



# URBAN DESIGN BRIEF

128 LAKESHORE ROAD EAST, MISSISSAUGA



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**Date:**

December, 2021

**Prepared for:**

BlackTusk Group Inc.

**Prepared by:**

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# Table of Contents

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<b>01 Introduction</b>	<b>4</b>
<b>02 How to Read This Document</b>	<b>5</b>
<b>03 Existing Site &amp; Context</b>	<b>6</b>
<b>04 The Proposal</b>	<b>10</b>
<b>05 Policy Context</b>	<b>12</b>
<b>06 Site Design &amp; Orientation</b>	<b>14</b>
<b>07 Site Circulation</b>	<b>18</b>
<b>08 Built Form &amp; Massing</b>	<b>20</b>
<b>09 Architectural Design</b>	<b>27</b>
<b>10 Landscape Design &amp; Public Realm</b>	<b>34</b>
<b>11 Parking, Servicing &amp; Loading</b>	<b>38</b>
<b>12 Sustainable Initiatives</b>	<b>40</b>
<b>13 Conclusion</b>	<b>42</b>
<b>14 Design Terms</b>	<b>44</b>

# Introduction

MacNaughton Hermsen Britton Clarkson Planning Limited ('MHBC') has been retained by BlackTusk Group Inc. (the 'Applicant') to seek planning approvals to redevelop the property municipally addressed as 128 Lakeshore Road East (the 'Subject Lands'), owned by Skinner & Middlebrook Limited, in the City of Mississauga's Port Credit Community.

This Urban Design Brief ('UDB') has been prepared on behalf of the Applicant in support of the comprehensive redevelopment of the Subject Lands through the proposed Official Plan and Zoning By-law Amendment applications. The redevelopment of the site will include demolition of the existing structure and construction of a 10-storey mixed-use residential building with at grade retail/commercial uses along Lakeshore Road East and a 3-storey podium.

This report provides a comprehensive assessment and justification for the proposed redevelopment and requested approvals, in the context of the existing physical location as well as the current policy and regulatory framework. This report includes the following:

- A general description of the Subject Lands, surrounding land uses, and existing physical conditions;
- A description of the proposed redevelopment and design elements;
- A summary of the technical studies and plans prepared to support the proposal;
- A description of the proposed amendments to the Port Credit Local Area Plan and City of Mississauga Zoning By-law 0225-2007;
- A review of the existing policy and regulatory framework in relation to the proposed redevelopment and an assessment of its consistency and conformity with provincial, regional and city policies and regulations; and,
- A summary of key recommendations and conclusions related to the proposed redevelopment.

## Our Approach

In response to this design vision, MHBC, on behalf of the Owner, has prepared this Urban Design Brief to illustrate how the proposed development has responded to the Official Plan policies and the Urban Design Guidelines applicable to the Subject Lands.

Should you have any questions or wish to discuss the brief in further detail, please do not hesitate to contact us.

Sincerely;

**MHBC**



Eldon C. Theodore, BES, MUDS, MLAI, MCIP, RPP

Partner | Planner | Urban Designer



Nimita Chandiramani, B.Arch

Urban Designer

## 2.0

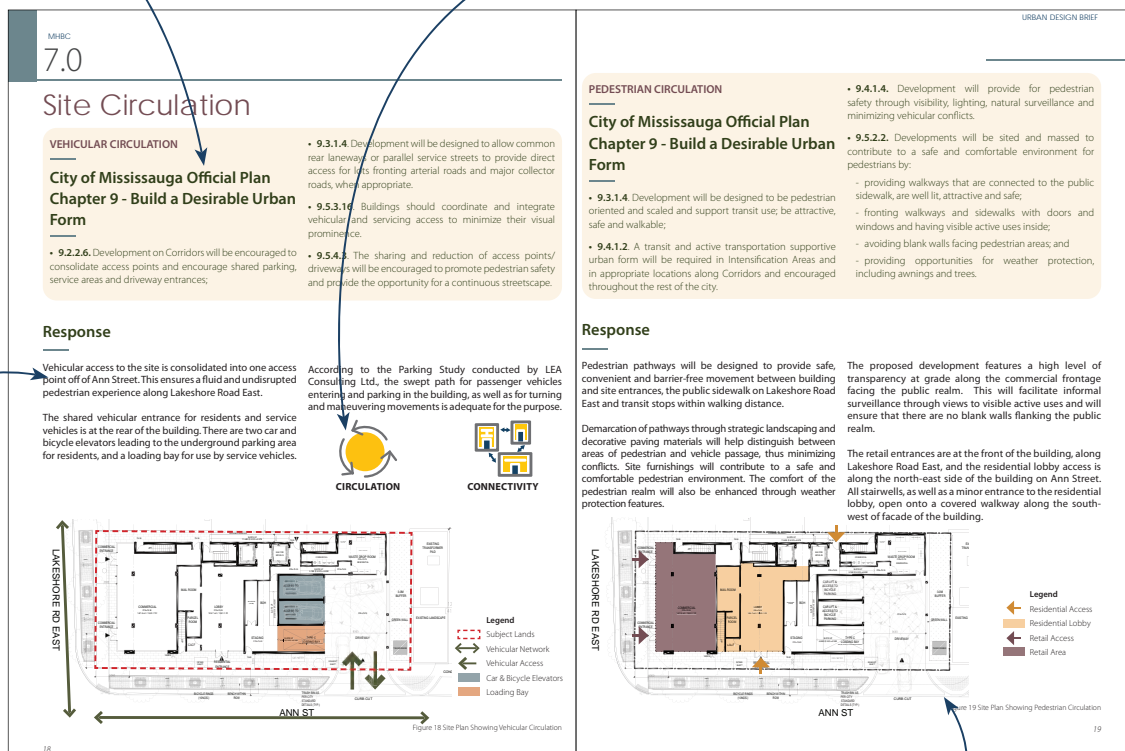
# How to Read This Document

This Urban Design Brief organizes key urban design principles into categories. Within each category, a written response demonstrating adherence with those principles is provided. In some cases where strict compliance is not feasible, design rationale is provided to outline how the design intent continues to be respected.

Well-designed developments can help to connect people with places, balance the protection of the environment with emerging built form, and achieve development that promotes a sense of place and local identity within a community. Key urban design terms have been used in this brief to further articulate how the proposal achieves good design principles and enhances the relationship with the surrounding community.

Design policy and guidelines from the city

Design terms referenced



Response to design policy and guidelines

Figure illustrating adherence where applicable or Photo / rendering examples



# Existing Site & Context

The consideration of an existing site's context is important in the planning analysis for the proposed development. Context must be evaluated not only as it relates to the existing physical environment and surrounding area, but to the specific and immediate urban setting and urban structure which includes future land uses and infrastructure.

## Site Description

As shown in Figure 1, the Subject Lands are located on the north side of Lakeshore Road East, and municipally addressed as 128 Lakeshore Road East in the City of Mississauga. The Subject Lands have a total area of approximately 934 m<sup>2</sup>, with approximately 20.0 metres of frontage on Lakeshore Road East, and approximately 46.2 metres of frontage on Ann Street.

The Subject Lands are currently occupied by the Skinner & Middlebook Funeral Home. The existing structure is 2 storeys in height, with pedestrian access provided from both Lakeshore Road and Ann Street. Vehicular access is currently provided from Ann Street to the rear of the site.



Figure 1 Map showing the subject lands location context

## Surrounding Land Uses

The Subject Lands are generally surrounded by the following land uses:

### North

- 1 Immediately north of the subject lands is a 15-storey residential building. A number of high density mixed use and residential buildings are located to the north of the Subject Lands, as well as low-rise single detached houses and stacked townhouses.

- 2 Further to the northwest is Port Credit GO Station (approx. 300m).

### East

- 3 Immediately east of the Subject Lands, across Ann Street is an existing 21 storey residential apartment building. Further east, a mixture of low-rise commercial and residential buildings are located along and behind Lakeshore Road.

### South

- 4 Immediately south of the Subject Lands, a number of low-rise commercial and retail building line Lakeshore Road, followed by mid-rise mixed use and residential buildings further to the south towards Lake Ontario.

- 5 Further south, St. Lawrence Park is located along the shore of Lake Ontario, as well as Port Credit Harbour Marina.

### West

- 6 The predominant land use to the west of the Subject Lands is mid-rise residential buildings, as well as several low-rise retail and commercial buildings.



Figure 2 The Subject Lands



Figure 4 Looking north across the intersection of Lakeshore Road East and Hurontario Street, immediately east of the Subject Lands.



Figure 3 Looking southwest along Lakeshore Road East from the intersection with Elmwood Ave South.



Figure 6 Looking southeast from the intersection of Hurontario Street and High Street East towards the Subject Lands.



Figure 5 Looking south across the intersection of High Street East and Ann Street, towards the Subject Lands.



Figure 8 Looking north across Lakeshore Road East towards the Subject Lands.



Figure 7 Looking north along Lakeshore Road East from the intersection of Helene Street North and Lakeshore Road East.



## Neighbourhood Context

The Subject Lands are located on the north side of Lakeshore Road East, immediately west of Ann Street. The Subject Lands are near the intersection of Hurontario Street and Lakeshore Road East, both of which are important transportation and economic corridors for the City of Mississauga. The Subject Lands are located within the Port Credit Community Node in the City of Mississauga, which comprises the lands located south of the CN Rail Line to the waterfront and between Elmwood Avenue South and Mississauga Road South.

The Port Credit Community Node primarily consists of commercial, office, retail and mid- to high-rise residential uses along Lakeshore Road East. The Surrounding area provides for a number of parks and open spaces, waterfront, naturalized areas, and schools. As a result, the Subject Lands are located in proximity to a number of community facilities, parks, and amenities which are listed below in Table 1.

## Transportation Context

The Subject Lands front onto Lakeshore Road East, which is identified as an Arterial in the City of Mississauga Official Plan and Ann Street is identified as a Minor Collector.

There are several existing Mississauga Transit bus stops located in close proximity to the Subject Lands, serving routes 2, 8, 14, and 23. Route 2 provides local service from Square One in the north, down Hurontario Street, to Port Credit GO Station in the south. Route 8 provides local service from Square One in the north, down Cawthra Road, to Port Credit GO Station in the south. Route 14 provides local service from Clarkson GO in the west, along Truscott Drive and Indian Road, to Port Credit GO in the east. Route 23 provides local service from Clarkson GO in the west, along Lakeshore Road, to Long Branch GO in the east.

Port Credit GO Station is located approximately 300m north of the Subject Lands, providing frequent commuter rail service along the Lakeshore West GO line, to destinations including Niagara Falls, Hamilton, and Toronto.

In addition to the existing bus and GO Train service in proximity to the Subject Lands, the site is also located

in close proximity to the future Port Credit Hurontario LRT Station, which will be located at the intersection of the Lakeshore West GO line and Hurontario Street, approximately 350m from the Subject Lands. The Hurontario LRT is an 18 kilometre Light Rail Transit (LRT) line running along Hurontario Street from Port Credit in the south to Brampton in the north. Metrolinx is currently underway with construction on the LRT line, with an expected completion in fall 2024.

