



## MISSISSAUGA

### NOTICE OF THE PASSING OF AN OFFICIAL PLAN AMENDMENT Subsection 17(24) of the Planning Act

DATE OF NOTICE	August 18, 2022	
OPA NUMBER	OPA 145 (By-law 0193-2022)	
DATE PASSED BY COUNCIL	August 10, 2022	
LAST DATE TO FILE APPEAL	<b>September 07, 2022</b>	
FILE NUMBER	CD.03-DOW – OPA 145	Wards 4 & 7
APPLICANT	City of Mississauga	
PROPERTY LOCATION	The lands affected by this Amendment are located in the Downtown Fairview, Cooksville and Hospital Character Areas, as identified in Mississauga Official Plan.	

A decision was made on the date noted above to approve Official Plan Amendment Number 145 to the **Mississauga Official Plan** for the City of Mississauga as adopted by **By-law 0193-2022**. Council has considered the written and oral submissions from the public on this matter.

**THE PURPOSE AND EFFECT** of this amendment is to establish a vision and set of guiding principles and to add and revise policies on community infrastructure, parks, urban design, roads and pedestrian connections for the Downtown Fairview, Cooksville and Hospital Character Areas. A copy of By-Law **0193-2022** adopting this Amendment is attached.

#### WHEN THE DECISION IS FINAL

The proposed official plan amendment is exempt from approval by the Regional Municipality of Peel. The decision of the City of Mississauga is final if a Notice of Appeal is not received on or before the last date of appeal noted above.

**IF YOU WISH TO APPEAL** to the Ontario Land Tribunal, a copy of an appeal form is available from the OLT website at <https://olt.gov.on.ca/>. An appeal may be filed by registered mail or courier addressed to the Clerk of the City of Mississauga, Attention: Diana Rusnov, 300 City Centre Drive, Mississauga, Ontario L5B 3C1 no later than **September 07, 2022**

#### WHO CAN FILE AN APPEAL

Only individuals, corporations or public bodies may appeal a decision of the City of Mississauga to the Ontario Land Tribunal. A notice of appeal may not be filed by an unincorporated association or group. However, a notice of appeal may be made in the name of an individual who is a member of the association or the group on its behalf.

No person or public body shall be added as a party to the hearing of the appeal unless, before the plan was adopted, the person or public body made oral submissions at a public meeting or written submissions to the Council of the City of Mississauga or, in the opinion of the Ontario Land Tribunal, there are reasonable grounds to add the person or public body as a party.

The Notice of Appeal must:

- (1) set out the specific part of the proposed official plan amendment to which the appeal applies;
- (2) set out the reasons for the request for the appeal;
- (3) be accompanied by the fee prescribed under the *Ontario Land Tribunal Act* in the amount of \$1,100.00 per application, payable by certified cheque or money order to the Minister of Finance. A copy of the Ontario Land Tribunal Fee Schedule may be found at <https://olt.gov.on.ca/appeals-process/fee-chart/>; and
- (4) be accompanied by a fee in the amount of \$300.00 payable to the City of Mississauga.

#### MORE INFORMATION

A copy of this amendment in its entirety can be found at [www.mississauga.ca/portal/cityhall/publicnotices](http://www.mississauga.ca/portal/cityhall/publicnotices), or from **Christian Binette** of the City of Mississauga, Planning and Building Department at (905) 615-3200 X 5753.

Sacha Smith, Manager and Deputy Clerk  
Legislative Services,  
Corporate Services Department  
905-615-3200 ext. 4516



THE CORPORATION OF THE CITY OF MISSISSAUGA

BY-LAW NUMBER 0193-2022

A by-law to Adopt Mississauga Official Plan Amendment No. 145

WHEREAS in accordance with the provisions of sections 17 or 21 of the *Planning Act*, R.S.O. 1990, c.P.13, as amended, ("*Planning Act*") Council may adopt an Official Plan or an amendment thereto;

AND WHEREAS, pursuant to subsection 17(10) of the *Planning Act*, the Minister of Municipal Affairs and Housing authorized the Regional Municipality of Peel, ("Region" or "Regional"), an approval authority, to exempt from its approval any or all proposed Local Municipal Official Plan Amendments;

AND WHEREAS, Regional Council passed By-law Number 1-2000 which exempted all Local Municipal Official Plan Amendments adopted by local councils in the Region after March 1, 2000, provided that they conform with the Regional Official Plan and comply with conditions of exemption;

AND WHEREAS, the Commissioner of Public Works for the Region has advised that, with regard to Amendment No. 145, in his or her opinion the amendment conforms with the Regional Official Plan and is exempt;

AND WHEREAS, Council desires to adopt certain amendments to Mississauga Official Plan regarding S changes and mapping modifications within the Downtown Fairview, Cooksville and Hospital Character Areas;

NOW THEREFORE the Council of The Corporation of the City of Mississauga ENACTS as follows:

1. The document attached hereto, constituting Amendment No. 145 to Mississauga Official Plan, is hereby adopted.

ENACTED and PASSED this 10<sup>th</sup> day of August, 2022.

Approved by Legal Services <b>City Solicitor</b> City of Mississauga
<b>MEM</b>
Michal E. Minkowski
Date: July 29, 2022
File: CD.03-DOW

*Bonnie Crombie*

MAYOR

*W.D. Minkowski*

CLERK



**Amendment No. 145**

**to**

**Mississauga Official Plan**

By-law No. 0193-2022

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ENACTED and PASSED this 10<sup>th</sup> day of August, 2022.

Signed Bonnie Crombie  
MAYOR

Signed [Signature]  
CLERK

**Amendment No. 145**  
**to**  
**Mississauga Official Plan**

The following text attached constitutes Amendment No. 145.

Also attached but not constituting part of the Amendment are Appendices I and II.

Appendix I is a description of the Public Meetings held in connection with this Amendment.

Appendix II is a copy of the Planning and Building Department report dated June 10, 2022, pertaining to this Amendment.



## **PURPOSE**

The purpose of this Amendment is to establish a vision and set of guiding principles and to add and revise policies on community infrastructure, parks, urban design, roads and pedestrian connections for the Downtown Fairview, Cooksville and Hospital Character Areas.

## **LOCATION**

The lands affected by this Amendment are located in the Downtown Fairview, Cooksville and Hospital Character Areas, as identified in Mississauga Official Plan.

## **BASIS**

Mississauga Official Plan came into effect on November 14, 2012, save and except for the outstanding site specific appeals to the Ontario Land Tribunal.

The amendments to the Downtown Fairview, Cooksville and Hospital Character Areas provide further policies to guide, accommodate and support growth.

The proposed Amendment is acceptable from a planning standpoint and should be approved for the following reasons:

1. The proposed Amendment supports the Hurontario/Main Street Corridor Master Plan and Dundas Connects Master Plan by facilitating transit-supportive development and it supports Vision Cooksville to achieve a complete community.
2. The proposed Amendment implements Mississauga Official Plan key guiding principles that promote a strong and sustainable diversified economy; support the creation of a distinct, vibrant and complete community with a mix of uses including housing, jobs, parks and open spaces, community infrastructure and cultural uses; plan for new roads and pedestrian connections; and promote good urban design and a high quality public realm.

Notes included throughout this Amendment are for information purposes only and do not form part of this Amendment.

## **DETAILS OF THE AMENDMENT AND POLICIES RELATIVE THERETO**

1. Section 12.1.1, General, Downtown, of Mississauga Official Plan, is hereby amended by deleting Policy 12.1.1.1 and replacing it with the following:

12.1.1.1 Proponents of development applications within the Downtown may be required to demonstrate how new development contributes to a concentration and mix of jobs as a key component of a mixed use transit-supportive development.

2. Section 12.1.1, General, Downtown, of Mississauga Official Plan, is hereby amended by adding Policy 12.1.1.7 as follows:

12.1.1.7 Prior to development within floodplains, the development proponent will assess impacts, identify flood management measures and mitigate flood hazards to the satisfaction of the City and the appropriate conservation authority.

3. Section 12.1, Introduction, of Mississauga Official Plan, is hereby amended by adding Section 12.1.8 as follows:

### **12.1.8 Downtown Fairview, Cooksville and Hospital**

The following additional policies apply to the Downtown Fairview, Downtown Cooksville and Downtown Hospital Character Areas.

#### **12.1.8.1 General**

12.1.8.1.1 Partnerships and collaborations will be encouraged to identify community needs and develop community infrastructure such as integrating a school, a daycare facility or a community facility within a building with other uses.

12.1.8.1.2 Downtown Fairview, Cooksville and Hospital will develop with a high quality, well-designed and expanded network of public parkland and open space that is green, safe, attractive, and supports a range of social and recreation activities. Parkland may also provide gathering spaces and connections to existing and future parks and open spaces, commercial developments, community infrastructure and to surrounding neighbourhoods.

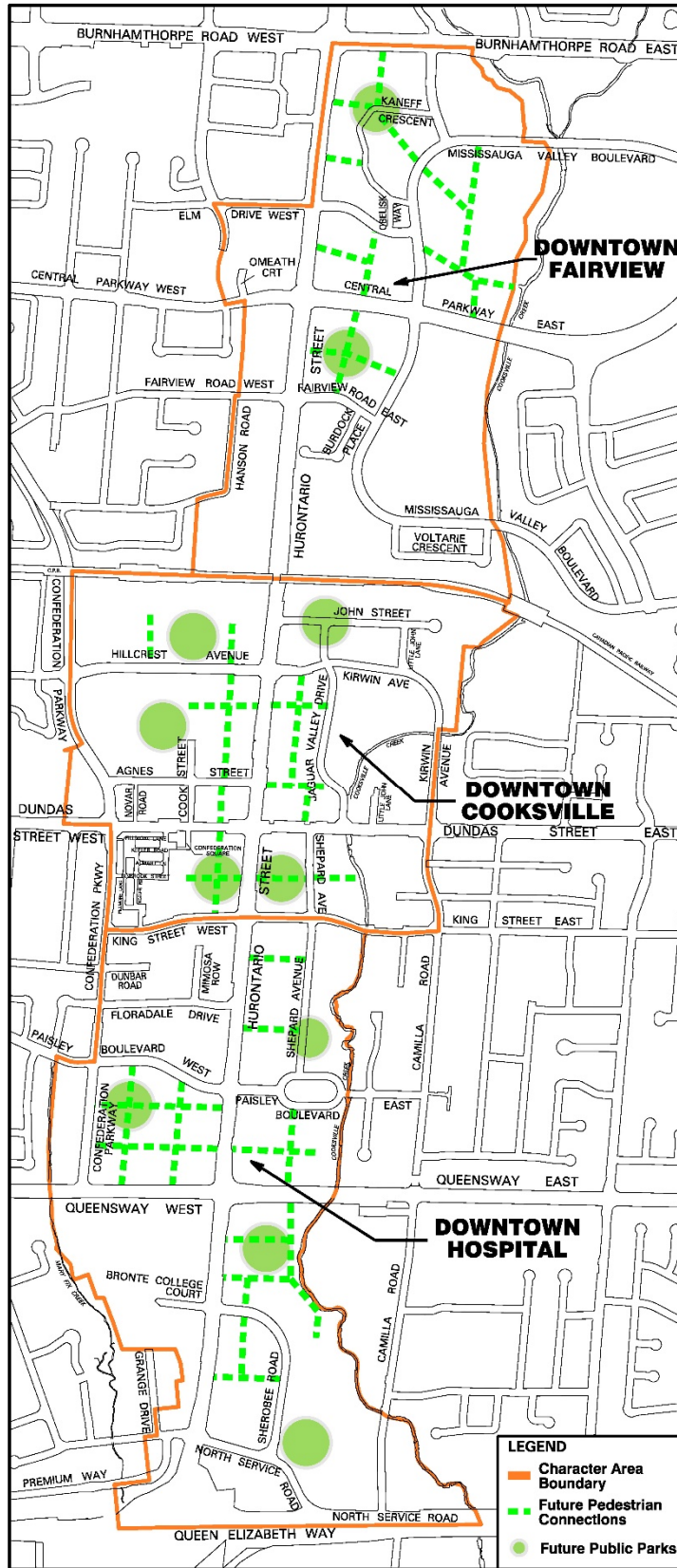


Figure 1: Future pedestrian connections and public parkland network. These symbols represent the general location of new pedestrian connections, as well as public parkland in the vicinity.



