54m	
AVERAGE GRADE:				108.57m	
LANDSCAPED OPEN SPACE					
FRONT YARD AREA:			123.05m <sup>2</sup>	95.16m <sup>2</sup>	
HARD SURFACES AREA - PORCH:			11.51m <sup>2</sup>	9.33m <sup>2</sup>	
HARD SURFACES AREA - DRIVEWAY:			39.52m <sup>2</sup>	39.29m <sup>2</sup>	
TOTAL LANDSCAPED OPEN SPACE (FRONT YARD):			72.02m <sup>2</sup> (59%)	46.54m <sup>2</sup> (49%)	
SIDE YARD AREA:			68.40m <sup>2</sup>	18.33m <sup>2</sup>	
HARD SURFACES AREA:			4.58m <sup>2</sup>	0m <sup>2</sup>	
TOTAL LANDSCAPED OPEN SPACE (SIDE YARD):			63.82m <sup>2</sup> (93%)	18.33m <sup>2</sup> (100%)	
REAR YARD AREA:			120.22m <sup>2</sup>	83.66m <sup>2</sup>	
HARD SURFACES AREA:			0m <sup>2</sup>	0m <sup>2</sup>	
TOTAL LANDSCAPED OPEN SPACE (REAR YARD):			120.22m (100%)	83.66m <sup>2</sup> (100%)	

Table 3 - Site Plan Statistics



## 4.2 Required Approvals

A Zoning By-law Amendment is required to permit the redevelopment of the subject properties to accommodate two (2) three-storey semi-detached dwellings. The subject properties are zoned as R3 (Residential - Detached Dwellings) in the City of Mississauga Zoning By-law (0225-2007). Semi-detached dwellings are not included within the R3 permitted uses.

The proposed Zoning By-law Amendment will revise the City's Zoning By-law to allow for the development of two three-storey semi-detached dwellings, which will replace two existing single storey dwellings. In summary, a residential zoning change from a R3 Zone to a RM2 Zone is proposed, as to allow for semi-detached dwellings. The Draft Zoning By-law Amendment seeks to adopt the RM2 zoning provisions, with no site specific exceptions.

The proposed amendment includes:

### *Residential Zoning Designation Change:*

- To change the residential zoning designation from a R3 Zone to a RM2 Zone.

Based on the pre-consultation meeting with the Development Application Review Committee (DARC) on October 17, 2018 and as part of the rezoning application, several technical studies have been completed in support of the proposed development, including:

- A Noise Impact Study;
- An Arborist Report and Tree Preservation Plan (consolidated report);
- A Functional Servicing and Stormwater Management Report (consolidated report); and,
- A Phase 1 Environmental Site Assessment.

In addition to the Submission Requirements Checklist (see **Appendix A**), the completed application form, as well as the additional supporting materials identified by the City through the pre-consultation process, the Zoning By-law Amendment application for 2476-2482 Confederation Parkway fulfills all requirements for a complete application, as referenced in the *Planning Act*.

Additional approvals will be required at a later stage. These include Consent applications to sever the existing properties and a Site Plan application.

## 4.3 Summary of Technical Reports

As part of a complete application for the Zoning By-law Amendment, the following technical studies were completed by Wood Environment & Infrastructure Solutions (including revisions):

### 4.3.1 Acoustical Feasibility Study

Wood has prepared a revised **Road Traffic Noise Impact Study** (NIS) dated March 2020, as per the City's specific request for an Acoustical Feasibility Study. The NIS includes revisions to the previously completed study from April 2019 (first submission), based on an updated set of drawings with the dwellings fronting on Dunbar Road as opposed to Confederation Parkway.

The analysis of noise impacts resulting from the proposed redevelopment illustrates that the proposal can meet the noise criteria requirements outlined in NPC-300 and the Peel Guideline provided the noise abatement recommendations are implemented (refer to Section 5.0 and Table 6.1 of the NIS for additional details).

### 4.3.2 Arborist Report and Tree Preservation Plan

Wood has prepared a revised **Tree Inventory, Arborist Report, and Tree Protection Plan** dated April 2020, as per the City's specific request for an Arborist Report and Tree Preservation Plan. The report includes revisions to the materials completed in February 2019 (first submission), based on an updated set of drawings with the dwellings fronting on Dunbar Road as opposed to Confederation Parkway.