There are a number of active transportation options in the surrounding area, both on-street and off-street. In 2018 a Cycling Master Plan update was approved by council. The updated Cycling Master Plan focused on a number of key aspects:

- Cycling infrastructure planning and design best practices have changed significantly and updates are required to achieve best practices.
- The cycling network must be safe, connected, convenient and comfortable for residents, and visitors of all ages and riding ability to try cycling.
- Implementation of new cycling infrastructure will be coordinated with road rehabilitation and major road construction projects, where possible.
- The cycling network laid out in the Cycling Master Plan will result in 897 kilometres of infrastructure to be built over 27 years. This includes:
  - Cycle tracks where a bicycle lane is physically separated from the road by a curb and is either at sidewalk level or slightly lower, reserved for bicycles only
  - Bicycle lanes separated from traffic lanes by flexible posts, planters, parking stalls, curbs or other barriers, reserved for bicycles only
  - Bicycle lanes where cyclists travel in a lane beside regular traffic lanes, reserved for bicycles only
- Multi-use trails along boulevards and also through parks
- Shared routes between cyclists and motorists on roads with lower speeds



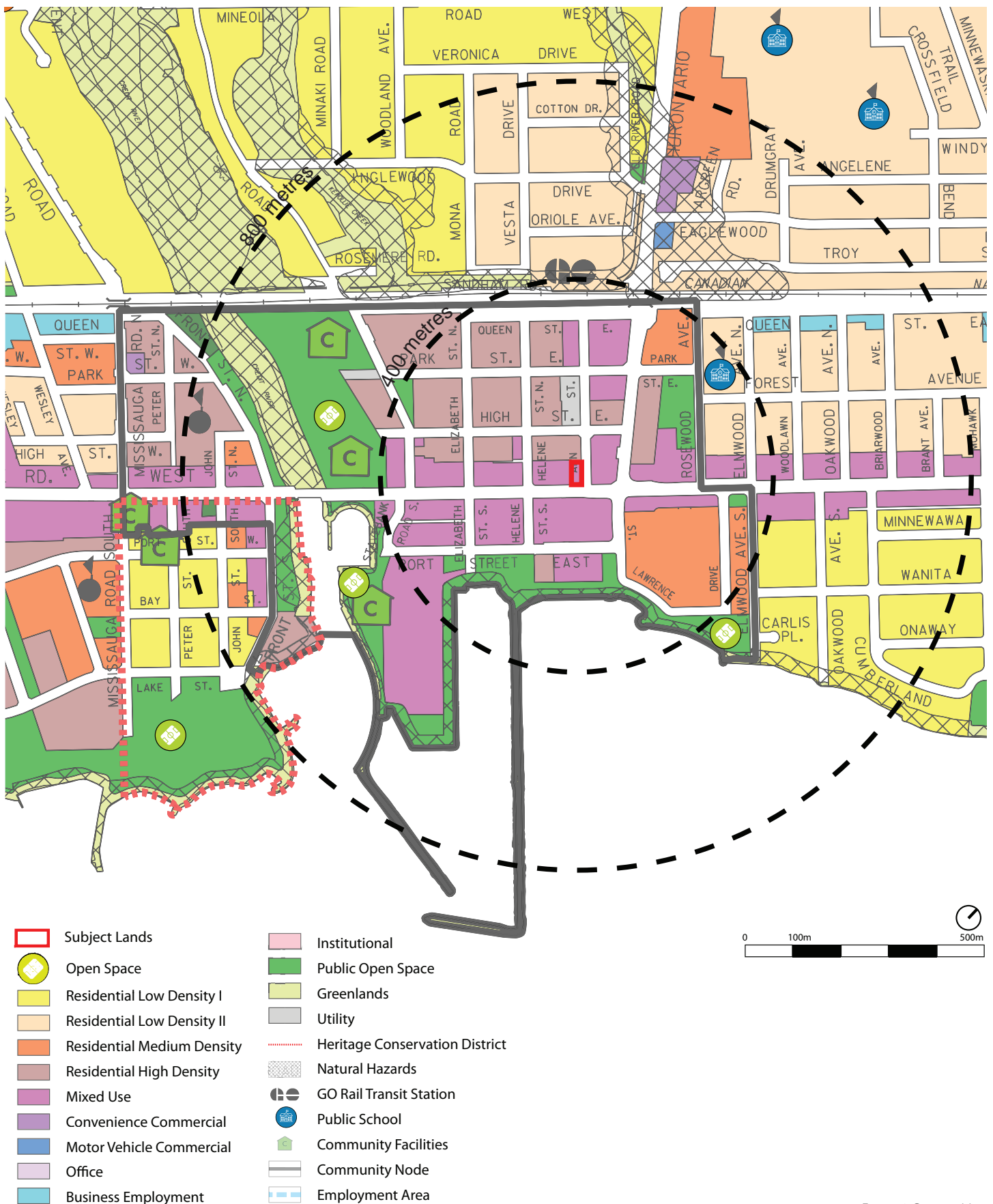


Figure 9 Context Map

# The Proposal

The current redevelopment proposal is the result of careful planning and design undertaken by the Project Team, including consideration of the Provincial policy framework for managing and directing growth, the Regional and local policy context, as well as the evolving physical landscape of the Port Credit MTSA area and the area surrounding the Subject Lands.

As noted previously, the Subject Lands are located within the Port Credit Community Node of Mississauga, with a total area of 0.068 hectares, that are currently occupied by Skinner & Middlebrook Funeral Home. The existing building on the Subject Lands is to be demolished in order to facilitate the development of one 10-storey + mechanical penthouse mixed-use residential apartment building with at-grade retail/commercial along Lakeshore Road East. The proposed development includes a 3-storey podium with stepbacks to a tower portion of the building.

As illustrated in Figure 10, the proposed redevelopment concept provides for an 10-storey + mechanical penthouse, mid-rise built form along Lakeshore Road East, with a transition in height and density towards the west. Stepbacks have been provided at the 4th floor, 7th floor, 9th floor and the penthouse. The intent of the proposed built form is to transition the height and scale of development from the existing low- and mid-rise residential and commercial buildings to the south and west, to the existing high-rise residential buildings to the north and east. Immediately north of the Subject Lands is an existing 15-storey residential building, and immediately east is an existing 20-storey residential building. The proposed 10-storey + MPH building height will soften the transition between the existing high-rise residential buildings to the low- and mid-rise buildings to the south and west, while providing for active uses at grade to activate the streetscape.

The proposal has been designed to be human-scaled and pedestrian-oriented through the proposed building placement along the street frontages and proposed integration with an internal driveway providing access from Ann Street to the internal part of the site. Retail/commercial space is also proposed at-grade along Lakeshore Road East to continue the existing streetscape, enhance the pedestrian environment, and activate the street.

A total of 42 residential dwelling units are proposed on the Subject Lands, which will range in size, in the form of 2 and 3 bedroom units; including larger units suitable for families, ranging in size from 830sqft to 1,330 sqft. Residential units are proposed on the 2nd to 10th floors and indoor amenity space is proposed on the penthouse floor. Vehicular access is proposed from Ann Street, with underground parking provided, to be accessed via car elevators due to space constraints. The proposed building provides for a total gross floor area of 5,334 m<sup>2</sup>.



Figure 10 Render of the proposed development prepared by IBI Architects



# Policy Context

## City of Mississauga Official Plan

The City of Mississauga Official Plan ('OP') provides direction for the City's growth and articulates a vision for the future of the City. It was adopted by City Council on September 29, 2010 and partially approved by the Region of Peel on September 22, 2011. The MOP came into partial effect on November 14, 2012, when the Ontario Municipal Board approved the MOP with modifications. Since this time, certain appeals have been withdrawn; further decisions have been made by the Board; and further amendments have been made to reflect Council approved Official Plan Amendments. The most recent consolidation reflects these decisions up to April 8, 2021. For those portions of the Plan that remain under appeal, the relevant policies in the Mississauga Plan (2003) will remain in effect.

The MOP provides planning policies to guide the City's development to the year 2031. The policies are intended to direct and manage the change of the City and set the context for the review and approval of development applications.

In accordance with the various schedules of the MOP, the Subjects Lands are designated as follows:

- Community Node (Schedule 1 – Urban System), Figure 11
- Intensification Corridor (Schedule 1 – Urban System)
- Major Transit Station Area (Schedule 2 – Intensification Areas), Figure 12
- Port Credit Community Node and Character Area (Schedule 9 – Character Areas), Figure 13
- Mixed Use (Schedule 10 – Land Use Designations) (refer to Figure 9)

An analysis of the policies of the Mississauga Official Plan, as they relate to the proposed development and amendment application, is provided in the following sections.