12.1.8.1.3 As part of the development application review process, the City will determine the location, configuration and size of new and/or expanded parkland blocks, and the requirement for land dedication. Opportunities to obtain parkland will also be explored for purchase by the City.

12.1.8.1.4 Through development applications, opportunities to increase the provision of open space will be explored. These open spaces will be complementary to the public parkland system.

12.1.8.1.5 Strategies to support the retention of local businesses and increase employment uses may be pursued including Community Improvement Plans and other incentives.

### **12.1.8.2 Urban Form and Building Transition**

12.1.8.2.1 New buildings will achieve a high quality urban design and built form, and will be designed and located to:

- a. create a transition in height generally consistent with a 45 degree angular plane that is measured from the property line adjacent to residential low and medium density land use designations;
- b. generally maintain a minimum separation distance of 30 metres between portions of buildings that are greater than six storeys;
- c. add visual interest by varying the massing of buildings; and
- d. promote visibility and interest from the street through the use of high quality materials and architectural detailing in the design of the podium.

12.1.8.2.2 For larger developments where more than one tower is proposed, a variation in building heights will be achieved.

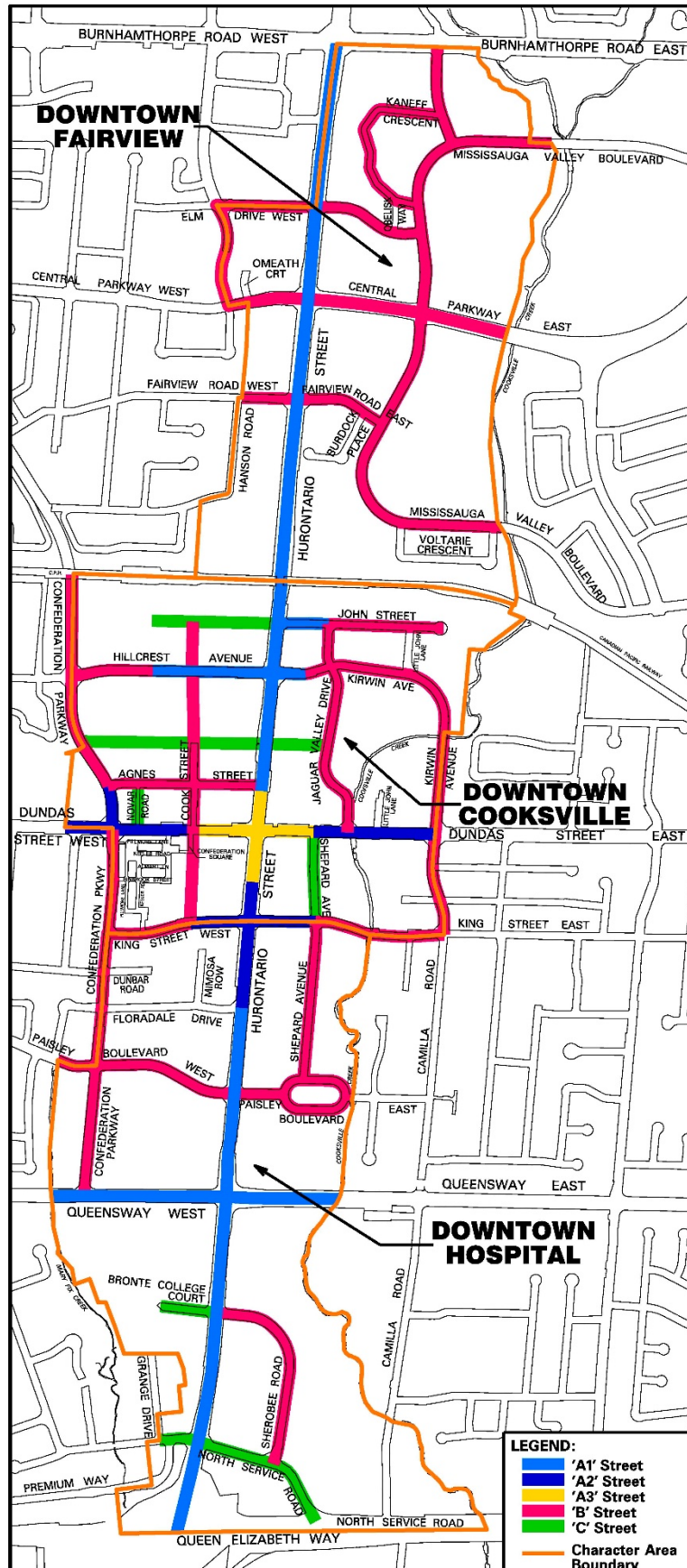
12.1.8.2.3 **Tall buildings** will incorporate podiums that are a minimum of three storeys and a maximum of six storeys.

12.1.8.2.4 The floor plate of buildings above the podium will vary in size depending on the height of the building. For **tall buildings**, the tower above the podium will have a smaller floor plate size.

12.1.8.2.5 Infill development on lands immediately adjacent to the Hurontario Street right-of-way will have buildings located along the street edge, where possible.

### **Street Frontage Provisions**

The majority of existing and new roads in Downtown Fairview, Downtown Cooksville and Downtown Hospital are categorized as 'A', 'B' or 'C' Streets and are shown on Map 12-1.2: Downtown Fairview, Cooksville and Hospital Street Types.



Map 12-1.2: Downtown Fairview, Cooksville and Hospital Street Types

## Provisions for 'A' Streets

12.1.8.2.6 'A1', 'A2' and 'A3' Streets will have a strong sense of place and a main street, pedestrian friendly character with active, mixed use building frontages. Buildings fronting these streets will reinforce a distinct high quality built form, public realm, landscaping and pedestrian amenities.

12.1.8.2.7 Development abutting 'A1', 'A2' and 'A3' Streets will incorporate ground floor non-residential uses. Development will:

- a. promote a main street character with smaller retail units and frequent entrances accessible from the public sidewalk;
- b. frame and animate streets and public spaces, with active building frontages including storefronts, prominent entrances and residential lobbies linking the building and the sidewalk;
- c. provide a consistent streetwall with building indentations as visual relief;
- d. design non-residential units at street corner locations with animated frontages that wrap the corner;
- e. provide generous setbacks from the street to accommodate, among other things, landscaping, street furniture, wayfinding, bus shelters, pedestrian-scaled lighting and outdoor patios;
- f. provide appropriate stepbacks between the edge of the podium and tower portion of the building; and
- g. coordinate private land abutting the sidewalk with the public boulevard to create an integrated design character and reinforce the main street context. Opportunities for the development of entry forecourts and plazas will be encouraged in order to create a varied **streetscape**.

## Provisions for 'B' Streets

12.1.8.2.8 Development along 'B' Streets will primarily support housing and pedestrian access and movement. Although 'B' Streets are primarily residential in nature, development may include some non-residential uses on the ground floor. The design of the built form and streetscape along 'B' Streets will establish a unified character while ensuring adequate light and skyviews, and a seamless interface and transition between the public and private realms. Development will address the following:

- a. a consistent streetwall with building indentations will be provided as visual relief;
- b. generous setbacks from the street to accommodate, among other things, landscaping, terraces, bus shelters, and pedestrian scaled lighting;



- c. a high quality seamless interface and transition between the public sidewalk and principal building entrance to ensure adequate separation, definition and privacy; and
- d. appropriate stepbacks between the edge of the podium and tower portion of the building.

#### Provisions for 'C' Streets

12.1.8.2.9 Development along 'C' Streets will support a pedestrian environment and vehicular access to parking and other service areas such as deliveries, garbage pick-up, service and loading. 'C' Streets may also support residential, retail and service commercial uses. Development should address the following:

- a. opportunities to consolidate service, parking and loading to serve a number of buildings;
- b. ensure the visual impact of the parking, loading and service areas from the street is minimized and screened from view; and
- c. provide a secondary entrance for pedestrian access, where appropriate.

#### 12.1.8.3 Transportation

12.1.8.3.1 Development will be required to provide pedestrian connections to connect to the Light Rail Transit (LRT) line, transit routes/stops, trails, and parks and open spaces.

12.1.8.3.2 Public easements will be required where pedestrian connections are proposed on private lands.

12.1.8.3.3 Vehicular access from Hurontario Street and Dundas Street will be restricted and consolidated to minimize driveways and conflict with pedestrians, cyclists and transit.

12.1.8.3.4 Roads will be designed as complete streets, and incorporate **active transportation** and transit infrastructure.

12.1.8.3.5 Existing arterial and collector roads bisecting and surrounding the Character Areas will incorporate **active transportation** and transit infrastructure when redesigned, as appropriate.

4. Section 12.3, Downtown Fairview Character Area, of Mississauga Official Plan, is hereby amended by renumbering Sections 12.3.1 and 12.3.2 to 12.3.4 and 12.3.5 and adding Sections 12.3.1 and 12.3.2 as follows:

NOTE: 12.3.3 - see Mississauga Official Plan Amendment No. 146

### 12.3.1 Introduction

Downtown Fairview is situated on the periphery of Mississauga's Downtown Core and provides an area of transition to Downtown Cooksville. Downtown Fairview contains a number of sub-areas, each with its own unique character and built form.

Along Mississauga Valley Boulevard, 'tower in the park' developments, with generally 20 to 24 storey apartment buildings are surrounded by large open space areas with connections to trails and the Cooksville Creek. The area at Hurontario Street and Elm Drive West contains the greatest densities in Downtown Fairview, with point towers as tall as 50 storeys. The townhouses located in the southern end of Downtown Fairview are the predominant built form and provide housing for families.

There are limited retail and commercial uses along Hurontario Street, however with planned **higher order transit**, new developments can transform the street into vibrant places to shop, work and visit. Anchored by a grocery store, the existing retail site located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard provides convenient shopping for nearby residents.

The Cooksville Creek is a significant **natural heritage feature** that runs north-south in Downtown Fairview and drains into Lake Ontario. Opportunities to enhance the ecological features of existing natural habitats and the urban forest will be encouraged.

The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.

### 12.3.2 Vision

Downtown Fairview will be a distinct, walkable community that provides a transition to lower heights and densities from the Downtown Core. With more shops and amenities along Hurontario Street, Downtown Fairview will be an attractive place where people can access their daily needs within a short distance from their homes and places of work. Mixed use development located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard will serve the retail and commercial needs of the local community. The area along Mississauga Valley Boulevard will continue to be defined by existing towers in the park and beautiful green spaces, with improved connections to trails and Cooksville Creek.

An expanded pedestrian network will connect new development with nearby amenities and residential areas, and improve access to transit, parks and open spaces.

The vision is based on the following guiding principles:

1. Heights and densities that are appropriate to the planned context. The greatest heights will be located at Hurontario Street and Elm Drive West,

with a transition to lower building heights and densities for new developments south along Hurontario Street and east towards Mississauga Valley Boulevard;

2. An attractive, walkable environment along Hurontario Street with a vibrant mix of shops, restaurants, cafes and service establishments;
  3. Accessible public spaces for all, with enhanced connections for walking and cycling; and
  4. A mix of housing forms and tenure with a range of housing options that meet the needs of a diverse community.
5. Section 12.4, Downtown Cooksville Character Area, of Mississauga Official Plan, is hereby amended by deleting Sections 12.4.1 and 12.4.2.
  6. Section 12.4, Downtown Cooksville Character Area, of Mississauga Official Plan, is hereby amended by renumbering Sections 12.4.3 and 12.4.4 to 12.4.8 and 12.4.9.

NOTE: 12.4.5 and 12.4.7 - see Mississauga Official Plan Amendment No. 146

7. Section 12.4, Downtown Cooksville Character Area, of Mississauga Official Plan, is hereby amended by adding Sections 12.4.1, 12.4.2, 12.4.3, 12.4.4 and 12.4.6 as follows:

#### **12.4.1 Introduction**

Centred at Hurontario Street and Dundas Street, Downtown Cooksville has a rich history as a destination for newcomers to Canada and as a place for entrepreneurs and small businesses to set up shop and thrive. The cultural diversity of the neighbourhood is reflected in the variety of shops, restaurants and services that are available in the area. Immigrant programs and services in Downtown Cooksville provide an important function in helping families transition to a new country.

Hurontario Street and Dundas Street are generally characterized by retail plazas that are one to two storeys with residential and/or office uses above and surrounded with surface parking. There are a few office and residential buildings along the **corridors** that are generally of a low or mid-rise form.