The purpose of the consolidated report is to provide a detailed inventory of trees that may be impacted by the proposed redevelopment. Based on the current design plans and current construction footprint, the following items will be required to

meet the requirements for construction space:

- A total of 21 trees will require removal (refer to Sections 4.0 and 4.1 of the Report for additional details);
- One tree has been identified for preservation (Tree No. 200); and,
- A total of 16 trees will require permits for removal (seven (7) at 2476 and nine (9) at 2482 Confederation Parkway).

### 4.3.3 Stormwater Management Plan and Functional Servicing Report

Wood has prepared a revised **Functional Servicing and Stormwater Management Report (FSSR)** dated October 2020, as per the City's specific request for a Stormwater Management Plan and Functional Servicing Report. The FSSR includes revisions to the previously completed report from February 2020. Revisions are based on City comments provided through the second submission (received in June 2020).

The purpose of the FSSR is to demonstrate that the proposed redevelopment can be both serviced and graded in accordance with the City of Mississauga and the Region of Peel design criteria.

The results from the analyses conclude the following:

- The proposed development can be serviced (water and sanitary) and graded in accordance with the municipal and regional design requirements;
- The proposed conditions peak runoff rates can be managed by implementing stormwater management flood controls within the proposed front yard areas; and
- Water balance requirements can be achieved by implementing Low Impact Development measures (at-surface infiltration and evapotranspiration) within the front yard areas.



#### 4.3.4 Phase 1 Environmental Site Assessment

Wood has prepared a revised **Phase 1 Environmental Site Assessment (ESA)** dated February 2020, as per the City's specific request. The Phase 1 ESA includes revisions to the previously completed report from December 2018 (first submission).

The purpose of the analysis is to assess the environmental condition of the subject properties based on their historical and current uses. The findings of the Phase 1 ESA conclude that there are no Potentially Contaminating Activities (PCA) for past and present land uses on the subject properties and within the Phase 1 Study Area. Accordingly, no further environmental assessment work is warranted.

## 5.0 CONCLUSIONS

As outlined in this Planning Justification Report and informed by the supporting technical reports and studies, we are of the opinion that the proposed development and associated Zoning By-law Amendment represent an appropriate intensification and use for the subject properties. Based on the existing site context, surrounding land uses and transportation network, technical findings, as well as a comprehensive analysis of the proposal within the existing planning policy and regulatory framework, we conclude the following:

1. The proposed development is consistent with the Provincial Policy Statement;
2. The proposed development conforms to the Growth Plan;
3. The proposed development conforms to the policies of the Region of Peel Official Plan;
4. The proposed development conforms to all policies of the City of Mississauga Official Plan;
5. The proposed Zoning By-law Amendment conforms to the City of Mississauga Official Plan;
6. The proposed development does not create adverse impacts to the adjacent properties and surrounding area; and,
7. The proposed development can be appropriately serviced.

It is our professional opinion that the proposed semi-detached dwellings are appropriate, desirable, represent sound planning, and will further assist the City of Mississauga in achieving its transit-supportive intensification goals. As such, the Zoning By-law Amendment should be approved to implement the proposed development at 2476 and 2482 Confederation Parkway.

Respectfully submitted,



David Sajecki  
MCIP RPP M.PI B.Eng LEED AP  
Partner  
Sajecki Planning Inc.



# APPENDICES

**Application for Rezoning,  
Official Plan Amendment,  
and/or Plan of Subdivision -  
Submission Requirements Checklist**

Planning and Building Department  
Development and Design Division  
300 City Centre Drive  
Mississauga, ON L5B 3C1  
Tel: 905-896-5511  
[www.mississauga.ca](http://www.mississauga.ca)

Application  
File No.



General Information		
Address / Legal Description of Site 2476 & 2482 Confederation Pkwy (ZBLA only)		Ward No. 7
Description of Proposal 2 semi-detached dwellings		Meeting Date Oct. 17, 2018
Applicant Name D.Sajecki, Sajecki Planning	Planner Name S.Hussen	Pre-Application Meeting No. DARC 18-279