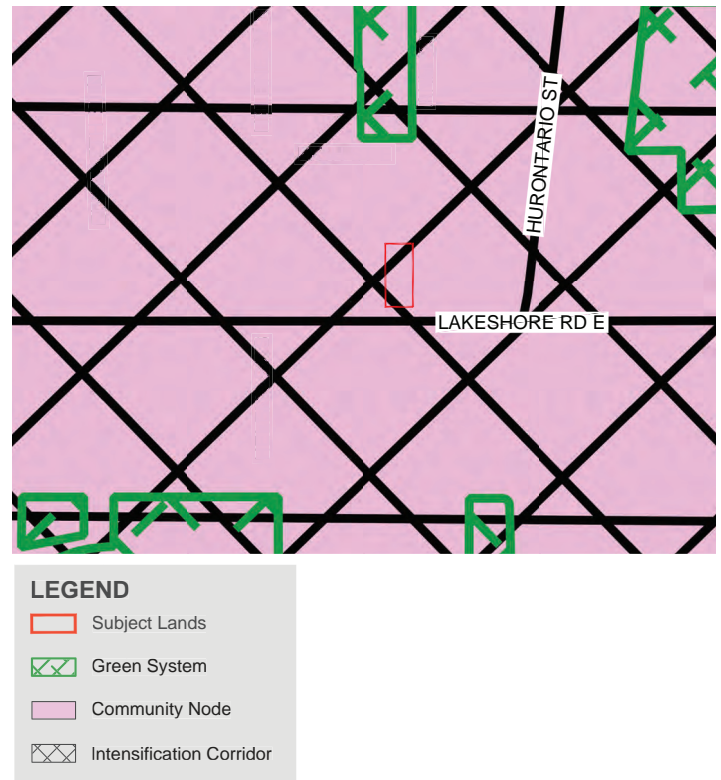


Figure 11 Subject Lands on OP Schedule 2

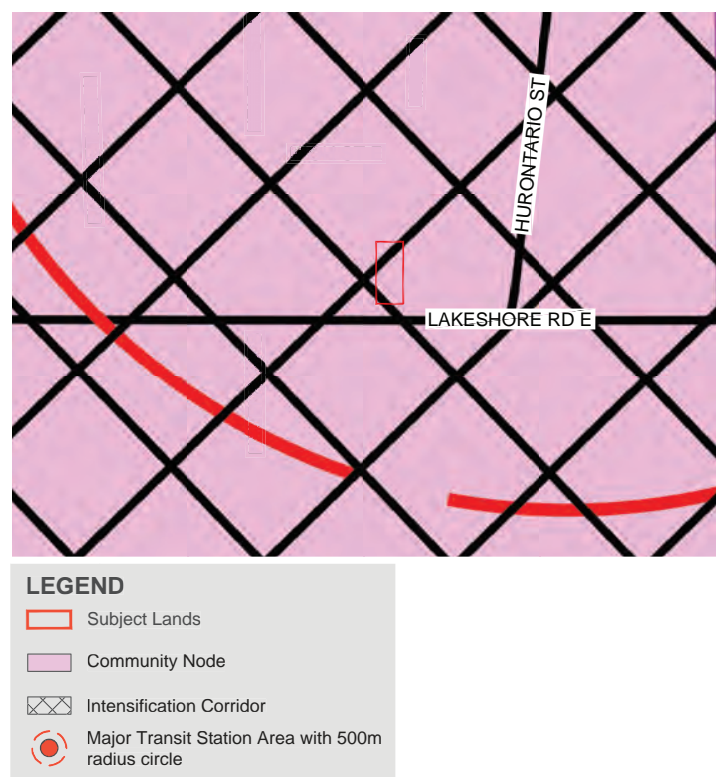


Figure 12 Subject Lands on OP Schedule 9

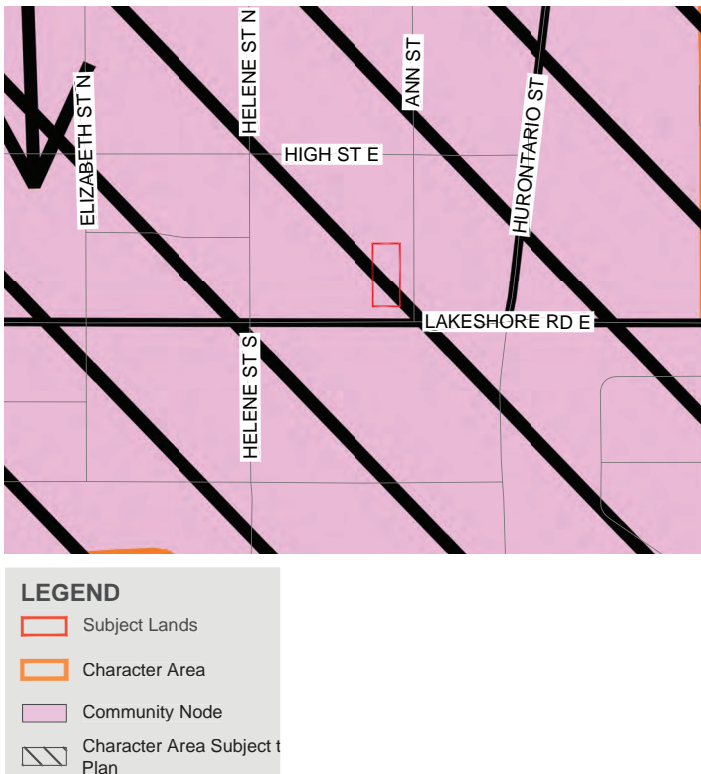


Figure 13 Subject Lands on OP Schedule 9

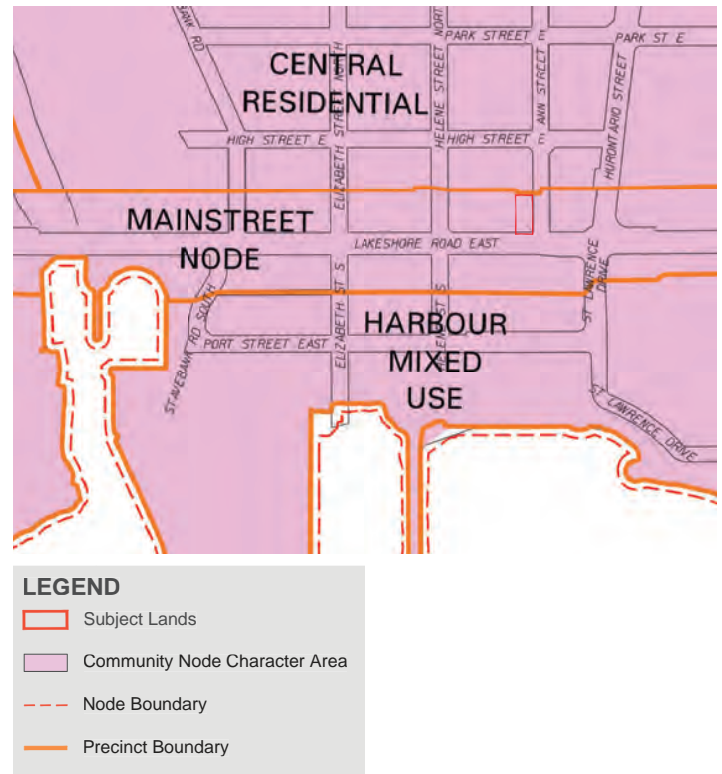


Figure 14 Subject Lands on Port Credit LAP Schedule 1

## Port Credit Local Area Plan

The Port Credit Local Area Plan (LAP) provides policies for lands located in South Central Mississauga. The Local Area Plan elaborates on, or provides exceptions to, the policies and schedules of the MOP. The Subject Lands fall within the Mainstreet Node Precinct of the Community Node Character Area in accordance with Schedule 1 of the Port Credit LAP (Figure 13).

The vision for Port Credit is intended to manage change to ensure an appropriate balance is maintained between growth and preservation of character. Ultimately, the vision for Port Credit is for an evolving urban waterfront village with a mixture of land uses, a variety of densities, compact pedestrian and cycling friendly, transit supportive urban forms, a significant public realm, public access to the waterfront, and development that incorporates high quality built form.



Figure 15 Subject Lands on Port Credit LAP Schedule 2B

# Site Design & Orientation

## Port Credit Local Area Plan

**10.2 Community Node Character Area-** The Community Node Character Area will exhibit high standards of urban design that reinforce and enhance the identity of Port Credit as a vibrant and memorable urban place.

**10.2.3 Mainstreet Node Precinct-** This precinct includes part of Port Credit's traditional mainstreet which generally extends a half block north and south of Lakeshore Road (east and west).

**10.2.3.1:** This precinct will contain street related commercial uses with a rhythm of closely spaced storefronts lining the street in order to encourage and foster an active pedestrian street.

**10.2.3.3:** The Mainstreet Node Precinct will meet both the day-to-day needs of local residents as well as those visiting the area.

## City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.1.10.** The city vision will be supported by site development that:
  - respects the urban hierarchy;
  - demonstrates context sensitivity, including the public realm;
  - promotes universal accessibility and public safety;
- **9.2.2.3.** While new development need not mirror existing development, new development in Neighbourhoods will:
  - respect existing lotting patterns;
  - minimize overshadowing and overlook on adjacent neighbours;
  - be designed to respect the existing scale, massing, character and grades of the surrounding area.

- **9.3.1.8.** The design of developments at intersections and along major streets should be of a highly attractive urban quality, recognizing that streets are important civic spaces and linkages.

- **9.3.1.11.** Reverse frontage lots will not be permitted, except for infill development where a street pattern has already been established.

- **9.4.3.1.** Mississauga is committed to the creation of an accessible city. The design of the physical and built environment will have regard for universal design principles.

- **9.4.3.2.** All development will be consistent with the Mississauga Accessibility Design Handbook.

- **9.5.1.1.** Buildings and site design will be compatible with site conditions, the surrounding context and surrounding landscape of the existing or planned character of the area.

- **9.5.1.2.** Developments should be compatible and provide appropriate transition to existing and planned development by having regard for the following elements:

- street and block patterns;
- the size and configuration of properties along a street, including lot frontages and areas;
- views, sunlight and wind conditions;
- privacy and overlook; and
- the function and use of buildings, structures and landscapes.

- **9.5.1.3.** Site designs and buildings will create a sense of enclosure along the street edge with heights appropriate to the surrounding context.

- **9.5.1.9.** Development proposals will demonstrate compatibility and integration with surrounding land uses and the public realm by ensuring that adequate privacy, sunlight and sky views are maintained and that microclimatic conditions are mitigated.

- **9.5.1.11.** New residential development abutting major roads should be designed with a built form that mitigates traffic noise and ensures the attractiveness of the thoroughfare.



- **9.5.1.12.** Noise will be mitigated through appropriate built form and site design. Mitigation techniques such as fencing and berms will be discouraged.
- **9.5.2.6.** Development proponents will be required to demonstrate the successful application of universal design principles and compliance with legislated standards.
- **9.5.2.7.** Site development should respect and maintain the existing grades on-site.
- **9.5.2.12.** Heating, venting and air conditioning equipment and mechanical/utility functions will be located away from the public realm and not be visible from public view.

## City of Mississauga Official Plan Chapter 11 – Land Use Policies

- **11.2.6.2:** Lands designated Mixed Use will be encouraged to contain a mixture of permitted uses;
- **11.2.6.3:** Mixed Use development is encouraged through infilling to consolidate the potential of these areas;
- **11.2.6.4:** Residential uses will be combined on the same lot or same building with another permitted use; and,
- **11.2.6.5:** Residential uses will be discouraged on the ground floor. The proposal responds to and implements these policies as follows:
  - A mix of residential units are provided on site, with commercial/retail uses at grade;
  - The proposal will facilitate the infill redevelopment of an underutilized parcel;
  - The at-grade commercial/retail uses will serve both the residents as well as the surrounding community;

## Response

The proposal is for a mixed-use development featuring ground floor commercial space and residential use on the upper floors, with frontage onto Lakeshore Road East. The presence of active commercial use will aid in cultivating a comfortable, animated and pedestrian-friendly streetscape along Lakeshore Road East; and the proposed height of 10 storeys + MPH, combined with setbacks, will help to create a sense of enclosure through a defined streetwall, without creating a canyon effect.

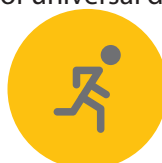
The proposed development preserves the predominantly mid-rise character along this part of Lakeshore Road East and reflects the planned mixed-use character of the Port Credit. Strategically placed landscaped buffers are used to mitigate noise as well as create a sense of privacy and enclosure.

The building is at the intersection of Lakeshore Road East and Ann Street, and features a highly articulated corner along the intersection, which places this building as a landmark location for the neighbourhood.

The proposed building is a mid-rise building, with appropriate setbacks so as to not create any negative shadow impacts on the site and surrounding public realm and will ensure that adequate access to sunlight is maintained.

The existing grading is relatively flat and will be maintained across the property to preserve existing topography. All mechanical equipment will be located at the penthouse, and will be screened from public view by the indoor amenity space in front of it, so that it does not negatively impact views from the public realm.

All walkways and building entrances will be barrier-free, consistent with the Mississauga Accessibility Design Handbook and designed in accordance with the principles of universal design.



ANIMATION



STREETWALL



ACCESSIBILITY

## INTENSIFICATION, GROWTH & COMPLETE COMMUNITIES

### City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.1.3.** Infill and redevelopment within Neighbourhoods will respect the existing and planned character.

## Response

The proposed development takes advantage of an underutilized site by providing additional commercial and residential uses in a rapidly developing area. The proposal is compatible with the existing context as residential and commercial uses are already common throughout the area, and because the modest height and scale of the proposed built form will not result in any negative impacts on the established neighbourhoods.

The proposal also supports the planned context for the Lakeview community and the intensification policies of the Port Credit Area Plan by providing a mix of transit-supportive uses along an identified Corridor where higher order transit and active transportation facilities are planned. The proposal is directly adjacent to a planned major development node and will complement the intended function and high-quality design of this area.

The proposal will contribute to a complete community in the Lakeview neighbourhood by enhancing the number of local services, amenities and housing opportunities available in the area.

The proposed building will use materials that reflect the architectural character of the Port Credit neighbourhood and the immediate surroundings. Facade materials such as red toned brick, vision glass, metal and a gold-hued spandrell glass ensure the seamless integration of this building within its surroundings.



## SAFETY

### City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

• **9.5.6.1.** Site layout, buildings and landscaping will be designed to promote natural surveillance and personal safety.

• **9.5.6.2.** Active building frontages should be designed to face public spaces including entries and windows to ensure natural surveillance opportunities.

• **9.5.6.3.** Development should clearly define areas of access and egress to avoid the creation of entrapment areas.