With the potential for two-way, all-day GO Transit service at Cooksville GO Station, planned Bus Rapid Transit (BRT) along Dundas Street and Light Rail Transit (LRT) along Hurontario Street, significant transit investments will strengthen its connection to the rest of Mississauga and beyond. These transit investments will increase access to employment, shopping, entertainment and other destinations, while also attracting more visitors. Transit-supportive development will ensure transit investments are supported with greater access to housing, jobs and services.

The Cooksville Creek is a significant **natural heritage feature** in Downtown Cooksville. Cooksville Creek serves as a wildlife corridor and linkage to other



features within and beyond Downtown Cooksville. This corridor is also used for recreation with opportunities for restoration, rehabilitation and hazard mitigation along the **watercourse**.

The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.

#### **12.4.2 Downtown Cooksville as a 15-Minute City**

Downtown Cooksville will continue to reflect its welcoming and multicultural heritage while integrating key elements of a 15-minute city concept. Downtown Cooksville is walkable and liveable, where the basic needs of everyday life are accessible and can easily be reached within a 15-minute walk. It has amenities that serve the community - parks, groceries, jobs, medical clinics, pharmacies, schools, shops, restaurants, and more - that can be conveniently accessed without the use of a car.

#### **12.4.3 Vision**

Downtown Cooksville will be a walkable, mixed use community with places for community amenities, culture and art. Hurontario and Dundas streets will be animated with storefronts and other active uses that support existing and future local businesses. Downtown Cooksville will continue to offer a diversity of local retailers and restaurants. The area immediately surrounding the intersection of Hurontario and Dundas Streets (Cooksville Corners) will be developed with a vibrant main street focus that includes a mix of uses and a human-scaled built form. The Cooksville GO Station area will be a focal point for transit-supportive, higher density development with a concentration and mix of residential, community, office, retail and service commercial uses.

The vision is based on the following guiding principles:

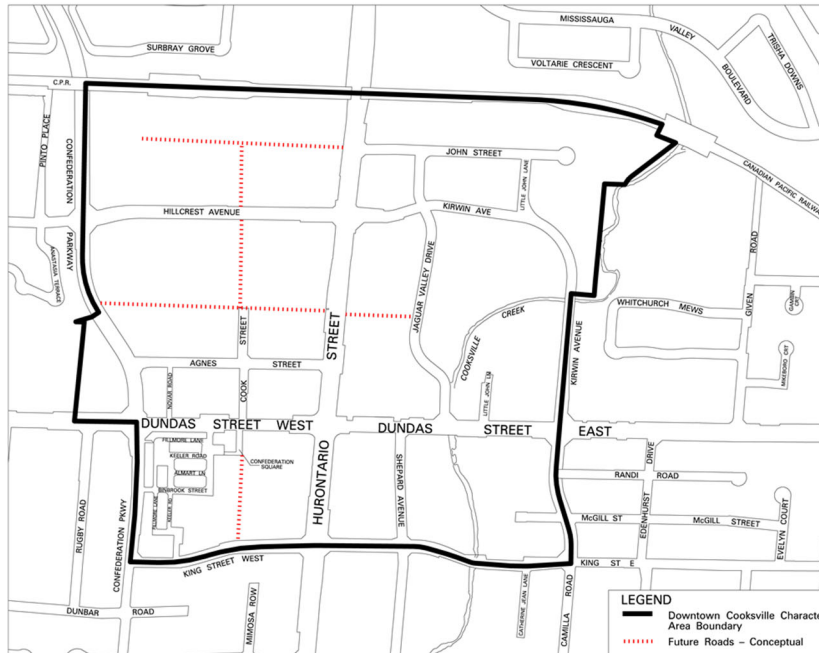
1. Establish a mixed use, vibrant community – create a range of local and unique businesses along Hurontario and Dundas streets, spaces for educational, cultural and community uses, a vibrant public realm, and a network of connected parks and open spaces;
2. Plan for more people and employment – provide a range of housing options both in type and affordability, and jobs with the greatest heights and densities located in proximity to the Cookville GO Station;
3. Achieve a walkable, connected community – promote a pedestrian-oriented main street character along Hurontario and Dundas streets, public art and a vibrant public realm, and improve pedestrian and cycling infrastructure; and
4. Plan for high quality transit – provide convenient and efficient transit with seamless connections to local and **higher order transit** services.

## 12.4.4 General

12.4.4.1 Arts and cultural uses are encouraged to concentrate in Downtown Cooksville to add vibrancy to the area.

## 12.4.6 Transportation

12.4.6.1 The road network identified on Map 12-4.2 will support improved connectivity within Downtown Cooksville for pedestrian and cycling movement and to transit.



Map 12-4.2: Downtown Cooksville Character Area Future Roads

12.4.6.2 Improvements to the road network will be achieved through development. Future additions to the road network include, but are not limited to the following:

- a new east-west road from Confederation Parkway to Jaguar Valley Drive on the southern boundary of T.L. Kennedy Secondary School and former Melissa Street, aligning with a new road on the east side of Hurontario Street;
- extension of Cook Street northerly from the southern boundary of T.L. Kennedy Secondary School to connect with John Street; and
- extension of Cook Street southerly from Dundas Street West to connect with King Street West.

12.4.6.3 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. all roads will be public;

- b. the design and access requirements for roads will be determined through the development application review process; and
  - c. minor adjustments to the road network may be made without an amendment to Map 12-4.2 at the City's discretion to accommodate development and breaking up large blocks with roads and pedestrian connections.
8. Section 12.4.3.1 (existing), Site 1, Downtown Cooksville Character Area, of Mississauga Official Plan, is hereby amended by deleting "provisions of the Mixed Use and Utility designations, the Urban Design Policies, and other provisions" from Policy 12.4.3.1.2 (existing) and replacing it with "policies".
9. Section 12.4.3.1 (existing), Site 1, Downtown Cooksville Character Area, of Mississauga Official Plan, is hereby amended by deleting Policy 12.4.3.1.2 (existing) 'b', 'c' and 'e' and replacing 'b' and 'c' with the following:
- b. **tall buildings** will incorporate podiums that are reflective of a main street character; buildings along Hurontario Street and Dundas Street will incorporate a generous setback between the edge of the podium and tower portion of the building;
  - c. to achieve vibrant walkable spaces, buildings along pedestrian connections will promote animation at-grade with outdoor patios and storefront entrances to individual retail and service commercial units; and
10. Section 12.5, Downtown Hospital Character Area, of Mississauga Official Plan, is hereby amended by deleting Section 12.5.3.
11. Section 12.5, Downtown Hospital Character Area, of Mississauga Official Plan, is hereby amended by renumbering Sections 12.5.2 and 12.5.4 to 12.5.4 and 12.5.6.

NOTE: 12.5.3 and 12.5.5 - see Mississauga Official Plan Amendment No. 146

NOTE: Policy 12.5.1.1 proposed to be deleted in Mississauga Official Plan Amendment No. 146

12. Section 12.5, Downtown Hospital Character Area, of Mississauga Official Plan, is hereby amended by adding Sections 12.5.1 and 12.5.2 as follows:

### **12.5.1 Introduction**

The focal point of Downtown Hospital continues to be the Mississauga Hospital, serving as one of Canada's leading health centres with specialized regional programs. Downtown Hospital consists primarily of residential high density uses. Small pockets of low rise dwellings are located on the edges of the Character Area. Office uses are located in the area surrounding the hospital along Hurontario Street and Queensway. Mixed commercial uses are clustered at three main intersections along Hurontario Street at King Street, Queensway and North Service Road.

Cooksville Creek is a significant ***natural heritage feature*** and portions of the creek have been impacted by channelization associated with flood and erosion control and storm water management. Enhancements to the ecological features of existing natural habitats and the urban forest will be encouraged.

The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.

### **12.5.2 Vision**

Downtown Hospital will be developed as a health district, anchored by the hospital with a broad range of uses clustered around the Mississauga Hospital, supporting more retail and service commercial uses, research and development, health facilities, offices, parks and residential uses.

Transit-supportive development along Hurontario Street and Queensway with maximum heights of 25 storeys for new residential development will ensure greater access to housing, jobs and services.

The vision is based on the following guiding principles:

1. Support health care services - promote a range of health care services, supportive uses and research and development that create a healthier community; and
  2. Create a more walkable and transit-supportive community - provide pedestrian connections to Hurontario Street and Queensway, and street related retail and service commercial uses along these corridors.
13. Section 12.5.2 (existing), Environmental Planning Area, Downtown Hospital Character Area, of Mississauga Official Plan, is hereby amended by deleting 'a' from Policy 12.5.2.2 (existing) and renumbering 'b' to 'd' to 'a' to 'c'.
  14. Section 12.5.2 (existing), Environmental Planning Area, Downtown Hospital Character Area, of Mississauga Official Plan, is hereby amended by deleting "Credit Valley Conservation" from the first, second and fourth bullets of Policy 12.5.2.2.c. (existing) and replacing it with "the conservation authority".
  15. Section 12.5.2 (existing), Environmental Planning Area, Downtown Hospital Character Area, of Mississauga Official Plan, is hereby amended by deleting "zone to provide direction as to future permitted uses while ensuring" from the fourth bullet of Policy 12.5.2.2.c. (existing) and replacing it with "provision to ensure".
  16. Section 12.5.4.1 (existing), Site 1, Downtown Hospital Character Area, of Mississauga Official Plan, is hereby amended by deleting "provisions of the Residential Low Density I, Residential Medium Density and Greenlands designations" from the first paragraph of Policy 12.5.4.1.2 (existing) and replacing it with "policies of this Plan".

17. Section 12.5.4.1 (existing), Site 1, Downtown Hospital Character Area, of Mississauga Official Plan, is hereby amended by deleting "the appropriate approvals from Credit Valley Conservation" from Policy 12.5.4.1.2.a. (existing) and replacing it with "approvals from the appropriate conservation authority".
18. Section 12.5.4.1 (existing), Site 1, Downtown Hospital Character Area, of Mississauga Official Plan, is hereby amended by deleting the third bullet of Policy 12.5.4.1.2.d. (existing).

## **IMPLEMENTATION**

Upon the approval of this Amendment by the Council of the Corporation of the City of Mississauga, Mississauga Official Plan will be amended in accordance with this Amendment.

This Amendment has been prepared based on the Office Consolidation of Mississauga Official Plan dated October 21, 2021.

## **INTERPRETATION**

The provisions of Mississauga Official Plan, as amended from time to time regarding the interpretation of that Plan, will apply in regard to this Amendment.

This Amendment supplements the intent and policies of Mississauga Official Plan.

[http://teamsites.mississauga.ca/sites/18/MOPA/CD.03-DOW.MOPA 145.Non-MTSA.kp.fs.cb.docx](http://teamsites.mississauga.ca/sites/18/MOPA/CD.03-DOW.MOPA%20145.Non-MTSA.kp.fs.cb.docx)

**APPENDIX I**  
**PUBLIC MEETING**

All property owners and residents within the City of Mississauga were invited to attend Public Meetings of the Planning and Development Committee held on May 9, 2022 and July 5, 2022 in connection with this proposed Amendment. Several comments were made regarding Downtown Cooksville Character Area boundaries, land uses, maximum building heights, urban design, transportation affordable housing and the review of development applications in process. These concerns have been addressed at the meeting and in the Planning and Building Department report dated June 10, 2022 attached to this Amendment as Appendix II.





City of Mississauga  
**Corporate Report**



<p>Date: June 10, 2022</p> <p>To: Chair and Members of Planning and Development Committee</p> <p>From: Andrew Whittemore, M.U.R.P., Commissioner of Planning &amp; Building</p>	<p>Originator's files: CD.03-DOW</p>
	<p>Meeting date: July 5, 2022</p>

## Subject

### **PUBLIC MEETING RECOMMENDATION REPORT (WARDS 4 and 7)**

### **Planning 15-Minute Cities: Downtown Fairview, Cooksville and Hospital Policy Review – Official Plan Amendments and Built Form Standards**

## Recommendation

1. That the amendments to Mississauga Official Plan proposed in Appendices 5 and 6 in the report titled "Planning 15-Minute Cities: Downtown Fairview, Cooksville and Hospital Policy Review – Official Plan Amendments and Built Form Standards" dated June 10, 2022, from the Commissioner of Planning and Building, be approved.
2. That the Official Plan Amendment By-law to implement Protected Major Transit Station Area policies as proposed in Appendices 5 and 6 in the report titled "Planning 15-Minute Cities: Downtown Fairview, Cooksville and Hospital Policy Review – Official Plan Amendment and Built Form Standards" dated June 10, 2022, from the Commissioner of Planning and Building, be prepared and submitted to the Region of Peel for approval.
3. That the Downtown Fairview, Cooksville and Hospital Built Form Standards proposed in Appendix 8 in the report titled "Planning 15-Minute Cities: Downtown Fairview, Cooksville and Hospital Policy Review – Official Plan Amendments and Built Form Standards" dated June 10, 2022, from the Commissioner of Planning and Building, be endorsed.
4. That staff be directed to explore strategies, including Community Improvement Plans and other incentives that support the retention and expansion of local businesses and employment uses in Downtown Fairview, Cooksville and Hospital Character Areas.