General Requirements	Required Reports / Studies (7 copies each, unless noted below)
<input checked="" type="checkbox"/> <a href="#">Complete Application Form</a>	<input checked="" type="checkbox"/> Planning Justification Report
<input checked="" type="checkbox"/> City Application Fees / Deposits	<input type="checkbox"/> Parking Utilization Study
<input type="checkbox"/> Commenting Agency Fee Collection Form	<input type="checkbox"/> Urban Design Study
<input type="checkbox"/> Region of Peel Commenting Fee	<input type="checkbox"/> Shadow Study
<input type="checkbox"/> Conservation Authority Review Fee	<input type="checkbox"/> Wind Study
<input checked="" type="checkbox"/> Cover Letter	<input checked="" type="checkbox"/> Acoustical Feasibility Study
<input checked="" type="checkbox"/> Context Plan / Map (40 copies)	<input checked="" type="checkbox"/> Arborist Report/Plan (Tree Inventory)
<input checked="" type="checkbox"/> Concept / Site Plan (40 copies)	<input checked="" type="checkbox"/> Tree Preservation Plan
<input checked="" type="checkbox"/> Grading / Site Servicing Plan (35 copies)	<input checked="" type="checkbox"/> Easements / Restrictions on Title
<input checked="" type="checkbox"/> Survey Plan (40 copies)	<input type="checkbox"/> Streetscape Feasibility Study
<input type="checkbox"/> Draft Plan of Subdivision (50 copies)	<input type="checkbox"/> Traffic Impact Assessment / Study
<input checked="" type="checkbox"/> Building Elevations (7 copies)	<input type="checkbox"/> Transportation Demand Management Strategy
<input type="checkbox"/> Draft Official Plan Amendment (3 copies)	<input type="checkbox"/> Traffic Safety Impact Study
<input checked="" type="checkbox"/> Draft Zoning By-law (3 copies)	<input checked="" type="checkbox"/> Storm Water Management Plan / Report
<input checked="" type="checkbox"/> Draft Notice Sign Mock-up (1 copy)	<input checked="" type="checkbox"/> Functional Servicing Report (FSR) (9 copies)
<input checked="" type="checkbox"/> Digital copy (PDF format) of all required documents, plans, drawings, studies and reports on USB memory stick (2 memory sticks)	<input type="checkbox"/> Environmental Impact Statement - Type (i.e. minor or major) to be determined following site visit prior to application submission (9 copies)
<input checked="" type="checkbox"/> List of Low Impact Design Features for Site and Building (1 copy)	<input type="checkbox"/> Slope Stability Study / Top of Bank Survey
<input type="checkbox"/> Urban Design Advisory Panel	<input type="checkbox"/> Geotechnical Report
<b>Other Requirements / Notes</b>	<input checked="" type="checkbox"/> Phase 1 Environmental Site Assessment
	<input type="checkbox"/> Phase 2 Environmental Site Assessment
	<input type="checkbox"/> Heritage Impact Statement
	<input type="checkbox"/> Archaeological Assessment
	<input type="checkbox"/>
	<input type="checkbox"/>
	<input type="checkbox"/>

Other Information
<ul style="list-style-type: none"> <li>Application forms can be obtained at <a href="http://www.mississauga.ca/portal/residents/pbformscentre">http://www.mississauga.ca/portal/residents/pbformscentre</a></li> <li>Drawings / Plans must be folded to letter size (8 1/2" x 11"). Rolled drawings/plans will not be accepted</li> <li>Additional information/reports/studies/plans may be required upon submission of the application</li> <li>As part of the Public Consultation Strategy for a complete application, the applicant will be required to host a Community Meeting with respect to the application in the event: the Ward Councillor decides not to host a Community Meeting; and the application includes of a residential component or the subject lands are located within 120m of residential uses</li> <li>Application submission is by appointment only. To book an appointment, please phone 905-615-3200 ext. 4199 or by email at <a href="mailto:sanja.blagojevic@mississauga.ca">sanja.blagojevic@mississauga.ca</a></li> <li>Applicants should consult with the Planning Services Centre of the Development and Design Division to verify the application fee calculation before preparing a cheque. Send your completed Fee Calculation Worksheet (in the application form) to <a href="mailto:epplans.devdes@mississauga.ca">epplans.devdes@mississauga.ca</a> for review</li> </ul>