• **9.5.6.4.** Development should incorporate lighting to ensure all designated areas of circulation, entrance, and connections are appropriately illuminated.

## Response

The proposal adheres to the principles of Crime Prevention Through Environmental Design (CPTED) in order to promote a safe environment for residents and visitors.

The proposed building façade features a high degree of glazing along the public realm, allowing visibility between the interior and exterior uses of the building for passive surveillance. The proposed residential units are also oriented to front onto Lakeshore Road East and Ann Street, and include balconies that directly overlook the public realm, enhancing the perception of safety through community vigilance.

Building entrances are easily identifiable, directly accessible from the street and will be well-lit. Appropriate lighting will be used along pedestrian paths of travel to signify designated areas of passage and ensure safe circulation.



Figure 16 Example of a clearly defined and well-lit entrance.



Figure 17 Example of an active commercial use at-grade providing informal surveillance.



# Site Circulation

## VEHICULAR CIRCULATION

### City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.2.2.6.** Development on Corridors will be encouraged to consolidate access points and encourage shared parking, service areas and driveway entrances;

- **9.3.1.4.** Development will be designed to allow common rear laneways or parallel service streets to provide direct access for lots fronting arterial roads and major collector roads, when appropriate.

- **9.5.3.16.** Buildings should coordinate and integrate vehicular and servicing access to minimize their visual prominence.

- **9.5.4.3.** The sharing and reduction of access points/driveways will be encouraged to promote pedestrian safety and provide the opportunity for a continuous streetscape.

## Response

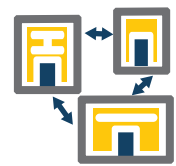
Vehicular access to the site is consolidated into one access point off of Ann Street. This ensures a fluid and uninterrupted pedestrian experience along Lakeshore Road East.

The shared vehicular entrance for residents and service vehicles is at the rear of the building. There are two car and bicycle elevators leading to the underground parking area for residents, and a loading bay for use by service vehicles.

According to the Parking Study conducted by LEA Consulting Ltd., the swept path for passenger vehicles entering and parking in the building, as well as for turning and maneuvering movements is adequate for this purpose.



CIRCULATION



CONNECTIVITY

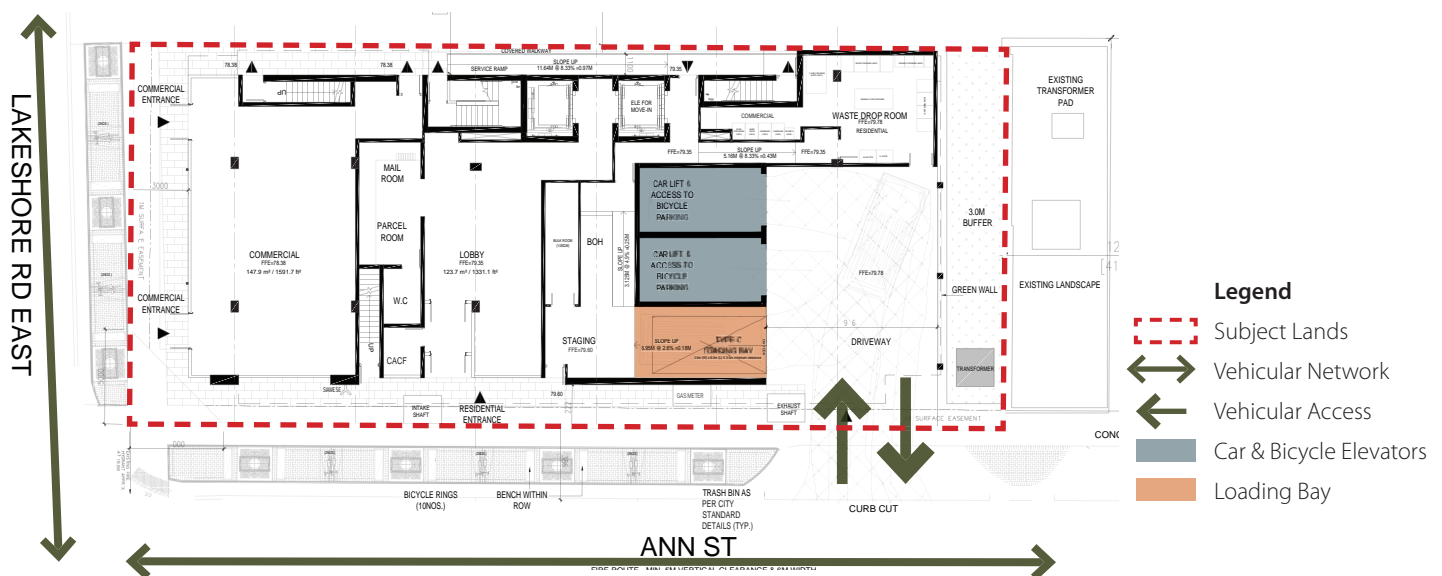


Figure 18 Site Plan Showing Vehicular Circulation

## PEDESTRIAN CIRCULATION

### City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.3.1.4.** Development will be designed to be pedestrian oriented and scaled and support transit use; be attractive, safe and walkable;
- **9.4.1.2.** A transit and active transportation supportive urban form will be required in Intensification Areas and in appropriate locations along Corridors and encouraged throughout the rest of the city.

• **9.4.1.4.** Development will provide for pedestrian safety through visibility, lighting, natural surveillance and minimizing vehicular conflicts.

• **9.5.2.2.** Developments will be sited and massed to contribute to a safe and comfortable environment for pedestrians by:

- providing walkways that are connected to the public sidewalk, are well lit, attractive and safe;
- fronting walkways and sidewalks with doors and windows and having visible active uses inside;
- avoiding blank walls facing pedestrian areas; and
- providing opportunities for weather protection, including awnings and trees.

## Response

Pedestrian pathways will be designed to provide safe, convenient and barrier-free movement between building and site entrances, the public sidewalk on Lakeshore Road East and transit stops within walking distance.

Demarcation of pathways through strategic landscaping and decorative paving materials will help distinguish between areas of pedestrian and vehicle passage, thus minimizing conflicts. Site furnishings will contribute to a safe and comfortable pedestrian environment. The comfort of the pedestrian realm will also be enhanced through weather protection features.

The proposed development features a high level of transparency at grade along the commercial frontage facing the public realm. This will facilitate informal surveillance through views to visible active uses and will ensure that there are no blank walls flanking the public realm.

The retail entrances are at the front of the building, along Lakeshore Road East, and the residential lobby access is along the north-east side of the building on Ann Street. All stairwells open onto a covered walkway along the south-west of facade of the building.

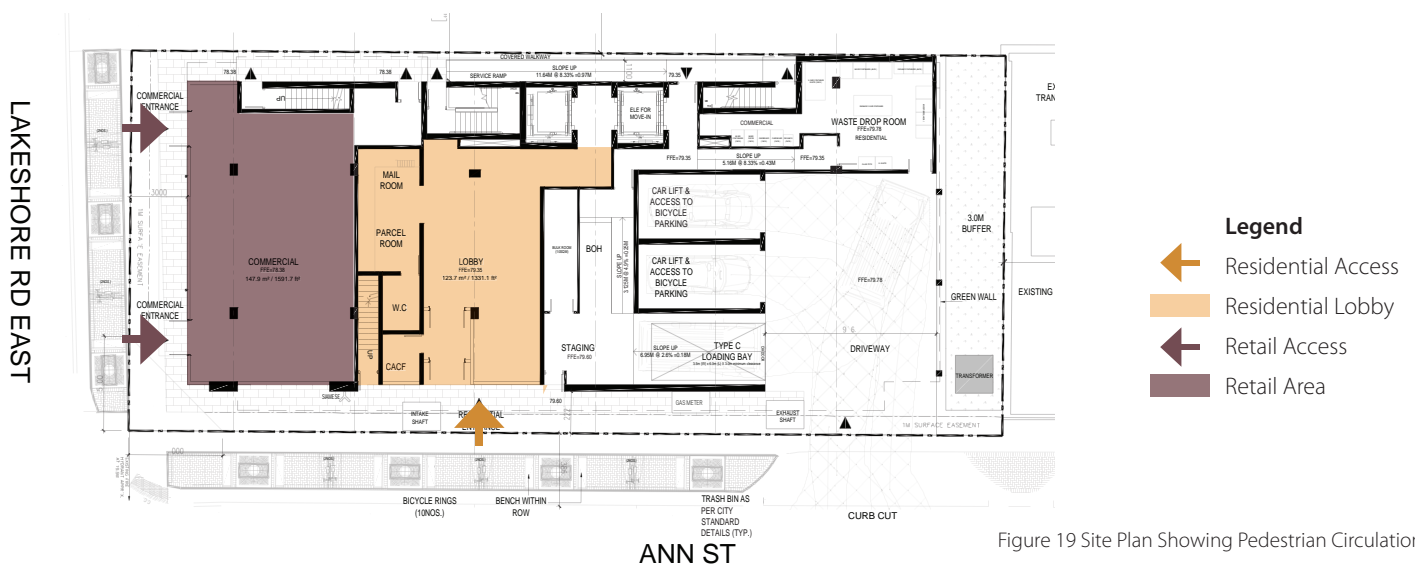


Figure 19 Site Plan Showing Pedestrian Circulation

# Built Form & Massing

## MASSING & TRANSITIONS

### Port Credit Built Form Guide

**2.2 Planned Building Heights** - Aligning tall buildings in key locations can create a strong reference point, which enriches urban legibility and aids in navigation.

As Community Nodes are intensification areas, all new buildings shall have a height of 2 storeys.

The relationship of a building size to the site area and configuration should be considered in order to avoid a building overwhelming its site. In such cases lower heights should be used. New towers should be compatible and characteristic with the existing and proposed neighbouring structures and in terms of their contribution to the skyline.

**2.4.1 Building Floor Plates and Building Orientation** - Buildings over 6 storeys should be designed so they are as square as possible to ensure minimal shadow impact and to ensure they do not create the visual impact of larger bulky floor plates. The maximum length of any building over 6 storeys should be 35.0 m including balconies for buildings under 15 storeys and 30.0 m for buildings over 16 storeys. The maximum floor plates of buildings over 7 to 10 storeys, inclusive of balconies, shall be 1200 m<sup>2</sup>.

### Port Credit Local Area Plan

#### 10.2.1 Community Node Character Area

**10.2.1.1** The overall development of the Node will be at a scale that reflects its role in the urban hierarchy.

**10.2.1.2** Floor plate size for buildings over six storeys will decrease as building height increases, to address, among other matters:

- overall massing (reduce “wall effect”);
- visual impact of buildings;
- protect skyviews; and
- limit shadow impact.

**10.2.1.3** Buildings over six storeys will maintain distance separations that, amongst other matters, address the following:

- existing distance separations between buildings;
- overcrowding of skyviews and skyline;
- protection of view corridors; and
- privacy and overlook of occupants.

## City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

**Policy 9.1.5.** Development on Corridors will be consistent with existing or planned character, seek opportunities to enhance the Corridor and provide appropriate transitions to neighbouring uses.

**Policy 9.4.1.3.** Development will support transit and active transportation by ensuring appropriate massing for the context;

**Policy 9.5.1.2.** Developments should be compatible and provide appropriate transition to existing and planned development by having regard for the following elements:

- the size and distribution of building mass and height;
- the orientation of buildings, structures and landscapes on a property.