## Executive Summary

- The City is proposing two Official Plan Amendments (OPA) and built form standards (BFS) to manage growth and development in Downtown Fairview, Cooksville and Hospital. The proposed OPAs and BFS aim to achieve vibrant, 15 minute cities with a high quality built form and a mix of housing, jobs, services and amenities.
- Public engagement on the draft OPAs and BFS began earlier this year. A public meeting was held on May 9, 2022, at which time an Information Report on the City's proposal and all submissions were received for information. Comments were received from residents, landowners, representatives of the development industry, agencies and other community groups.
- Based on the comments that have been received, several modifications have been made to the draft OPAs and BFS presented to the Planning and Development Committee (PDC) on May 9, 2022.
- The purpose of this report is to report back on comments received through public engagement and present staff's final recommendations on the proposed OPAs and BFS.
- Staff is also seeking approval of the two proposed OPAs, endorsement of the BFS and to begin preparing the implementing OPA By-laws. It is also recommended that strategies be explored to retain local businesses and employment in Downtown Fairview, Cooksville and Hospital.

## Background

The City is proposing two Official Plan Amendments (OPA) and built form standards (BFS) to manage growth and development, and achieve a vibrant, 15-minute city in Downtown Fairview, Cooksville and Hospital. On January 24, 2022, staff received approval from the Planning and Development Committee (PDC) to begin community engagement on the draft OPAs and BFS<sup>1</sup>. On May 9, 2022, a public meeting was held by the PDC, at which time a report on the City's proposal and all submissions were received for information<sup>2</sup>. Staff presented updated draft OPAs and BFS, and summarized the public engagement program and comments received.

## Comments

The purpose of this report is to report back on the comments received and present staff's final recommendations on the two proposed Downtown Fairview, Cooksville and Hospital OPAs and BFS. A number of comments were provided by residents, landowners, representatives of the development industry, agencies and other community groups through the policy review process. Appendix 1 contains copies of the written submissions and Appendix 2 provides staff's

<sup>1</sup> Link to January 24, 2022 PDC Information Report – <https://pub-mississauga.escribemeetings.com/FileStream.ashx?DocumentId=18981>

<sup>2</sup> Link to May 9, 2022 PDC Public Meeting Information Report – <https://pub-mississauga.escribemeetings.com/FileStream.ashx?DocumentId=25328>

responses to written and verbal submissions. In March, letters inviting Indigenous communities<sup>3</sup> to participate in the policy review were sent. No responses have yet been received from Indigenous communities. At the public meeting held on May 9, 2022, five deputations were made. The minutes of the public meeting are contained in Appendix 3.

### **SUMMARY OF COMMENTS RECEIVED ON THE DRAFT POLICIES AND BFS**

Comments generally reflected the following themes:

1. Boundary expansion – the boundary of the Downtown Cooksville Character Area should be expanded to include 189 Dundas Street West.
2. Built form standards – the standards are restrictive, do not facilitate the best use of land, and should exempt certain sites. The BFS should allow for more flexibility in design. Other comments were received that buildings should be designed with diverse architecture and the use of quality materials.
3. Building heights – maximum heights should be increased from 25 storeys to 29 and 35 to 40 storeys in key locations, including for properties near the Cooksville GO Station and along Hurontario Street, Dundas Street and Agnes Street. Other comments were received requesting a reduction in maximum heights, including a request to reduce the maximum height from 25 storeys to 3 storeys for the commercial plaza located on the southeast corner of Hurontario Street and Fairview Road East.
4. Community hub – the existing community is lacking community space to access service and supports. A new community facility including a community centre/hub or a sports facility in Downtown Cooksville is needed to serve a growing population.
5. Environment – Cooksville Creek should be recognized as a significant natural heritage feature in the vision for all three communities. Development applications within floodplain areas should assess the impacts and identify flood measures and mitigation. More generally, sustainable practices should be incentivized.
6. Housing – concern that Cooksville will become less affordable to own or rent units. Comments emphasized the importance of Inclusionary Zoning as a tool to provide sufficient rental and affordable housing units through development. The need for shelters to address homelessness was also identified.
7. Land use designation – general support for redesignation of several properties to facilitate transit-supportive development, such as the proposed change in land use designation for

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<sup>3</sup> Indigenous communities sent an invitation letter: Haudenosaunee Development Institute, Huron-Wendat First Nation, Mississaugas of the Credit First Nation, and Six Nations of the Grand River

the lands near Shepard Avenue and King Street from Residential Medium Density to Residential High Density.

8. Local businesses and employment – request by local residents and community organizations to support existing local businesses in Downtown Cooksville. Comments expressed support for proposed policies that aim to expand local employment by retaining and expanding non-residential uses.
9. The Mississauga Hospital – request that the vision and policies place more emphasis on creating a healthier community, including health care services, and that land use permissions be expanded to support the redevelopment of the hospital.
10. Mix of uses – several property owners requested exemption from policies that require a) replacement of existing office and retail and service commercial floor space through redevelopment, b) three floors of retail and service commercial floor space at key locations and c) retail and service commercial uses at-grade. Other comments identified the need for more office space in Downtown Cooksville. Policies should allow for a broader range of uses on the ground floor of mixed use buildings, including community amenities and facilities.
11. Parking – minimum parking standards should be removed or lowered to help reduce housing prices and promote transit-oriented communities. Policies should allow for vertical parking that is publicly-accessible.
12. Parks and greenspace – concern that parkland provisions will impact the ability to achieve minimum density requirements and that parkland provision should be determined through the development application review process.
13. Transportation – concern that new roads and pedestrian connections may negatively impact development potential. Concern was also expressed for worsening traffic and gridlock that should be considered by the policy review. Others valued the walkability of Cooksville today and liked the addition of more walking trails, pedestrian pathways and new roads to improve connectivity. Comments were received that policies should support expansion of cycling infrastructure.
14. Schools – schools should be expanded to serve a growing population.
15. Urban form – policies on podium heights, angular plane transition and building height variations where more than one tower is proposed should be less restrictive. Another comment was made that the view south along Hurontario Street from Downtown Fairview should not be obstructed.
16. Vision – support for more walkable, mixed use development in Downtown Fairview and animated streets with storefronts and active uses in Downtown Cooksville. The vision for

Downtown Cooksville should also emphasize the importance of existing businesses, community amenities and housing affordability.

### **PLANNING FOR MAXIMUM BUILDING HEIGHTS**

A number of comments were received on maximum building heights as identified in the section above. Staff is proposing minimum and maximum building heights through the City-Wide Major Transit Station Area (MTSA) OPA to be identified in new schedules 11-K and 11-L. Appendix 4 contains the proposed City-Wide MTSA OPA building height schedules for Downtown Fairview, Cooksville and Hospital.

Within Downtown Fairview, Cooksville and Hospital, proposed building heights vary based on the new vision for these areas. In Downtown Fairview, building heights will transition down from Burnhamthorpe Road to emphasize the prominence of the Downtown Core within the UGC. In Downtown Cooksville, the proposed heights align with the desire to have a vibrant mainstreet focus and human-scaled built form around the Hurontario and Dundas Street intersection, and more generally along Dundas Street. The greatest heights in Downtown Cooksville will be located near the Cooksville GO Station. In Downtown Hospital, transit-supportive development along Hurontario Street and the Queensway with maximum heights of 25 storeys for new residential development will ensure greater access to housing, jobs and services.

Staff also note the following points which informed the recommended building height policies.

- The proposed building heights can accommodate the City and Region's planned density target of 300 people and jobs per hectare (ppj) to 2051 and beyond for these areas. Additional density beyond the 300 ppj target is not needed to accommodate the city's overall growth allocation<sup>4</sup>.
- The existing Official Plan height structure allows for development and growth to support light rail transit, bus rapid transit and GO service improvements, and major changes are not required to achieve the density target.
- The policies are intended to create more development certainty around heights and aid prospective land purchasers with their investment decisions.
- Staff has considered comments from landowners and made several adjustments where they are consistent with the vision for these areas – e.g. increasing maximum heights from 25 storeys to 30 storeys around the Cooksville GO Station site.

<sup>4</sup> Link to April 19, 2022 PDC Information Report Mississauga Official Plan Review – Technical Memo: Mississauga's City Structure and Residential Growth Accommodation – <https://pub-mississauga.escibemeetings.com/FileStream.ashx?DocumentId=23040>

- The Downtown Core allows for unlimited heights and densities and is planned to receive the greatest amount of growth within Mississauga. There are many development sites available in the Downtown Core that can accommodate tall buildings above 30 storeys. Locating these buildings in other parts of the Urban Growth Centre (UGC) would impact land values, infrastructure planning and the Official Plan vision for unique communities. Downtown Fairview, Cooksville and Hospital, while also part of the UGC, are not planned to accommodate the same level of growth.
- The proposed building heights align with existing or proposed land use designations with lower heights for Residential Medium Density, Residential Low Density I and II designations and greater heights for Residential High Density, Office and Mixed Use designations.
- In several locations, proposed building heights recognize approved or existing development.

### **CHANGES TO OFFICIAL PLAN POLICIES IN RESPONSE TO COMMENTS**

On May 9<sup>th</sup> at the PDC, staff presented changes to the Downtown Fairview, Cooksville and Hospital official plan policies and BFS. Following that meeting, staff received further comments on the draft policies and BFS. Based on a review and consideration of the comments, staff propose further modifications to the policies and BFS. Appendices 5 and 6 of this report identify each modification to the draft official plan policies and schedules, respectively. A summary of key changes to the draft policies and schedules since the May 9<sup>th</sup> PDC meeting are provided below.

1. Add policy text identifying when proposed protected MTSA policies in Chapter 12: Downtown will come into force and effect.
2. Remove the proposed policy directing staff to work in collaboration with school boards to determine the need for educational facilities through the review of development applications. Section 7.3 of Mississauga Official Plan contains similar direction that will continue to apply to development within Downtown Fairview, Cooksville and Hospital.
3. Revise the figure showing the future pedestrian connections and public parkland network at Shepard Avenue and Paisley Boulevard East by identifying a public park symbol on both sides of Shepard Avenue, north of Paisley Boulevard East.
4. Revise the proposed angular plane height transition policy to have it apply where a development is adjacent to a residential low or medium density land use designation. The new policy wording recognizes the planned context.
5. A change to the land use designation on the east side of Cook Street, north of Agnes Street from Residential High Density to Residential Medium Density. This change recognizes existing townhouses on the site.

6. Remove policy text that is referred to in another section of the OPAs, or in the City-Wide MTSA OPA. For example, text introducing A, B and C streets has been deleted where it is referenced elsewhere in the OPAs.
7. New policies allowing for additional building heights by no more than 3 storeys to incentivize non-residential uses through development in Downtown Fairview, Cooksville and Hospital. The additional height would be subject to meeting building transition policies. Downtown Cooksville, Special Site 1 is excluded from the policy in recognition of the vision for the area and existing non-residential uses within the site.
8. Recommend that the City-Wide MTSA OPA amend Schedule 11-L: Building Heights Schedule to increase maximum building heights at 2 locations – a) from 12 to 25 storeys for the Bronte College site, and b) from 16 to 25 storeys for the Kaneff property located at the northwest corner of Confederation Parkway and Queensway West.
9. New policy to explore strategies, including Community Improvement Plans and other incentives that support the retention and expansion of local employment uses.

Appendix 7 provides detailed planning rationale for the proposed amendments to Downtown Fairview, Cooksville and Hospital Character Areas and Schedules of Mississauga Official Plan.