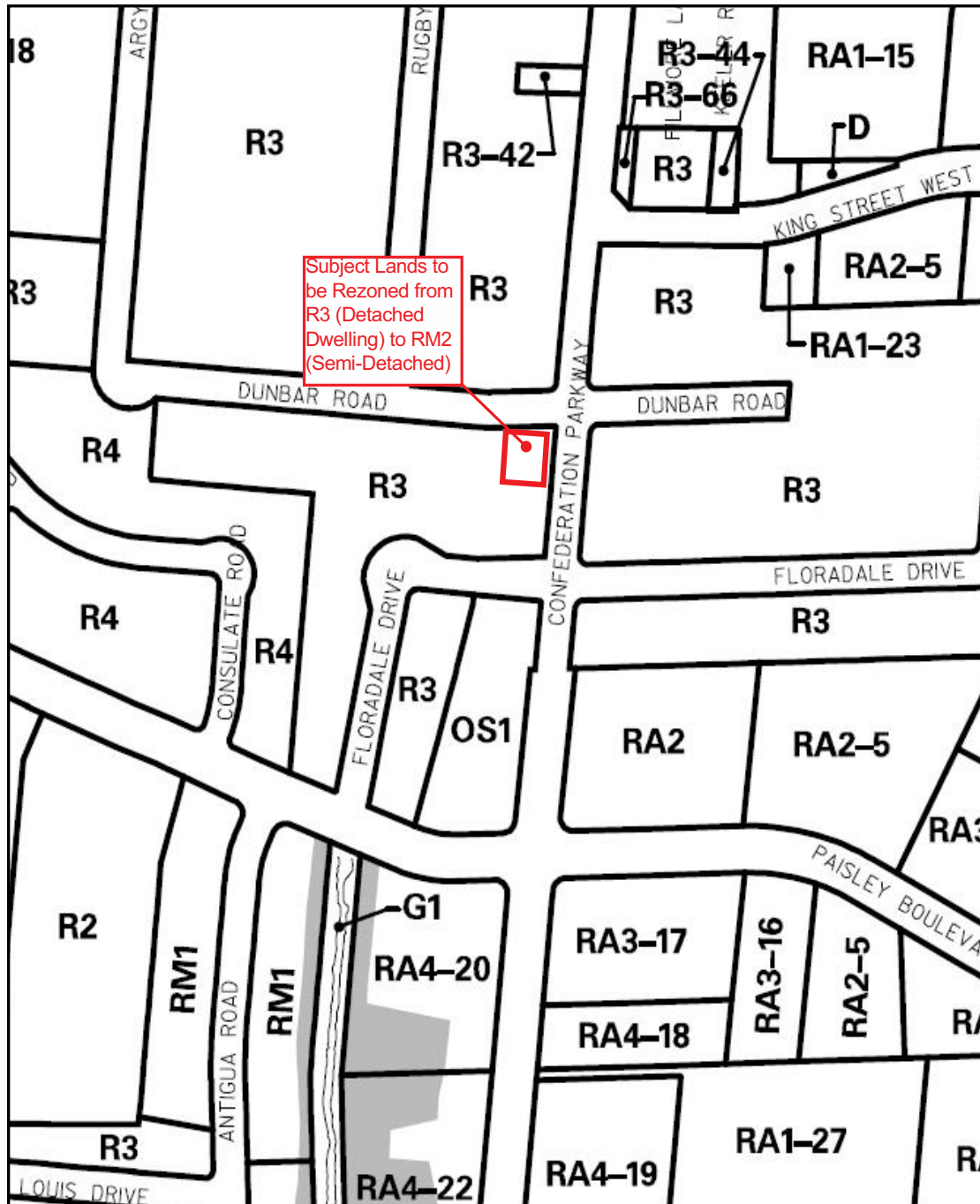
A By-law to amend By-law 0225-2007, as amended.

WHEREAS pursuant to section 34 of the planning Act, R.S.O. 1990, cP.13, as amended, the Council may amend a Zoning By-law;

NOW THEREFORE the Council of The Corporation of the City of Mississauga ENACTS as follows:

1. By-law 0225-2007, as amended, being a City of Mississauga Zoning By-law, is amended by rezoning the subject lands located at 2476-2482 Confederation Parkway from R3 to RM2 (Map 15, Schedule "B" to By-law No. 0225-2007).





**LOCATION:**

PLAN 500 LOT 30, PLAN 500 LOT 29  
City of Mississauga  
Regional Municipality of Peel  
Subject Lands (2476, 2482 Confederation Parkway)

Not to Scale

THIS IS SCHEDULE XX  
TO BY-LAW AMENDMENT \_\_\_\_\_



October XX, 2020

**Sajecki** →  
**Planning**