## COMPATIBILITY

### Port Credit Built Form Guide

**2.3.3 Mainstreet Precinct** - This Precinct includes part of Port Credit's traditional mainstreet, which generally extends a half block north and south of Lakeshore Road East and West. New developments along Lakeshore Road East and West shall be a minimum height of 2 storeys and a height of 7.5 m for any new construction along the Mainstreet Corridor Precinct. A maximum height of 3 storeys or 12.0 m is permitted. The second and third storey must be usable space.

The mixed use component of new development should be continuous for the first half block of Lakeshore Road East and West.

The minimum first floor height of a building along the Lakeshore Road East and West frontage shall be 4.5 m, to ensure buildings and structures relate to human scale and reinforce the scale of the community:

- a. Built form should be closely related to, and integrated with, the street line, and with minimal building setbacks, to provide spatial enclosure and street-related activity;
- b. New buildings should be compatible in bulk, massing and scale of the built form to provide an integrated streetscape;
- c. Retail uses will be required along Lakeshore Road with direct access to the public sidewalk;
- i. Continuity of built form should exist from one property to the next with minimal gaps between buildings;

### Port Credit Local Area Plan

#### 8.4 Distinct Identity & Character

**8.4.1** The character will reflect the vision of an urban waterfront village. City initiatives, including investments in lighting, public art, transportation features, streetscape improvements, parks planning, will contribute to the vision.

**8.4.2** Lakeshore Road (east and west) is a key location for the promotion of Port Credit as a place for vibrant cultural activities and providing a "sense of place". The mainstreet environment, including the built form, public realm, street furniture, lighting, cycling amenities, trees, gathering places are to be protected and, where possible, enhanced.

### City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

**9.1.5.** Development on Corridors will be consistent with existing or planned character, seek opportunities to enhance the Corridor and provide appropriate transitions to neighbouring uses.

**9.2.2.3.** While new development need not mirror existing development, new development in Neighbourhoods will:

- respect the scale and character of the surrounding area;
- be designed to respect the existing scale, massing, character and grades of the surrounding area.

**9.3.1.4.** Development will be designed to: accentuate the significant identity of each Character Area;

**9.3.1.7.** Streetscapes will be designed to create a sense of identity through the treatment of architectural features, forms, massing, scale, site layout, orientation, landscaping, lighting and signage.

**9.4.1.3.** Development will support transit and active transportation by ensuring buildings respect the scale of the street;

**9.5.3.1.** Buildings will be designed to create a sense of identity through the site layout, massing, forms, orientation, scale, architectural features, landscaping and signage.

## Response

The proposed development will complement the scale and character of development along Lakeshore Road East as well as the surrounding established neighbourhoods, while achieving the intensification and density targets of the growth plan. The development is located within a Major transit station area (MTSA) which is intended for intensification.

The proposal introduces a high-quality built form that will add to the visual appeal and sense of identity of the Lakeview neighbourhood, and will help define the planned character of the area as the Lakeview Corridor begins to intensify. The proposed development fronts onto Lakeshore Road East and contributes to a defined street edge that will help enhance the Corridor. The proposal is spaced closely to the existing buildings to minimize breaks in the streetwall.

The proposed development exhibits an elongated rectangular form that is consistent with the scale and massing of existing buildings seen along Lakeshore Road East. The proposed massing is articulated through a variety of materials and architectural design elements that add visual interest and clearly indicate a change in use between the ground floor commercial and residential uses on the upper storeys. The material choice is in accordance with the materials and colours used in the Port Credit area, to create a seamless experience in the neighbourhood.

The height, scale and massing of the proposed mixed-use development will establish continuity with neighbouring commercial and other planned uses along the Lakeview Corridor. It provides a buffer and a height transition from the existing buildings to the north to the built form in the south. An angular plane is achieved along the street-facing edges of the property with stepbacks, ensuring the transition in massing.

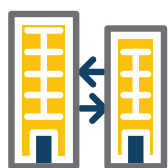
The proposed development will support active transportation in the area through a human-scaled design that creates a pleasant interface between the building and the existing streetscape.



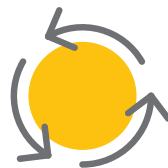
Figure 20 Rendering of the proposal, showing stepped massing toward Lakeshore Road East



**PEDESTRIAN-ORIENTED**



**COMPATIBILITY**



**CIRCULATION**



**CHARACTER**



**ANIMATION**

## SETBACKS & STREETWALL

### Port Credit Built Form Guide

**2.4.4 Site Size** - A building on a small site may only be constructed to 6 storeys above which a 45 degree angular plane may be used for additional levels set back from the street and the side and rear property lines.

**2.4.7 Building Setback - Mixed Use Setbacks** - Setbacks on streets where retail is required should generally be 0.6 m to 3.0 m from the property line.

New development must ensure that a minimum of 5.6 m public realm from the sidewalk/street curb to the face of the building can be accommodated to ensure appropriate streetscape treatment can be achieved

### City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.2.2.3.** New development in Neighbourhoods will respect the continuity of front, rear and side yard setbacks;

- **9.2.2.6.** Development on Corridors will be encouraged to:

- face the street, except where predominate development patterns dictate otherwise;
- site buildings to frame the street and where non-residential uses are proposed to create a continuous street wall;

- **9.4.1.3.** Development will support transit and active transportation by locating buildings at the street edge;

- **9.5.1.2.** Developments should be compatible and provide appropriate transition to existing and planned development by having regard for the front, side and rear yards.

- **9.5.3.5.** Front building façades should be parallel to the street. Consideration may be given to allow for periodic indentation for visual relief and features such as urban plazas.

- **9.5.4.1.** Development proposals should enhance public streets by creating a desirable street edge condition.

- **9.5.4.4.** Along Corridors where an urban character is appropriate, buildings should be located close to and aligned with the street to enclose the street.

## Response

The proposed building has a front yard setback of 3 metres which features a landscaped area between the building and the street. A continuous street wall and a sense of enclosure along Lakeshore Road East will be achieved by locating the building in close proximity to the street edge and aligning the façade with the street so that active uses frame the public realm.

The proposed development will also help to generally enhance the street edge condition along Lakeshore Road East as many of the existing commercial uses are disconnected from the public realm due to large front yard setbacks which currently accommodate surface parking.

The proposed development maintains a side setback of 2.2m from Ann Street, and the 3m rear yard setback incorporates a green buffer between the proposed building and existing building to the north. The west side has a 1.5m setback that includes a covered walkway for secondary access to the residential lobby, as seen in Figure 21.



SETBACK

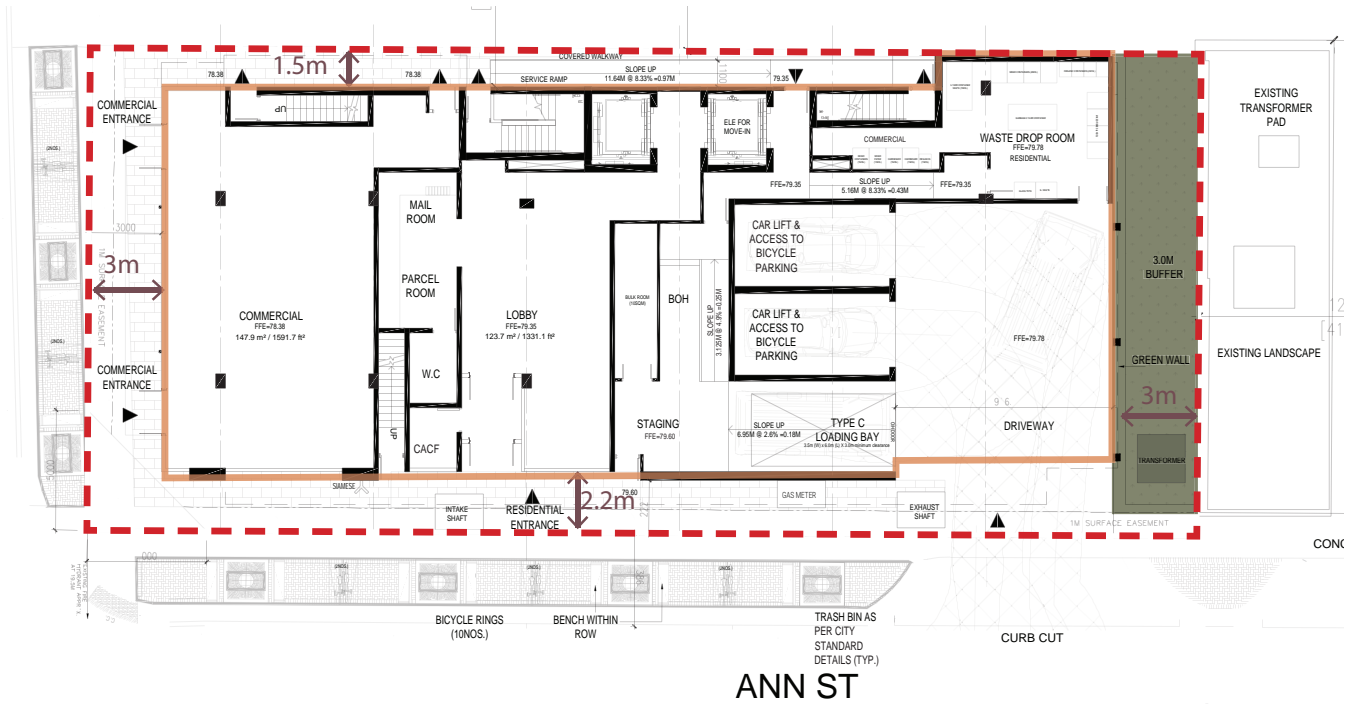


STREETWALL



ANIMATION

LAKESHORE RD EAST



ANN ST

**Legend**

- Subject Lands
- Setbacks
- Building Footprint
- Green Buffer

Figure 21 Site Plan showing setbacks



Figure 22 Example of vegetative, screening buffer within the setback



## HEIGHT

### Port Credit Built Form Guide

**2.4.2 Building Separation Distances** - The spacing between the faces of tall buildings enhances privacy, opens up views between buildings and permits access to sunlight and views of the sky.

Building separations in the Node are on average approximately 38.0 m for any building over 5 storeys, however many of the taller buildings are over 40.0 m apart.

A minimum of 35.0 m to 40.0 m from any portion of a building that is over 6 storeys to another building that is over 6 storeys is required. Taller buildings require greater separation distances and therefore will be required to meet the 40.0 m separation distance. These separation distances will ensure that new tall buildings maintain sky views, and develop as an elegant skyline. The current eclectic mix of low and high rise buildings, a diverse housing stock and mature landscaping should be maintained.

**2.4.3 Skyline** - The skyline is the finger print of Port Credit. Key sites should be highlighted in the skyline to ensure way finding. The architecture of the top of buildings should be

unique and should highlight the skyline and the location of the building. It is important to maintain the existing views to Lake Ontario and within Port Credit and ensure that future buildings maintain sky views. The placement and orientation of new buildings should also be oriented to maximize sky views along the length of Lakeshore Road East and the West Corridor so as not to create a wall effect.

New developments will be required to demonstrate how their building fits into the context through photographic imagery.

### City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.2.2.2.** Tall buildings will generally not be permitted.
- **9.5.1.3.** Site designs and buildings will create a sense of enclosure along the street edge with heights appropriate to the surrounding context.
- **9.5.4.5.** Built form will relate to the width of the street right-of-way.

## Response

The proposed development is 10-storeys + MPH in height, and accommodates the appropriate setbacks to allow for building separation, limited shadows and view-angles, resulting in a unique massing form that acts as a beacon for this part of Lakeshore Road East.