## **CHANGES TO THE BUILT FORM STANDARDS**

The built form standards have been revised to clarify that balconies should be inset and should not project from the podium or tower portion of the podium. Appendix 8 contains the proposed BFS with modifications.

## **ENSURING CONSISTENCY AND CONFORMITY TO PROVINCIAL AND REGIONAL PLANS**

The proposed OPAs have regard for matters of Provincial interest set out in the *Planning Act*, is consistent with the *Provincial Policy Statement*, and conform to the *Growth Plan for the Greater Golden Horseshoe*, the in force Region of Peel Official Plan and the adopted, but not yet approved Region of Peel Official Plan passed by Region of Peel By-law 20-2022. Appendix 9 provides an overview of consistency and conformity with applicable Provincial and Regional plans and policies.

## **IMPLEMENTING THE PROPOSED OFFICIAL PLAN AMENDMENTS**

Following approval of the recommendations in this report by the PDC, Staff will be preparing two OPA By-laws to City Council for adoption: 1) OPA No. 145 will implement policies on vision, guiding principles, community infrastructure, urban design, roads and pedestrian connections; and 2) OPA No. 146 will implement protected MTSA policies on heights and land uses that are specific to Downtown Fairview, Cooksville and Hospital.

The policies of OPA No. 146 are intended to complement the land use, height and density policies that are being introduced through the separate City-Wide MTSA OPA. The City-Wide



MTSA Amendment will come into force and effect when the new Region of Peel Official Plan is approved by the Province. As such, the in-force date of OPA No. 146 is contingent on the City-Wide MTSA OPA and the new Region of Peel Official Plan coming into force and effect.

The Region of Peel has indicated that OPA No. 145 is exempt from Regional approval and that OPA No. 146 must be forwarded to Regional Council for approval.

## Financial Impact

There are no financial impacts resulting from the recommendations in this report.

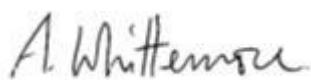
## Conclusion

A number of comments were made by participants on the two proposed OPAs and BFS. Staff has considered the comments received and propose further modifications to the OPAs and BFS. The proposed OPAs support the creation of vibrant, 15 minute cities in Downtown Fairview, Cooksville and Hospital, and facilitates transit-supportive development with a mix of housing, jobs, services and amenities. The proposed BFS will help ensure that development contributes to a high quality built form and a vibrant public realm.

Staff is seeking approval of the two Downtown Fairview, Cooksville and Hospital OPAs, endorsement of the BFS and to begin exploring strategies to retain local businesses and employment uses. The next step is to prepare the OPA By-laws for City Council's adoption and to submit OPA No. 146 to Regional Council for approval.

## Attachments

- Appendix 1: Written Submissions
- Appendix 2: Response to Comments
- Appendix 3: Public Meeting Minutes
- Appendix 4: Proposed Building Heights and Land Uses for Downtown Fairview, Cooksville and Hospital – Draft Protected MTSA Schedules 11-K, 11-L and 11-M
- Appendix 5: Recommended Official Plan Amendments – Revised Character Area Policies
- Appendix 6: Recommended Official Plan Amendments – Revised Official Plan Schedules
- Appendix 7: Planning Rationale for Proposed Amendments to Mississauga Official Plan
- Appendix 8: Downtown Fairview, Cooksville and Hospital Built Form Standards
- Appendix 9: Applicable Policy Analysis



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Andrew Whitemore, M.U.R.P., Commissioner of Planning & Building  
Prepared by: Christian Binette, Planner

**Appendix 1**

**Written Submissions**



January 21, 2022

GSAI File: 1348-001

*(Via Email to: [megan.piercey@mississauga.ca](mailto:megan.piercey@mississauga.ca))*

Chairman and Members of Planning & Development Committee  
c/o Megan Piercey, Legislative Coordinator  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

RE: **Downtown Fairview, Cooksville and Hospital Policy Review**  
**Augend Investments Limited**  
**189 Dundas Street West, City of Mississauga**

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Glen Schnarr & Associates Inc. ('GSAI') are the planning consultants to Augend Investments Limited (the 'Owner') of the lands municipally known as 189 Dundas Street West, in Cooksville. The City is currently processing a rezoning/OPA under file OZ 21/009 W7 for a large scale mixed use development of these lands. For some time we have been monitoring the various studies that will effect intensification and mixed use redevelopment in Cooksville, in light of existing and future transit infrastructure which will service this area via the Cooksville GO, Hurontario LRT and proposed Dundas BRT. While our client's lands are just outside of the Downtown Cooksville boundary, we were encouraged to see that the Dundas Connects Master Plan had identified these lands as appropriate for possible future inclusion within the Downtown Cooksville area as part of that study.

The intersection of Dundas and Hurontario Street has historically been the "epicentre" of Cooksville. In earlier iterations of Cooksville District policy documents dating back to the 1980s and up until present, the Cooksville Node boundary (now the Downtown Cooksville boundary) has remained unchanged. Historically this boundary has been confined within the CNR Tracks to the north, King Street to the south, Kirwin Avenue/Camila Road to the east and Confederation Parkway to the west. We believe with current and pending transit infrastructure improvements and the recent surge of investment in the area, that the boundaries of the Downtown Cooksville area should be reviewed as part of this ongoing study and are disappointed to see the draft OPA maintaining the boundaries dating back to the 1980s that pre-date the current provincial policy regime and numerous City initiated land use studies seeking to promote transit supportive redevelopment in Cooksville. GSAI have made earlier written submissions to staff on both this exercise and Dundas Connects that similarly reflect this position.

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[www.gsai.ca](http://www.gsai.ca)



The staff report speaks to the concept of creating a "15 Minute City" in Downtown Cooksville and GSAI participated in the recent webinar on this very topic co-ordinated by Ward 7 Councillor Dipika Damerla which was very well received. Drawing on the concept of the historical "four corners" of the Dundas/Hurontario Intersection being the epicentre of this community, we support the concept of a 15 Minute City centred on this area. Based on provincial planning objectives, the attached graphic visually depicts an 800 m radius around this intersection where mixed use redevelopment would benefit from an ideal trifecta of GO, LRT and future BRT transit. This will improve walkability and reduce auto dependency within this area such that it functions as a future mixed use node where increased density will help a more street related and pedestrian friendly forms of commercial development grow and thrive. We believe this reflects the vision of re-creating Cooksville as a 15 Minute City as outlined on page 4 of the staff report.

Our client's lands represent a larger scale redevelopment opportunity within 800 m of all three transit forms and have the locational attributes that, in our opinion, contribute to the creation of a 15 Minute City. We believe the inclusion of these lands, as well as potentially other sites outside the Node boundaries, warrant consideration for inclusion within the Downtown Cooksville area. While the staff report is an Information Report only, we see this as an appropriate opportunity to present our views to Council while our proposal is still in its early stages and the final Public Meeting on the forthcoming OPA has yet to take place. We trust Council and staff will take our submission into consideration and appreciate the opportunity to participate in this important initiative.

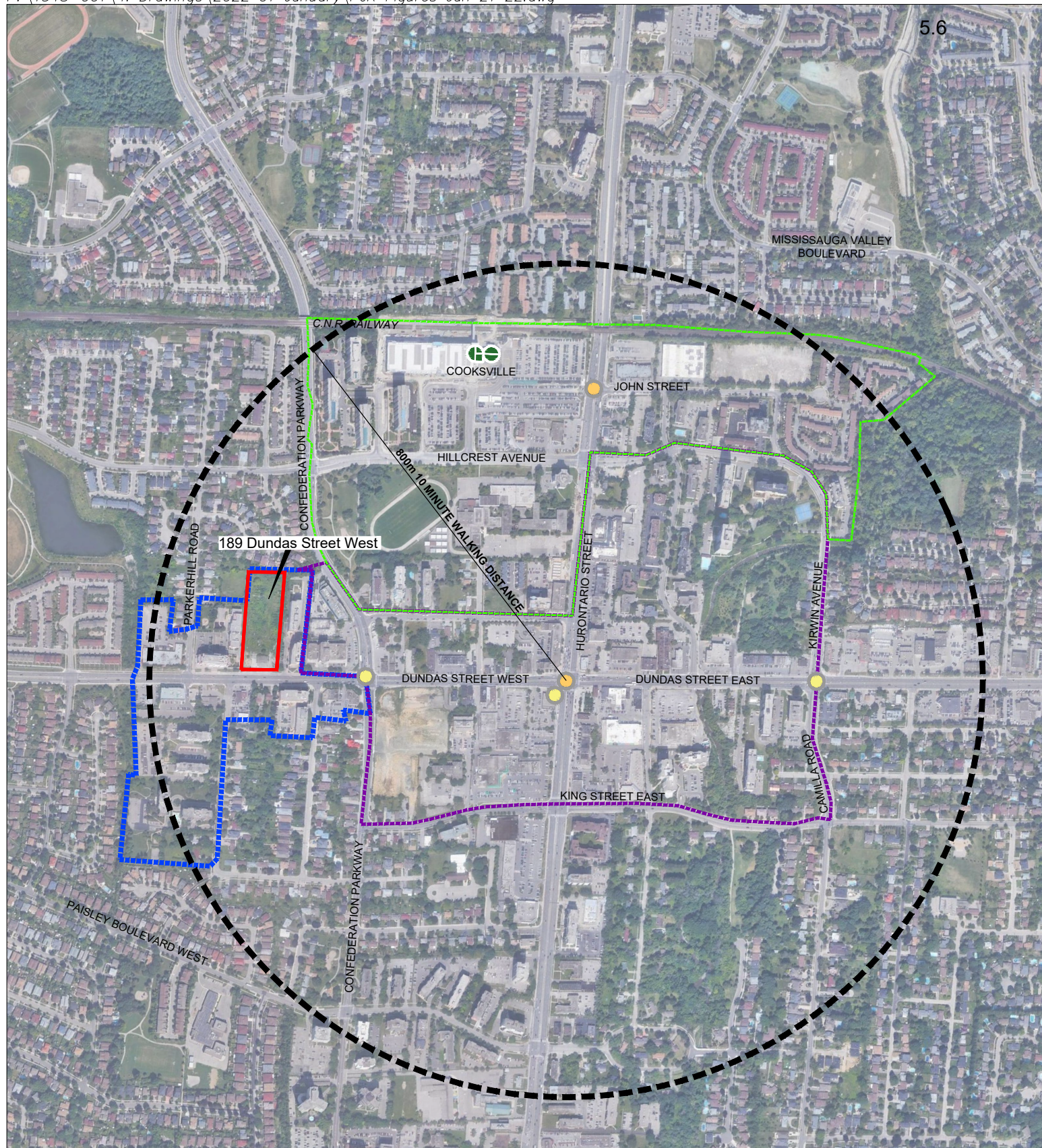
Yours very truly,

**GLEN SCHNARR & ASSOCIATES INC.**

Jim Levac, MCIP, RPP  
Partner

Copy: client





## FIGURE 1 SITE CONTEXT PLAN

189 Dundas Street West,  
City of Mississauga, Regional Municipality of Peel

### LEGEND

- Subject Property (189 Dundas Street West)
- Cooksville  
GO Station
- Proposed BRT Station
- Proposed LRT Station
- Confederation Parkway MTSA
- Dundas MTSA
- Cooksville GO MTSA