As the surrounding area context consists primarily of mid and low-rise commercial and residential developments, the proposal will achieve compatibility with and not result in any negative impacts on the existing built form. Setbacks are incorporated along the faces of the property along the main streets, ensuring adequate transitions, viewing distances and massing articulation between the proposed building and the neighbouring properties.

The proposed height of 10 storeys + MPH, with ample setbacks, along Lakeshore Road East will help to generate

a sense of enclosure along the street without creating a canyon effect.

One of the special exceptions proposed in the Draft Zoning By-law Amendment, that is required to implement the Local Area Plan and facilitate the development on the site as proposed, is a modified separation distance regulation (35 to 40 metres for any portion of a building that is six storeys or higher to another building that is six storeys or higher). Based on the context and the separation provided, there are no adverse effects to overview from the proposed building to adjacent properties.



## MICROCLIMATE

### Port Credit Built Form Guide

**2.4.5 Microclimate** - Tall buildings over 10.7 m can adversely effect the environmental quality of surrounding areas through the diversion of high speed winds and through the overshadowing of the adjacent public realm and residential buildings, including public/private amenity spaces.

The impact of shadows at different times of the day and throughout the year will need to be assessed. The use of architectural devices such as screens, terraces, awnings and also façade setbacks can be adopted to minimize the effects of high speed wind at the base of buildings and of shadow effects.

**2.4.5.1 Shadow Impacts** - Shadow studies will be required for buildings greater than 10.7 m in height which may cause a new shadow impact on adjacent residential properties, properties identified as listed or designated on the Heritage register, and public parkland, open space and the public realm. Particular attention will be focused on Lakeshore Road East and West and Hurontario Street

where a comfortable pedestrian environment is strongly encouraged.

Development applications are to adhere to the City's Standards for Shadow Studies.

**2.4.5.2 Wind Comfort** - The study may be required on development applications higher than 3 storeys or 16.0 m. The criteria to be used for the analysis should be signed and sealed by a certified engineer. The effects of wind on the pedestrian environment should be mitigated in accordance with the parameters set out in the Pedestrian Wind Comfort and Safety Studies.

The evaluation of the existing wind conditions in the immediate and surrounding area, prior to the proposed development will be required along with a comparison of the wind conditions based on the proposed development.

The evaluation will include conditions based on seasonal variations (i.e. summer, spring, fall and winter). The intended use of the area will be considered to determine the appropriate wind conditions that will be permitted.

Areas will be considered appropriate for their intended use if microclimate/wind conditions are satisfied 80% of the time.

## Response

A shadow impact study has been created by IBI Design Architects in accordance with the City's Standards, and it demonstrates that the development meets the City's standards for sun and daylight access on neighbouring properties and in the public realm

A Wind Impact Study was conducted by Gradient Wind Engineers and Scientists, in accordance with the Pedestrian Wind Comfort and Safety Studies set out by the city. The study concluded that conditions over most pedestrian-sensitive areas within and surrounding the development site will be acceptable for the intended pedestrian uses on an annual and seasonal basis.

All secondary building access points (including stairwell exits and vehicle entrances) will be suitable for standing

throughout the year. The rooftop outdoor amenity terrace will be comfortable for sitting or more sedentary activities during the summer, without the need for mitigation.

A comparison of the existing versus future wind comfort surrounding the study site indicates that the proposed development will have a generally neutral influence on grade-level wind conditions, with a few minor exceptions. The laneway directly southwest of the site will experience a slight improvement in pedestrian comfort upon the introduction of the proposed development, while portions of sidewalk along Ann Street and Lakeshore Road East become somewhat windier. Where wind speeds reduce, conditions nevertheless remain acceptable for the intended uses.

## 9.0

# Architectural Design

## FAÇADE ARTICULATION & DESIGN

### Port Credit Built Form Guide

#### 2.3 Community Node Precincts - 2.3.3 Mainstreet Precinct

- e. Blank walls must be avoided facing principal frontages and intersections;
- g. Front building facades should be parallel with the street and provide periodic indentations for visual relief and features such as urban squares;

## Response

The proposed development will contribute to define a unique identity for the Lakeview neighbourhood and Port Credit area through a high quality and distinctive contemporary architectural design that employs innovative design elements and enhances the visual appeal of the area.

The building gradually steps back on the facades along the main streets, thus creating terraces for the residents to enjoy, whilst ensuring an angular view plane from the street.

Each of the four façades is given individual architectural treatment based on their location and context. The southern and eastern facades achieve this through a bold combination of glazing, brick masonry, and spandrel glass in colours that compliment the surrounding developments.

A high percentage of glazing is employed at-grade for the commercial use to create a vibrant frontage along Lakeshore Road East and Ann Street. These features will work in unity to enhance the building's articulation, create zones of interest that break up the massing of the building and achieve a well-articulated rhythm. The northern and western facades have minimal public exposure, and as such include secondary windows, but deploy elements of the front and rear facade such as materials and banding to reflect architectural harmony.



Figure 23 Detail render of the proposed building showing architectural style and character



FAÇADE



ARTICULATION



ANGULAR PLANE



## BUILDING ENTRANCES & GROUND FLOOR

### Port Credit Built Form Guide

#### 2.3 Community Node Precincts - 2.3.3 Mainstreet Precinct

- h. Signage should be integrated with the scale and character of built form;

#### 2.4.6 At Grade Commercial Requirements -

Retail Guidelines:

- a. A main front usable door shall face Lakeshore Road East/West;
- b. Generally retail areas require a minimum of 4.5 m (15 ft.) of clear height from grade and a minimum of 15.0 m (50 ft.) of frontage; c. A minimum of 75% glazing is required for retail storefronts along the street wall;
- d. A minimum 6.0 m (20 ft.) store front extension around the corner from a primary street where retail is required;
- e. Retail tenant signs shall be designed of high quality material, colour and scale in keeping with the design of the building;
- f. Fascia signs shall be limited to the first floor level; g. Tenant signage shall be of a consistent design if there is more than one tenant;
- h. Ground signs are prohibited;
- i. Store front window signage is permitted up to 25% of the glass surface area and shall not block clear views of exits or entrances and shall maintain visibility into the interior of the premises at all times; and
- j. Tables and other active uses adjacent to storefront windows are encouraged where permitted.

#### 2.4.8 Building Frontages - Mixed Use Streets

Buildings fronting onto streets that are required to have retail should be closely spaced with no driveway access points. Front doors shall face Lakeshore Road.

A minimum of 90% of the building face shall front onto Lakeshore Road and be within 0.6 m to 3.0 m of the front property line.

### City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

**9.2.2.6.** Development on Corridors will be encouraged to provide entrances and transparent windows facing the street for non-residential uses;

**9.4.1.3.** Development will support transit and active transportation by requiring front doors that open to the public street;

**9.5.3.2.** Buildings must clearly address the street with principal doors and fenestrations facing the street to:

- ensure main building entrances and at grade uses are located and designed to be prominent, face the public realm and be clearly visible and directly accessible from the public sidewalk;
- provide strong pedestrian connections and landscape treatments that link the buildings to the street; and ensure public safety.

**9.5.3.4.** Principal building entrances should be covered with a canopy, awning, recess or similar device to provide visual prominence and pedestrian weather protection.

## Response

The proposed main building entrance to the development's commercial component faces Lakeshore Road East and the residential access is along Ann Street.

Pedestrian-scaled lighting, clear signage and landscaping will be used to promote an attractive and safe pedestrian pathway to and from the entrance. The pathway connects the building entrance directly to the public sidewalk on Lakeshore Road East, as seen in Figure 24. A high percentage of glazing is featured at-grade to ensure visibility between exterior and interior uses of the building, animating the streetscape as well as providing a means of informal surveillance. The proposal will help to establish an animated streetwall on a rapidly transforming corridor.

Secondary building entrances are provided at the eastern facade of the building serving the residential component of the proposed development, and service access to the commercial uses is at the rear of the building. These entrances will be well-lit with direct sightlines to the public street, as seen in Figure 25.



Figure 24 Detail of the South Elevation, along Lakeshore Road, showing retail access



Figure 25 Detail of the East elevation, facing Ann Street, showing the primary residential lobby access



**ANIMATION**



**PUBLIC REALM**



**PEDESTRIAN-ORIENTED**

## BUILDING MATERIALS

### Port Credit Built Form Guide

**2.4.13 Architectural Expression and Materiality** - New buildings should reference their physical, cultural and historic surroundings through their architectural language and high quality materials. Materials should show sensitivity to their surroundings and should aim to be of the highest quality, responding directly to the existing urban fabric, whether by utilizing similar or sympathetic materials or by positive contrast.

Materials should be chosen with regard to their performance in sustainable terms. The use of local or recycled materials, and/or materials from renewable resources is encouraged.

High quality materials will be required in all new developments. The material most widely found in Port Credit is red tone brick. New developments are encouraged to incorporate this material into their development.

For the mainstreet, vision glass should be used for all store fronts. No spandrel glass will be permitted along Lakeshore Road. Materials that are not dominant in Port Credit are discouraged as the dominant feature of any new development. These materials include architectural concrete block, stucco, spandrel glass and EIFS panels. These materials will not be permitted below the 4th storey. Concrete block is not permitted to be exposed.

Building scale should be broken down through the use of stepping, projections, canopies, trellises, changes in scale, fenestration patterns, materials and finishes.

The private space that extends from the building face to the public right-of-way must be designed in a such a way that it seamlessly blends with the design of the public realm. Future sidewalk/boulevard treatment including site elements such as seating and lighting, should match that of the public right-of-way in order to blur the line between public and private realm.

## City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.5.3.6.** Street facing façades should have the highest design quality. Materials used for the front façade should be carried around the building where any façades are exposed to the public view at the side or rear.
- **9.5.3.11.** Building materials should be chosen for their functional and aesthetic quality, sustainability and ease of maintenance.
- **9.5.3.12.** The choice of building materials should minimize the risk for bird collisions.

## Response

The proposed building will stand out with its innovative use and design of facade materials, and stand as a landmark building in this area. The materials used are vision glass, grey metal and a red-tone brick, that help seamlessly integrate with the neighbourhood's architectural character, while also standing out for its stimulating design.

The proposed building design features floor-to-ceiling windows at-grade facing Lakeshore Road East. Glazing, along with brick is integrated in the design of the upper storeys, and is used skillfully to mask private areas.

Columns and rows of brick articulate the structure of the building, and gradually recede towards the top of the building along the south-east corner, revealing a gold-hued spandrell glass. The alternation of opaque and transparent materials contributes to a distinctive and visually appealing architectural design.

This materiality is carried across each of the four façades, all of which are given individual architectural treatment based on their location and context, while taking into consideration their visual impact on the public realm. The proposed building materials will serve an aesthetic purpose in addition to a functional purpose by promoting passive surveillance and activation along Lakeshore Road East, and providing weather protection and shading. The materials have also been selected based on their sustainable qualities and ease of maintenance.

All glazing up to 16m will be bird-friendly, to minimize the risk of collisions.