SCALE NTS  
JANUARY 21, 2022





January 19<sup>th</sup>, 2022

Sent via Email

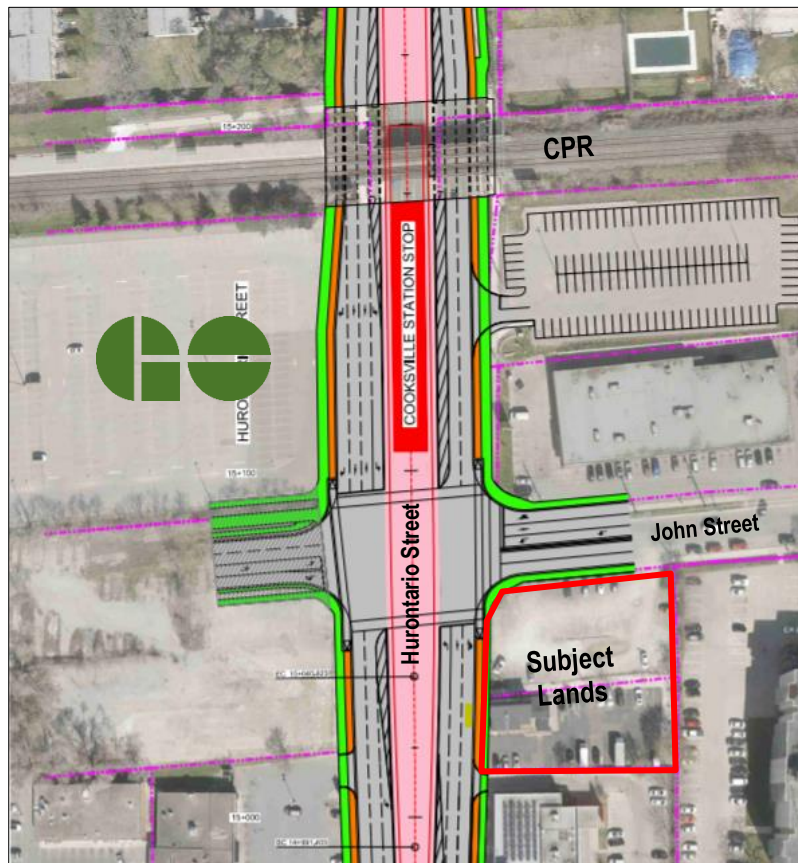
Mississauga City Council Attn: Development Assistant  
[application.info@mississauga.ca](mailto:application.info@mississauga.ca)

**Attention: Mississauga City Council Attn: Development Assistant**  
**c/o Planning and Building Department – 6th Floor**  
**300 City Centre Drive, Mississauga, ON, L5B 3C1**

Dear Mayor Crombie and Members of Council:

**Re: Comments on Downtown Fairview, Cooksville and Hospital Policy Review**  
**Addresses: 3161 – 3173 Hurontario St., Mississauga**  
**Planning & Development Committee Meeting – Jan. 24, 2022 – CD.03-DOW**

I am writing to express concern in respect of the proposed *Downtown Fairview, Cooksville and Hospital Policy Review – Draft Official Plan Amendment and Built Form Standards* and their impact in relation to a potential development at the southeast corner of John Street and Hurontario Street (Soffron Tax). For context, the subject lands front onto the under construction Cooksville LRT Station and reside directly across Hurontario from the Cooksville GO Station, shown below:



To learn more about Trolleybus, visit our website at [trolleybusdevelopment.com](http://trolleybusdevelopment.com)

### **Background**

On May 29, 2019 Trolleybus Urban Development Inc. attended a Development Application Review Committee (DARC) Meeting to discuss a proposed 37 storey residential building with ground floor commercial on the subject lands. Since that time, our firm is continuing to advance the details of a development proposal on the site generally consistent with the built form and statistics presented at the DARC meeting in 2019.

While a formal planning application to the City of Mississauga is expected in late 2022, we have had a chance to review the Draft Official Plan and Built Form Standards in relation to our site. A summary of our primary concerns are below:

### **Major Transit Station Area within an Urban Growth Centre**

It is our opinion that the recommended building heights do not reflect the elevated hierarchy of available transit options within the Cooksville Major Transit Station Area (MTSA). The proposed 25 storey height limit has been carried forward from an outdated planning regime and does not promote minimum transit supportive densities within this elevated MTSA, in line with Growth Plan direction around promoting intensification.

This site is in a unique proximity across from the Cooksville GO Station, Cooksville LRT Station and within a short walk of the proposed Dundas/Hurontario BRT Station. Accordingly, this specific site is located within one of the most transit rich intersections in the entire GTA. Therefore, we believe the proposed 25 storey height limit for this site (and intersection) should be reconsidered to illustrate the tallest built forms (height peak) within the Cooksville MTSA.

### **Existing Precedents**

The block of addresses form a high rise development site that can adhere to the proposed built form standards including tower setbacks and transition to low-rise neighbourhoods. There are numerous sites along Hurontario and within Downtown Fairview that exceed the 25 storey height limit approved through Council or the Ontario Land Tribunal (OLT) and its predecessors. The result of carrying forward this outdated height limit in the Official Plan will create undue burden for staff and Council to defend future applications consistent with past approvals and Provincial Growth Policies at the Ontario Land Tribunal with a low likelihood of success.

### **Recent Proposal at 3085 Hurontario Street**

On January 10<sup>th</sup>, 2022 I attended a Virtual Public Meeting for a similar high-rise residential proposal located at 3085 Hurontario Street (a site in further proximity to higher order transit). At that meeting, there was general support from Council through their feedback that the proposed 35 storey height was generally consistent with intensification goals to be expected in the Cooksville node. It is also our opinion, one which was raised at the meeting, was that it is unlikely the existing (and proposed) 25 storey height limit would be defensible through an OLT appeal process.

### **Request**

Our suggestion is that the properties north of Hillcrest Ave., south of the CP Railway with frontage on Hurontario be reconsidered as the height peak of the Cooksville MTSA with building height limits between 35-40 storeys. This revision would adequately reflect the best practices of other higher order MTSA's in the GTA and acknowledge the existing precedents in the Cooksville node and Hurontario LRT corridor.





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Please provide us with notice of all matters concerning the OPA updates in accordance with subsections 34(18) and 17(23) of the Planning Act. We look forward to staff and Council considering our concerns and speaking further on this matter.

Yours truly,

A handwritten signature in black ink, appearing to read "Greg Gilbert", is written over a light blue horizontal line.

**Greg Gilbert**

Director of Planning & Design ([greg@trolleybusdevelopment.com](mailto:greg@trolleybusdevelopment.com))

**TROLLEYBUS URBAN DEVELOPMENT INC.**

4950 Yonge Street, Suite 900

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# GLEN SCHNARR & ASSOCIATES INC.

URBAN & REGIONAL PLANNERS, LAND DEVELOPMENT CONSULTANTS

PARTNERS: 5.6  
GLEN SCHNARR, MCIP, RPP  
GLEN BROLL, MCIP, RPP  
COLIN CHUNG, MCIP, RPP  
JIM LEVAC, MCIP, RPP

January 21, 2022

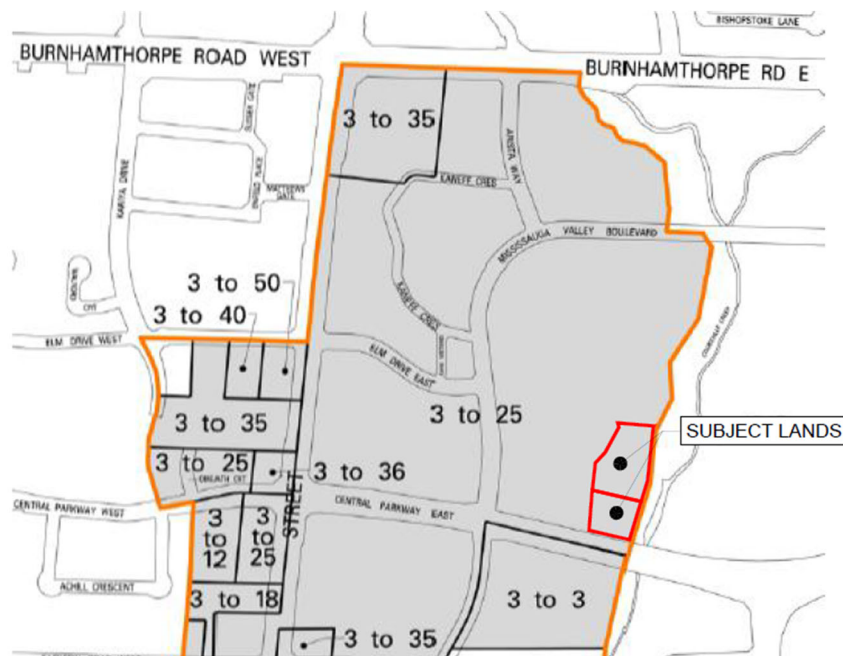
GSAI File: 1348-001

(Via Email to: [megan.piercey@mississauga.ca](mailto:megan.piercey@mississauga.ca))

Chairman and Members of Planning & Development Committee  
c/o Megan Piercey, Legislative Coordinator  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

**RE: January 24, 2022 Mississauga PDC Report: Agenda Item 5.3  
Downtown Fairview, Cooksville and Hospital Policy Review  
Morguard Corporation  
1477 & 1547 Mississauga Valleys Blvd., City of Mississauga**

Glen Schnarr & Associates Inc. ('GSAI') are the planning consultants to Morguard Corporation, registered owners of the lands municipally known as 1477 & 1547 Mississauga Valleys Blvd. Collectively, these two sites occupy an area of 5.46 hectares (13.5 acres), much of which is occupied by existing rental apartments. There remains, however, a smaller undeveloped 1.3 hectare (3.2 acre) parcel of vacant lands having frontage onto Central Parkway East, just west of the Cooksville Creek. See location plan below.



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**GLEN SCHNARR & ASSOCIATES INC.**  
URBAN & REGIONAL PLANNERS, LAND DEVELOPMENT CONSULTANTS

The subject lands are within the Downtown Fairview area and are depicted on the proposed draft OPA height schedule as having a maximum height of 25 storeys. On behalf of our client, we wish to highlight the following issues regarding the proposed Draft Official Plan Amendment and Built Form Standards appended to the January 24, 2022 staff information report:

- Map 12.3, which outlines the various FSI density ranges for existing developed sites and recognizes FSIs on more recent site-specific rezoning and OPA applications, is proposed to be revised to delete FSI references under the proposed red line revisions. Will these be replaced with updated FSI density ranges as many of the older developments were developed and built in a different era which predates the establishment of Growth Centres, Intensification Corridors and Major Transit Station Areas associated with the funded Hurontario LRT line?;
- The proposed Built Form Standards are very prescriptive on matters related to setbacks, podium setbacks and floorplate sizes. As there are a multitude of various property sizes and shapes from small corner sites to “tower in the park” sites, we believe greater flexibility should be provided to the designers of future buildings to avoid replicating older slab massed buildings from an earlier era. While these are intended to merely be guidelines that do not form part of the policy text of the OPA, they are often rigidly interpreted by City staff during the review of Development Applications.

Thank you for the opportunity to participate in this exercise and we look forward to working with staff as the proposed OPA for the Downtown Fairview area is further developed prior to the final Public Meeting

Yours very truly,

**GLEN SCHNARR & ASSOCIATES INC.**

Jim Levac, MCIP, RPP  
Partner

Copy: Christine Cote/Mark Bradley/Brian Athey, Morguard Corporation

Calvin Lantz  
Direct: (416) 869-5669  
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January 21, 2022  
File No.: 146656.1001

**By E-mail**  
*megan.piercey@mississauga.ca*

Planning & Development Committee  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

Attention: Ms. Megan Piercey, Legislative  
Coordinator

Dear Sirs/Mesdames,

**Re: CD.03-DOW  
Information Report: Downtown Fairview, Cooksville and Hospital Policy  
Review – Draft Official Plan Amendment and Built Form Standards  
Letter of Concern**

We are counsel to BET Realty Limited and 3420 Hurontario Street Incorporated, the owners of the lands municipally known as 3420 & 3442 Hurontario Street, Mississauga (the “**Property**”). The Property is located at the southwest corner of Hurontario Street and Central Parkway West, within the Downtown Fairview Character Area, as outlined in the Draft Official Plan Amendment (the “**Draft OPA**”), which is the subject of this agenda item.

By way of background, our client submitted applications for Official Plan and Zoning By-law Amendments (File No. OZ 20/022) to redevelop the Property with a two-tower mixed-use development, comprised of a 36-storey building at the north end and a 30-storey building at the south end (the “**Proposed Development**”). Our client’s applications are presently proceeding before the Ontario Land Tribunal under Case No. OLT-21-001693.

On behalf of our client, we are writing to comment on the Draft OPA and the proposed Built Form Standards (“**BFS**”).

In general, our client is supportive of the City’s initiative to introduce policies that support greater height and density along the Hurontario corridor. We believe that this is a positive and necessary step in planning for future growth in the City of Mississauga, particularly in view of the strong Provincial direction for transit-oriented development.

That being said, we have reviewed the Draft OPA and proposed BFS and are concerned that several of the policies and guidelines will prevent certain lands, including the Property, from being developed in an optimal manner in line with Provincial policy direction.

In particular, the Draft OPA contemplates a height limit of 25 storeys for the Property, which represents an underutilization of the site given the planned and emerging context. This 25-storey height limit appears to be without apparent planning basis, especially when compared to other lands to the north, south, and east, which are given permissions for significantly greater heights, ranging from 35 to 50 storeys.

In addition, the Draft OPA appears to propose the deletion of the planned FSI ranges currently existing in Map 12-3 of the Official Plan. We wish to clarify whether staff's intention is to eliminate FSI ranges completely as a form of planning control for these areas, or whether the intention is to replace the existing FSI standards with FSI figures that better reflect the density planned for the Hurontario corridor.

As well, we are concerned that the prescriptive restrictions introduced through the Draft OPA and proposed BFS will frustrate the strong policy direction for mixed-use intensification and efficient development at transit-supportive densities, particularly as envisaged through the Proposed Development for the Property.

It is also concerning that City staff are proposing such a high degree of regulatory prescription—akin to a zoning by-law—through the BFS, which is not a *Planning Act* instrument, and therefore exists outside the statutory planning process and not subject to independent review by the Ontario Land Tribunal.

We welcome the opportunity to discuss these and other concerns with City staff. If you have any questions or require additional information, please contact me or my associate, Jonathan Cheng, at (416) 869-6807 or [jcheng@stikeman.com](mailto:jcheng@stikeman.com).

Please provide us with notice of all upcoming meetings of Council and Committees of Council at which this matter will be considered, and we ask to be provided with notice of the Committee's and Council's decision with respect to this and any related item.

Thank you for your attention to this matter.

Yours truly,



For: Calvin Lantz

CL/jsc/ec

cc. Jim Levac, *Glen Schnarr & Associates Inc.*  
Client



January 24, 2022

GSAI File: 1278-001

*(Via Email)*

Chairman and Members of Planning & Development Committee  
c/o Megan Piercey, Legislative Coordinator  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

RE: **Downtown Fairview, Cooksville and Hospital Policy Review**  
**33HC TAS LP; 33HC Corp.**  
**25, 33 Hillcrest Avenue and 3154 Hurontario Street, City of Mississauga**

Glen Schnarr & Associates Inc. ('GSAI') are the planning consultants to 33HC TAS LP and 33HC Corp. (the 'Owner') of the lands municipally known as 25, 33 Hillcrest Avenue and 3154 Hurontario Street, in Cooksville, in the City of Mississauga (the 'Subject Lands' or 'Site'). On behalf of the Owner, we are submitting this Comment Letter in relation to the ongoing City of Mississauga Downtown Fairview, Cooksville and Hospital Policy Review initiative ('Initiative').

GSAI has been participating in the Initiative, as well as the concurrent City of Mississauga Official Plan Review and the Region of Peel Municipal Comprehensive Review (referred to as 'Peel 2051+') initiatives. We understand that when complete, the City's Downtown Fairview, Cooksville and Hospital Policy Review Initiative will culminate in an Official Plan Amendment ('OPA') that will modify the policy framework permissions for lands within the Downtown Fairview, Cooksville and Hospital communities, including the Subject Lands.

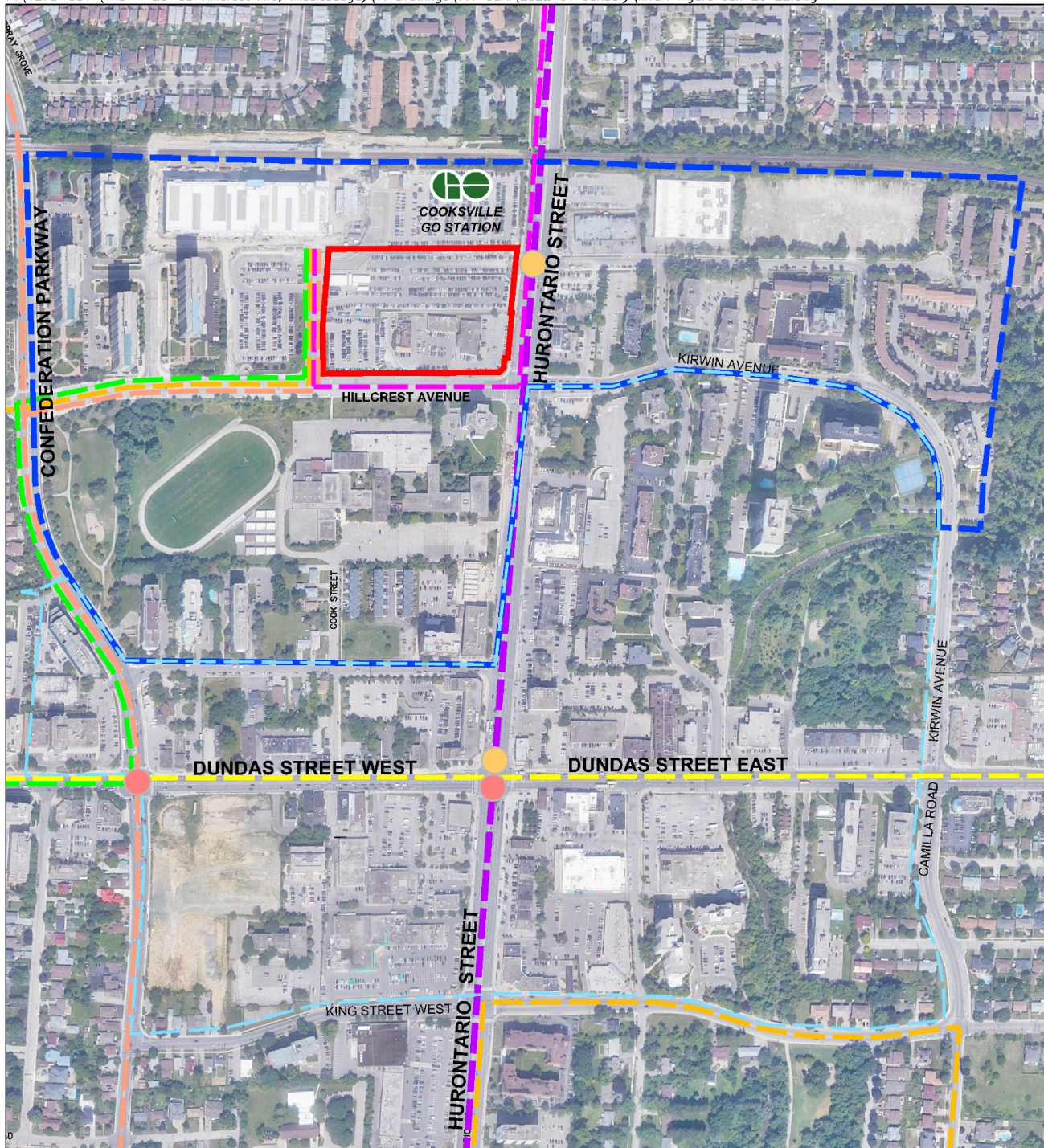
The Subject Lands are located on the west side of Hurontario Street, north of Hillcrest Avenue (see **Context Map** on the next page). Based on the in-effect planning policy framework, the Site is situated within the Downtown Mississauga Urban Growth Centre (in accordance with Schedule 1, A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2020), a Mobility Hub – Gateway (in accordance with the Peel Regional Official Plan), the proposed Cooksville GO Major Transit Station Area (in accordance with the proposed Peel Regional Official Plan, Schedule Y7 – Major Transit Station Areas), the City's Downtown Intensification Area, the Downtown Cooksville Character Area, the Downtown Cooksville Focus Area (in accordance with the Hurontario / Main Street Master Plan, 2011), and the Cooksville Focus Area (in accordance with the Dundas Connects Master Plan).

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P:\1278-001 (TAS - 25-35 Hillcrest Ave, Mississauga)\7. Drawings\7.1 GSAI\2022 01 January\MTSA Figure Jan 20 22.dwg



## CONTEXT MAP 25 & 33 HILLCREST AVENUE & 3154 HURONTARIO STREET

CITY OF MISSISSAUGA, REGIONAL MUNICIPALITY OF PEEL

### LEGEND

- Subject Lands
- Cooksville GO Major Transit Station Area
- Dundas Major Transit Station Area
- MiWay - Route 1 (Dundas)
- MiWay - Route 2 (Hurontario)
- MiWay - Route 4 (Sherway Gardens)
- MiWay - Route 28 (Confederation)
- MiWay - Route 38 (Creditview)

- MiWay - Route 53 (Kennedy)
- HuLRT Station
- Dundas BRT Station



SCALE: N.T.S.  
JANUARY 20, 2022

**GSAI**  
Glen Schnarr & Associates Inc.





When considered collectively, the in-effect policy framework identifies the Subject Lands as an appropriate location for higher density, compact, mixed-use, transit-supportive development to occur. This is strengthened (see **Context Map** on the previous page) by the Site's frontage on Hurontario Street (a recognized Regional Intensification Corridor, a recognized local Corridor and future location of the Hurontario Light Rail Transit ('HuLRT') network), its location immediately adjacent to the Cooksville GO Station, its location within walking distance of future HuLRT Stations, its location within walking distance (422 metres) of the planned Dundas Bus Rapid Transit ('BRT') network, and its location within walking distance of existing street-level transit services operating in the surrounding area. The Site is also within walking distance of various services, amenities and facilities to meet daily needs and support Cooksville as a healthy, vibrant, complete, 15-minute community. We note that the local policy framework, including the Hurontario / Main Street Master Plan, Cooksville Mobility Hub Study and Dundas Connects Master Plan identify the Subject Lands as an appropriate and desirable location for high-density, mixed-use, pedestrian-oriented, 'tall' development to occur.

We have reviewed the draft Mississauga Official Plan Amendment ('draft Amendment'), dated December 23, 2021, and offer the following comments.

In Section 12.4.4 of the draft Amendment, the preferred location of public parks and open space are identified. In accordance with Figure 1 of the Amendment, the Subject Lands are identified as an area where future public parkland and pedestrian connections are desired. In our opinion, the appropriateness of a site for future parkland can and should be determined through the development review process. Additionally, provision of parkland in proximity to the Cooksville GO Station and within a proposed Major Transit Station Area ('MTSA') will impact the ability for any development proposal to achieve minimum density requirements as established in Provincial and Regional policies.

The draft Amendment proposes a policy requirement (Policy 12.4.5) and a building height schedule (Map 12-4.2) for lands within the Downtown Cooksville Character Area. Collectively, the proposed policy and Map 12-4.2 direct that the greatest heights are to be directed to lands near the Cooksville GO Station and along Hurontario Street, north of Agnes. Furthermore, maximum permitted building heights are to be in accordance with Map 12-4.2: Downtown Cooksville Character Area Minimum to Maximum Building Heights. The Subject Lands are identified as an appropriate location for building heights of 3 to 25 storeys. It is our opinion that this building height range should be modified given the Subject Lands are adjacent to the Cooksville GO Station, are in front of and within walking distance of existing and planned transit services, it is situated in an area where intensification is to be directed and there is an inconsistent applicable of building heights. More specifically, we note that lands to the north of Downtown Cooksville have permitted heights of up to 50 storeys in instances, despite these lands within the Downtown Fairview Character Area having less transit connectivity than the Subject Lands. Additionally, there is an inconsistency in the application of permitted building heights as building heights of up to 29 storeys are permitted on



lands to the east and south within Downtown Cooksville.

In Section 12.4.6.1 of the draft Amendment, refined urban form policies are proposed. Of concern is Policy 12.4.6.1.2 which states that larger developments where more than one tower is proposed, the heights of buildings are to be varied by three to five storeys. We request that this policy be modified to facilitate the ability of a development proponent to identify whether height variation between multiple structures on a lot is desirable and to determine on a site-by-site basis what an appropriate height variation between structures would be. The proposed policy as contemplated is prescriptive and can limit the development potential of lands, while also hindering the ability for development to provide for compact, higher density, pedestrian-oriented, transit-supportive forms.

The draft Amendment proposes refinements to the local road pattern. These refinements are presented on Map 12-4.3: Downtown Cooksville Street Types and Figure 9: Street Types in the Downtown Cooksville Character Area. Overall, the draft Amendment directs that Hillcrest Avenue is to be identified as an 'A' Street which is to have a continuous streetwall condition along 90% of the Site's frontage. This preferred streetwall condition does not specify deviations to allow for pedestrian connections or parkland. Clarification is requested.

The draft Amendment also identifies John Street to be extended as a public road and terminate west of the Subject Lands. Furthermore, this extension of John Street is identified as a 'C' Street, or a tertiary public road. The draft Amendment also directs that the current 'GO Access Road' along the Site's western property line is to be reconstructed as a new public road, identified as a 'B' Street, and as a northward extension of Cook Street.