Figure 26 East Elevation, along Ann Street, with vehicular entrance at the rear



Figure 27 South Elevation, along Lakeshore Road East, with retail front

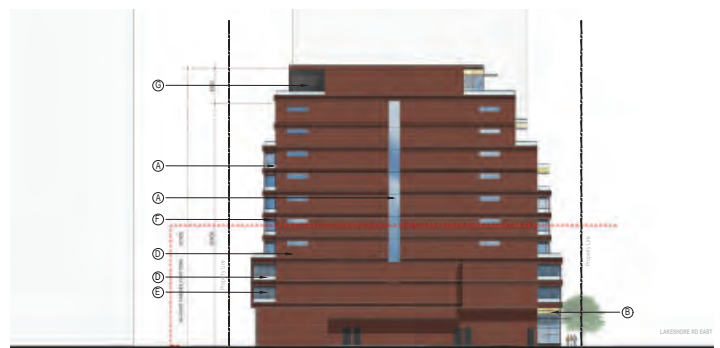


Figure 28 West Elevation, facing adjacent lot, with clerestory windows and stairwell fenestrations to ensure privacy



Figure 29 North Elevation, facing adjacent lot, with a green wall screening the vehicular entryway

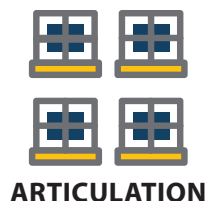


Figure 30 Material palette



## ROOF DESIGN

### Port Credit Built Form Guide

#### 2.4.12 Roof Top Mechanical Penthouse Units

All rooftop units should be internal to the buildings and hidden from public view. All mechanical penthouses should be designed and clad with materials to compliment the building façade.

The portion of the roof not utilized as mechanical penthouse should be developed as green roofs and/or usable outdoor amenity space.

## City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.5.3.17.** Mechanical equipment, vents and metering devices will be integrated into the building design and will not be visible from the public realm.
- **9.5.3.18.** Rooftop mechanicals and appurtenances will be integrated into building design and will not be visible from the public realm and residential developments.

## Response

The proposed roof design features a flat roof to complement the building's orthogonal style and massing, as well as the form of surrounding commercial development. The roof design demonstrates a distinct horizontal frame articulated by changes in materiality for visual interest.

Mechanical equipment is located on the roof, toward the rear of the site, out of pedestrian view. An indoor amenity space is located adjacent to the mechanical area on the roof. Owing to the public and open nature of this amenity space, it is wrapped in glazing, while the mechanical area is covered in brick. The choice of materials reflects the treatment given at the ground level of the building and creates a balance and harmony in the elevation of the building, and avoiding the pop-out of a typical rooftop penthouse.



Figure 31 East Elevation of the building, from Ann Street



**ARTICULATION**

## BUILDING SIGNAGE

# City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.5.7.1.** Signage should:
  - identify businesses and services;
  - promote and enhance an area's character;
  - follow universal design principles.

- **9.5.7.2.** Building and site designs will integrate signage and have regard for the character of the building, landscape and context. Signage should identify and inform as well as complement and enliven the streetscape. Signage must be designed to minimize visual clutter.

## Response

Signage for the proposed mixed use building will be well-integrated into the overall massing and architectural style of the building. Signage is contemplated on the ground floor level only, along a horizontal stone face above the commercial entrance to clearly indicate the use. Signage will be limited to this location to minimize visual clutter.

Signage will be scaled so that it does not obscure any important building features or pedestrian sightlines, while being clearly readable from a range of distances. Signage will incorporate high quality design and materials to complement and promote the character of the Lakeview neighbourhood and add vibrancy to the streetscape.

The design and placement of building signage will be further established through the detailed design stage.



Figure 32 Detail of the South Elevation, along Lakeshore Road East



Figure 33 Examples of well-integrated, inconspicuous, appropriately scaled storefront signage.

# Landscape Design & Public Realm

## LANDSCAPING

### Port Credit Built Form Guide

**2.4.9 Landscape Area** - In the Mainstreet Precinct, where development is intended to create a compact commercial main street, no minimum landscape area will be required, however, a minimum 4.5m landscape buffer is required when a mixed use zone abuts a residential zone.

A landscape buffer is defined as a continuous, open, unobstructed width of land substantially parallel to and adjoining a lot line that is intended for the growth and maintenance of plant material including trees, shrubs and other landscape features such as retaining walls.

### Port Credit Local Area Plan

#### 10.0 Desirable Urban Form

**10.2.1.4** New development will provide for landscape areas that, amongst other matters, address the following:

- a. landscaped character of existing properties and the planned function of the precinct;
- b. provide buffer between uses;

- c. incorporate stormwater best management practices;
- d. enhance the aesthetic quality of the area; and
- e. provide opportunities to enhance the tree canopy.

### City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.5.1.6.** Existing vegetation patterns and preservation and/or enhancement of the Urban Forest will be addressed in all new development.
- **9.5.2.11.** Site development will be required to:
  - incorporate stormwater best management practices;
  - provide enhanced streetscape;
  - provide landscaping that complements the public realm;
  - include the use of native non-invasive plant material;
  - protect and enhance habitat;
  - incorporate techniques to minimize urban heat island effects such as providing planting and appropriate surface treatment; and
  - provide landscaping that beautifies the site and complements the building form.

## Response

Landscaping is provided within the front yard setback and as a continuous buffer along the rear and eastern site perimeter to enhance the public realm and the neighbourhood's urban forest. Landscaping will aid in beautifying the proposed development and framing the building and site edges.

Landscaping will incorporate trees and a variety of plantings that will contribute to a comfortable pedestrian realm by providing shade, a buffer from wind and visual interest within and around the site. Landscaping is used to

screen the entrance to the underground parking, and the service area at the rear of the proposed development from neighbouring properties, maintaining a sense of privacy.

Landscape species will be selected for their drought tolerance, salt resistance and ease of maintenance. Landscaping will include primarily native species that will exhibit seasonal variety.

Private amenity space is also provided for the residential use in the form of balconies, offering additional opportunity for landscaping, as well as a shared indoor and outdoor amenity space at the roof of the building.

## PUBLIC REALM & STREETSCAPING

### Port Credit Built Form Guide

#### 2.4.10 Pedestrian Realm/ Streetscape

New developments should enhance public streets and the open space system by creating a desirable street edge condition that is ideal for the use of pedestrians. Sidewalk width and content should relate to its function.

Parking garage ventilation should not be located at grade along any street frontages and should be integrated into the façade of the building.

#### Port Credit Community Node - Mixed Use

Lakeshore Road East and West require streetscape improvements which will help to revitalize the most important streets in Port Credit.

Additional requirements for building setbacks may be required to achieve the ideal pedestrian experience within the streetscape corridor.

Utilities such as overhead wires and underground cables are important uses that occur within the right of way. Careful consideration will be given when planning streetscape improvements such as tree planting, to ensure compatibility with utilities. The design of utility infrastructure should reflect best practices in design, while ensuring sufficient technical resilience to provide for increasing number and quality of service demanded by the public.

### Port Credit Local Area Plan

#### 7.2 Urban Forest

**7.2.3** Opportunities to improve the tree canopy and promote a healthy urban forest will be reviewed when considering improvements to the streetscape along public roads, with particular attention to the Community Node and Lakeshore Road Corridor.

### City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.3.1.9.** Development and elements within the public realm will be designed to provide continuity of the streetscape and minimize visual clutter.
- **9.3.1.10.** Consideration will be given to the location of utilities on private property and the public right-of-way. Utilities will be grouped or located underground where possible to minimize visual impact. The City encourages utility providers to consider innovative methods of containing utility services.
- **9.5.1.2.** Developments should be compatible and provide appropriate transition to existing and planned development by having regard for continuity and enhancement of streetscapes;
- **9.5.2.5.** Development proponents may be required to upgrade the public boulevard and contribute to the quality and character of streets and open spaces by providing:
  - street trees and landscaping, and relocating utilities, if required;
  - lighting;
  - weather protection elements;
  - screening of parking areas;
  - bicycle parking;
  - street furniture.
- **9.5.2.13.** External lighting for site development should:
  - be energy efficient;
  - utilize dark skylight fixtures; and
  - not infringe on adjacent properties.
- **9.5.4.2.** An attractive and comfortable public realm will be created through the use of landscaping, the screening of unattractive views, protection from the elements, as well as the buffering of parking, loading and storage areas.



Appropriate lighting will be provided at the proposed primary and secondary entrances, around the parking entrance area and along pedestrian pathways both

Parking, storage and mechanical equipment is located to the rear of the site and screened by the proposed building and landscaping so that they will not have any visual impact on the public realm.

The placement and design of lighting, utilities and street furniture will be further established through the detailed design stage.



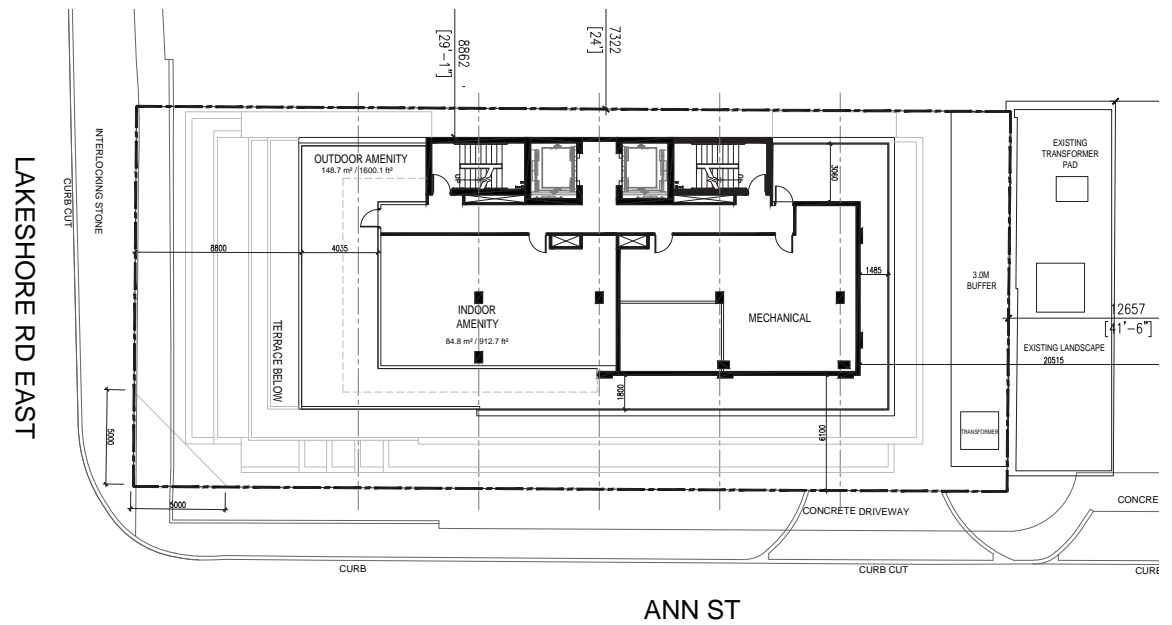


Figure 35 Penthouse Level Plan showing the outdoor and indoor amenity spaces as well as the terraces on the floors below



Figure 36 Examples of streetscape treatments that may be considered along Lakeshore Road East.

# Parking, Servicing & Loading

## Port Credit Built Form Guide

### 2.3 Community Node Precincts - 2.3.3 Mainstreet Precinct

- d. No parking lots or areas should be provided between the building and the street line on principal street frontages, with the exception of on-street parking;
- f. Service loading and garbage storage should be accessed from the rear or side lanes;

### 2.4.11 Parking, Loading and Service Areas - Port Credit Community Node

The design of parking, servicing and loading areas are a key component in the development of sites. These areas serve a functional need, but should be designed in a manner that screens less desirable aspects and provides high quality treatment of exposed areas while addressing safe and efficient movement of pedestrians, cyclists and vehicles.

Service, loading and garbage storage areas should be integrated into the buildings, located interior to the building or alternatively at the rear of the building and screened from the public realm and adjacent residential uses. Parking should be located underground, internal to the building or to the rear of buildings. Above grade parking facilities will be designed to be compatible with the surrounding character through the use of architectural elements that fit with the scale, style and streetscape of the community. Consideration should be given to active pedestrian related uses on the ground floor of the structure in order to improve the animation of street edge conditions. Above grade parking structures should be designed in such a manner that they are integrated into the development. Vehicles should not be visible to the public realm.

## Port Credit Local Area Plan

### 9.2 Parking and Transportation - Demand Management

**9.2.1** Reduced parking requirements and maximum parking standards may be considered within:

- a. the Community Node, particularly in proximity to the GO Station and future LRT stops; and
- b. the Mainstreet Neighbourhood Precinct.

**9.2.2** Public parking lots in the Community Node and Mainstreet Neighbourhood Precinct will be maintained and supplemented, where appropriate.

## City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

• **9.2.2.6.** Development on Corridors will be encouraged to:

- not locate parking between the building and the street;
- consolidate access points and encourage shared parking, service areas and driveway entrances;

• **9.5.5.1.** Parking should be located underground, internal to the building or to the rear of buildings.

• **9.5.5.3.** Where surface parking is permitted, the following will apply. Parking should:

- not be located between the building and the street;
- incorporate stormwater best management practices, such as, permeable paving, bioretention areas and tree clusters;
- incorporate universal design principles;
- be configured to permit future development;
- have appropriate landscape treatment including trees and lighting, throughout parking lots;
- provide appropriate landscape treatment to provide shading of parking areas;

• **9.5.5.6.** Site plans will demonstrate the ability for shared servicing access between adjacent developments.

• **9.5.5.7.** Service, loading and garbage storage areas should be internal to the building or located at the rear of the building and screened from the public realm.

## Response

Residential parking associated with the proposed development is located on three basement levels, with the entrance to the rear of the property away from public view. The basement parking accommodates 37 total car parking spaces, and 52 bike parking spaces for the residents.

Parking is accessed via a driveway from Ann Street, and two internal parking elevators. The development features a landscaped buffer as well as a green wall to screen the entrance to the underground parking and the service area. The underground parking is accessed via two car and cycle elevators.

According to the Parking Study prepared by LEA Consulting Inc., the swept path of passenger vehicles entering the car elevators the building illustrate that sufficient aisle width is provided for vehicles to safely enter and exit.

Garbage storage areas are located at the rear of the proposed building and are enclosed within the building envelope so as not to be visible from the public realm.

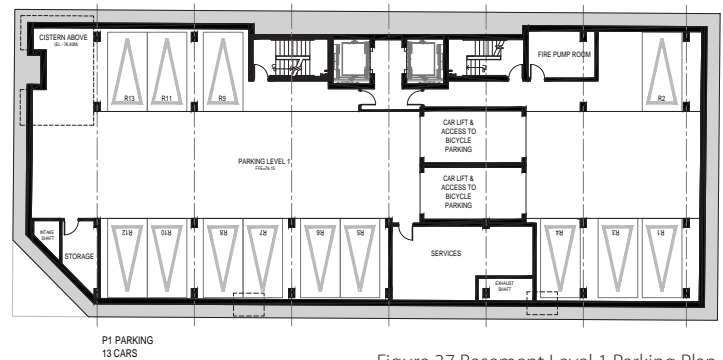


Figure 37 Basement Level 1 Parking Plan

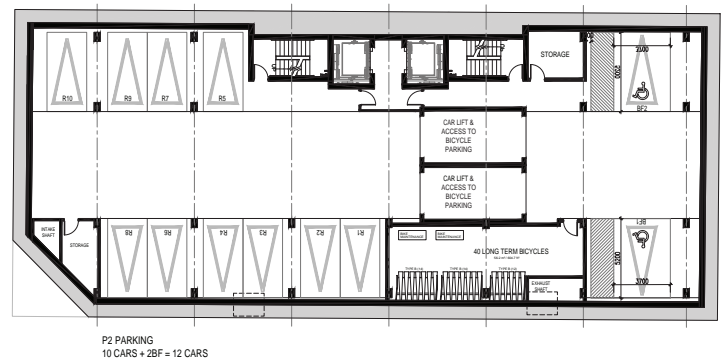


Figure 38 Basement Level 2 Parking Plan

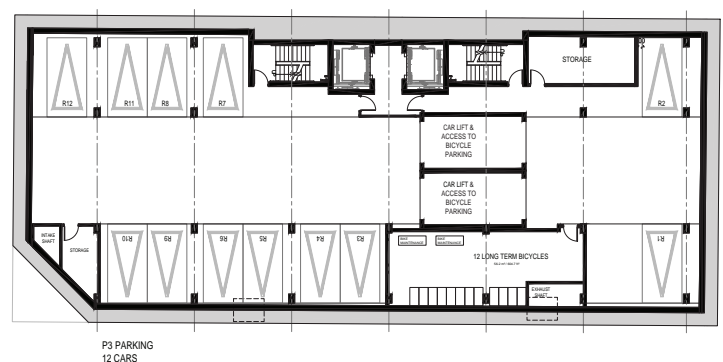


Figure 39 Basement Level 3 Parking Plan





# Sustainable Initiatives

## SUSTAINABLE BUILDING DESIGN

### Port Credit Built Form Guide

#### 4.0 Environmental Sustainability

New developments must be sustainable in all aspects, taking into account social and economic impact, based on whole life costs and benefits. Advances in construction technology combined with a growing body of architectural knowledge mean that sustainable practices should be easily achievable.

The City strongly encourages applicants to incorporate green sustainable elements into proposed buildings, site works, construction methods and long-term maintenance programs. Further, the City also encourages that applicants pursue LEED-NC credits required to achieve Silver certification.

## City of Mississauga Official Plan Chapter 9 - Build a Desirable Urban Form

- **9.1.10.** The city vision will be supported by site development that utilizes best sustainable practices;
- **9.1.13.** Development will have positive, restorative, ecological benefits on a site through the practice of sustainable building and site design.
- **9.5.2.8.** Site designs that conserve energy will be encouraged. Energy conservation will be addressed at the development application stage and during the preparation of building and site designs. Buildings should be designed, oriented, constructed and landscaped to minimize interior heat loss and to capture and retain solar heat energy in the winter and to minimize solar heat penetration in the summer.
- **9.5.2.9.** Site designs will be encouraged that minimize the consumption of water.
- **9.5.2.10.** Site development will be encouraged to meet a minimum standard of LEED Silver or custom green development standards.
- **9.5.3.13.** Where appropriate, development should be designed to incorporate measures that minimize urban heat island effects.
- **9.5.3.14.** Buildings should be designed to conserve energy and incorporate sustainable material.
- **9.5.3.15.** Buildings should be designed to minimize the consumption of water and to utilize stormwater best management practices.

## Response

The proposal incorporates Low Impact Development features that will serve to enhance the long-term sustainability of the site and surrounding community while simultaneously offering aesthetic benefits.

The proposed site design incorporates low impact features, such as, the promotion of active forms of transportation through the creation of a comfortable and accessible pedestrian environment, transit use through proximity to existing and proposed transit systems, as well as the selection of energy efficient materials and practices.

A construction waste management plan will be implemented in the construction process to divert recyclable material from landfill. Where possible, construction materials will be chosen for their low carbon footprint and sourced responsibly to reduce carbon footprint of the shipment of materials.

A target of 50% of all proposed planting will be native, where feasible. Seven shade trees, approximately 6-8 metres apart, will be provided along all street frontages and public walkways with sufficient soil volume. This will ensure a climate positive landscape design.

All non-amenity roof areas will be designed with intensive green roof systems where feasible. Outdoor rooftop amenity areas will be designed with raised planting beds and high albedo paved surfaces to reduce heat island effect.

Rainwater harvesting systems which intercept, convey and store rainfall for irrigation uses will be proposed. High-efficiency toilets and plumbing fixtures will be used to reduce water consumption.

Benchmarking and sub-metering will ensure energy performance can be tracked and improved over time through resident awareness while making the building more resilient to power disruptions and encourage the use of renewable energy.

Airtightness testing will be undertaken to ensure the building envelope integrity minimizes infiltration and or ex-filtration, thus ensuring indoor temperatures are efficiently maintained.

Sustainable design features will be addressed further through the detailed design stage.



**SUSTAINABILITY**

# Conclusion

Based on our review of the Port Credit Area Plan, Port Credit Built Form Standards as well as the City of Mississauga's Official Plan, it is our opinion that the proposal adheres to the vision and design direction for the Port Credit neighbourhood and the Lakeshore Corridor. Overall, the proposal represents an exemplary design that promotes intensification and a mix of uses, and provides sensitive transition to the surrounding neighbourhoods while contributing to placemaking in the community.

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# Design Terms



## ACCESSIBILITY

Providing for ease, safety, and choice when moving to and through places



## ADAPTIVE REUSE

Converting an existing building into a new use



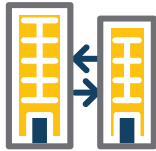
## ANGULAR PLANE

A geometric measurement that maintains solar access and height transition



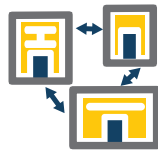
## ANIMATION

Support sustained activity on the street through visual details, engaging uses, and amenities



## COMPATIBILITY

Similar size, form and character of a building relative to others around it



## CONNECTIVITY

The ease of movement and access between a network of places and spaces



## DESIRE LINE

Shortest or most easily navigated route marked by the erosion of the ground caused by human traffic



## FACADE

The exterior wall of a building exposed to public view



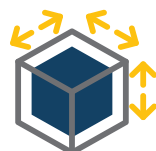
## HEIGHT TRANSITION

The gradual change in height between buildings within a community



## LANDMARK

Highly distinctive buildings, structures or landscapes that provide a sense of place and orientation



## MASSING

The effect of modifying the height and bulk of the form of a building or group of buildings



## NODE

A place where activity and circulation are concentrated



## STEP BACK

A recess of taller elements of a building in order to ensure an appropriate built form presence on the street edge



## STREETWALL

The consistent edge formed by buildings fronting on a street



## STREET FURNITURE

Municipal equipment placed along streets, including light fixtures, fire hydrants, telephones, trash receptacles, signs, benches, mailboxes, newspaper boxes and kiosks



## SUSTAINABILITY

Developing with the goal of maintaining natural resources and reducing human impact on ecosystems



### ARTICULATION

The layout or pattern of building elements (e.g. windows, roofs) that defines space and affects the facade



### BUILT FORM

The physical shape of developments including buildings and structures



### CHARACTER

The look and feel of an area, including activities that occur there



### CIRCULATION

The movement patterns of people and vehicles through a site or community



### FIGURE GROUND

The visual relationship between built and unbuilt space



### FINE GRAIN

A pattern of street blocks and building footprints that characterize an urban environment



### FOCAL POINT

A prominent feature or area of interest that can serve as a visual marker



### GATEWAY

A signature building or landscape to mark an entrance or arrival to an area



### PEDESTRIAN-ORIENTED

An environment designed to ensure pedestrian safety and comfort for all ages and abilities



### PUBLIC REALM

Public spaces between buildings including boulevards and parks; where pedestrian activities occurs



### RHYTHM AND PATTERN

The repetition of elements such as materials, details, styles, and shapes that provide visual interest



### SETBACK

The orientation of a building in relation to a property line, intended to maintain continuity along a streetscape



### URBAN FABRIC

The pattern of lots and blocks in a place



### VIEW TERMINUS

The end point of a view corridor, often accentuated by landmarks



### VISTA

Direct and continuous views along straight streets or open spaces



### WAYFINDING

Design elements that help people to navigate through an area (e.g. signs, spatial markers)



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