We note that 'C' Streets are intended to provide for mid-block connections and to facilitate 'vehicular access to on-site service, loading, parking and garbage storage areas', while 'B' Streets are proposed to be residential in nature and development is to provide for 'generous setbacks from the street' (Policy 12.4.6.2.2.1.b).

As currently contemplated, the proposed identification of John Street as a 'C' Street and GO Access Road as a 'B' Street will impact the development potential of the Subject Lands. It will also impact site design considerations given vehicular accesses are to be encouraged off of 'C' Streets. Similarly, the provision of these new public roads may require additional road widening and would influence massing, scale and site design for the Site.

Furthermore, Figure 9 of the draft Amendment directs that new pedestrian connections are to be provided across the Subject Lands and other lands in order to provide safe, comfortable and convenient access across the Downtown Cooksville community. Provision of a pedestrian connection in the desired location on the Subject Lands would bisect the Site and adversely impact the development potential of the Subject Lands. Based on the above, we request that modifications be made to preserve the development potential of the Subject Lands and the ability for





development to provide an optimal site design that incorporates a mixing of residential and non-residential uses on the lot, a network of pedestrian pathways, landscaped open spaces and safe, convenient access into and beyond the Site.

The draft Amendment presents refined land use policies, in particular policies pertaining to lands designated as 'Mixed Use'. The Subject Lands are currently split designated 'Residential High Density' and 'Mixed Use' by the Mississauga Official Plan. While these designations may be refined, particularly should a development application be brought forward, the proposed Mixed Use policy (Policy 12.4.8.2.1) which states that existing office, retail and service commercial floor space is to be replaced is concerning. The Subject Lands are an ideal location for higher density, compact, mixed-use development to occur. Given the Subject Lands' current condition as a larger scale local retail plaza, this policy requirement that existing office, personal and service commercial floor space be replaced will adversely impact the ability to provide a mixture of residential, non-residential and potential community uses on the lot, while also providing for new employment opportunities and a range of new housing opportunities in support of Provincial policy directions.

Furthermore, proposed policy 12.4.8.1 states that on lands in a specified area, including the Subject Lands, at-grade retail and service commercial uses are required, and at-grade residential dwellings are not permitted. In our opinion, this policy is unnecessarily restrictive and hinders the ability to provide a wide range of complimentary uses on the same lot. We request that consideration be given to modifying the above-noted policy requirements or granting a site-specific exception.

In Section 12.4.9.9 of the draft Amendment, a new area-specific policy (Site 9) is proposed. This proposed Site 9 policy states that a minimum of three (3) floors of non-residential uses will be required for buildings on lands designated Mixed Use and Residential High Density. As stated above, both of these land use designations currently apply to the Subject Lands and this policy is concerning. We request that given the Site's locational attributes and recognized development potential to support the creation of Cooksville as a healthy, vibrant, complete 15-minute community, the Subject Lands be exempt from this policy. Exclusion of the Subject Lands from this policy requirement is appropriate given redevelopment of the Site will provide for an opportunity to incorporate a wide range of complimentary uses on the same lot while also facilitating a compact, higher density development with a refined, high-quality architectural design. Additionally, the provision of new community uses and spaces on the Subject Lands will further support the direction of City Staff to provide such spaces at-grade where they are safely, comfortably and conveniently accessed. It will also support the creation of Cooksville as a complete community, where residents and visitors are able to access facilities, services and amenities to meet daily needs within a comfortable walking distance.

Finally, we highlight that the draft Amendment seeks to implement Downtown Fairview, Cooksville and Hospital Built Form Standards ('Standards'). We understand that these Standards, which serve as urban design guidelines, are to further implement the proposed built form and Character Area



policies of the Mississauga Official Plan. We are concerned that the draft Standards which provide detailed design guidelines for developments occurring in the Downtown Fairview, Cooksville and Hospital Character Areas are restrictive and do not facilitate the best utilization of land, resources and infrastructure nor the creation of Cooksville as a vibrant, complete, 15-minute community.

In summary, we are concerned about the proposed Amendment and request that modifications be considered. Thank you for the opportunity to provide these comments. Our Client wishes to be included in the engagement for the Downtown Fairview, Cooksville and Hospital Policy Review initiative and wishes to be informed of updates and future meetings.

We look forward to being involved. Please feel free to contact the undersigned if there are any questions.

Yours very truly,

**GLEN SCHNARR & ASSOCIATES INC.**



Glen Broll, MCIP, RPP  
Partner



January 21, 2022

Planning and Development Committee  
City of Mississauga  
300 City Centre Drive  
Mississauga, Ontario L5B 3C1

Dear Members of the Committee:

***Re: Comments relating to Downtown Fairview, Cooksville and Hospital Policy Review-Draft Official Plan Amendment and Built Form Standards***

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On behalf of our client, Almega Asset Management (Almega), which is a Mississauga-based private equity investment, development and asset management company, we are providing comments to you in relation to proposed amendments to Mississauga's Official Plan (MOP) for three communities along Hurontario Street, which are identified as Downtown Fairview, Cooksville and Hospital Character Areas. The City has started this planning initiative due to the proposed Hurontario Light Rail Transit (LRT) line and forecasted increases in the population and employment in these areas.

Almega currently owns a commercial plaza site located on the south side of Dundas Street, east of Hurontario Street, municipally known as 60 Dundas Street East (the subject site). Almega proposes to redevelop and revitalize the subject site with a mixed-use development, incorporating intensification of new housing opportunities and a new public park to complement increased pedestrian access to the adjacent Cooksville Creek. The subject site is located within the Downtown Cooksville Character Area which would be affected by these proposed MOP amendments.

While we recognize that City staff will need to undertake further analysis on how these Character Areas will be impacted with the introduction of higher order transit, we believe certain issues which have been raised by the City's Corporate Report, dated December 23, 2021, merit further discussion and consideration.

Firstly, the proposed building height restrictions on Map 12-4.2 provide for a maximum of 3 to 16 storeys on the subject site. This height restriction does not appear to recognize that the Dundas Street East corridor, especially within the Downtown Cooksville Character Area, will also see higher order transit with the proposed Dundas bus rapid transit (BRT) service which will run in an east-west configuration from the City of Hamilton to the Kipling Transit Hub in the City of Toronto. As such, it is our view that increased heights should be considered at the subject site.

We also seek additional clarity with respect to Policy 12.4.8.2.1, which requires the planned function of the non-residential components to be maintained or replaced, as well as Policy 12.4.7.4, which would restrict vehicular access to Dundas. We also note that there is an existing pedestrian trail along Cooksville Creek that will be enhanced by the new park space proposed for the subject site. In our view, the gateway to this trail would benefit from a signalized intersection to allow safe pedestrian crossing.

The comments provided are intended to assist in our ongoing discussions with staff as we proceed with an application to revitalize the subject site. We look forward to continuing to engage with City staff and officials to ensure these Amendments are drafted to meet the needs of both Mississauga residents and business-owners.

Yours truly,  
**Bousfields Inc.**



Michael Bissett, MCIP, RPP

- c. Marianne Cassin, Mississauga Planning  
Adam Lucas, Mississauga Planning  
Jodi Shpigel, Almega



January 24, 2022

GSAI File: 1396-001

*(Via Email)*

Chairman and Members of Planning & Development Committee  
c/o Megan Piercey, Legislative Coordinator  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

RE: **Downtown Fairview, Cooksville and Hospital Policy Review**  
**RASDA Holdings Limited and Baro Construction Limited**  
**55, 93 Dundas Street East, City of Mississauga**

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Glen Schnarr & Associates Inc. ('GSAI') are the planning consultants to RASDA Holdings Limited and Baro Construction Limited (the 'Owner') of the lands municipally known as 55 Dundas Street East and 93 Dundas Street East, in Cooksville, in the City of Mississauga (the 'Subject Lands' or 'Site'). On behalf of the Owner, we are submitting this Comment Letter in relation to the ongoing City of Mississauga Downtown Fairview, Cooksville and Hospital Policy Review initiative ('Initiative').

GSAI has been participating in the Initiative, as well as the concurrent City of Mississauga Official Plan Review and the Region of Peel Municipal Comprehensive Review (referred to as 'Peel 2051+') initiatives. We understand that when complete, the City's Downtown Fairview, Cooksville and Hospital Policy Review Initiative will culminate in an Official Plan Amendment ('OPA') that will modify the policy framework permissions for lands within the Downtown Fairview, Cooksville and Hospital communities, including the Subject Lands.

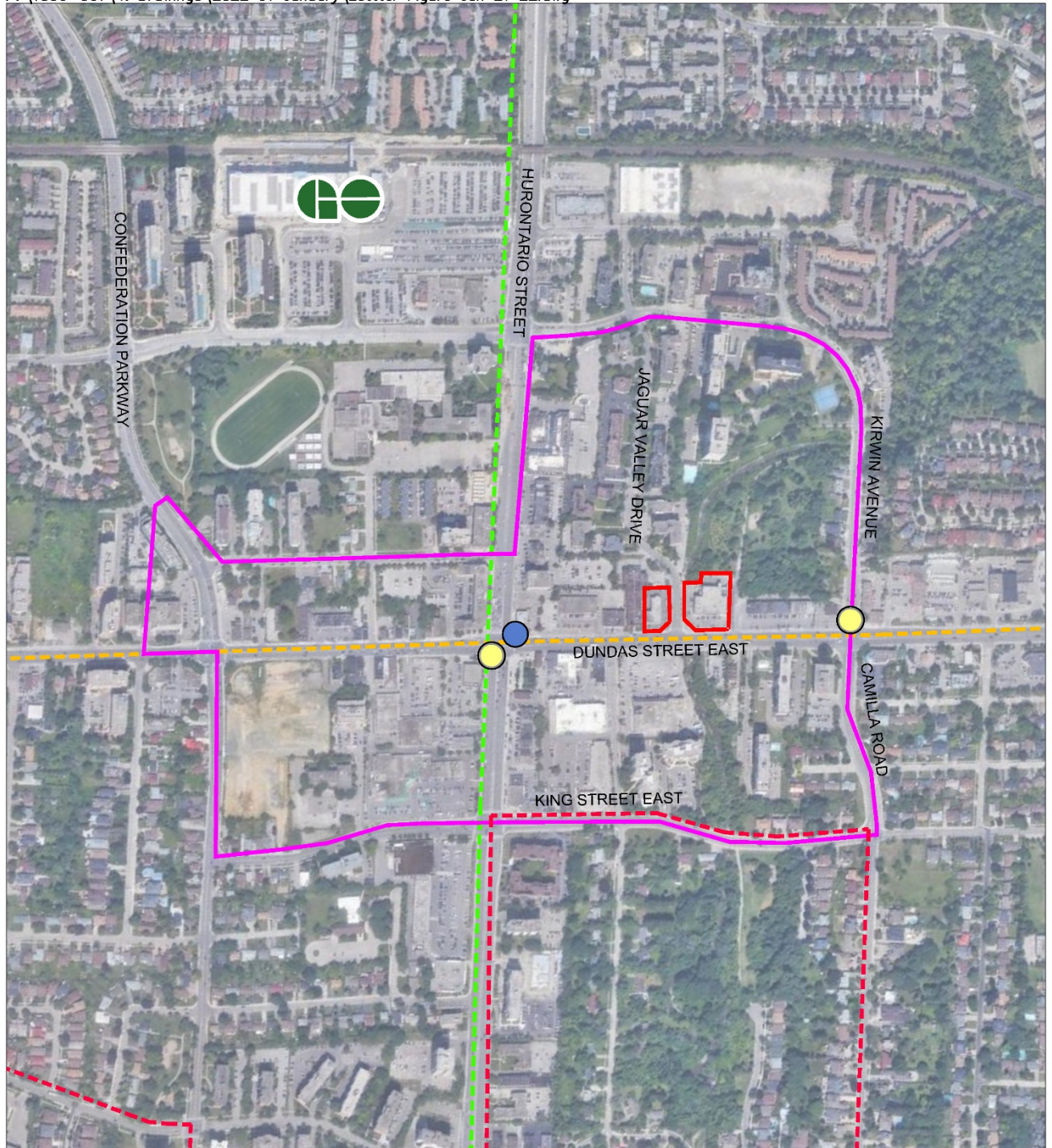
The Subject Lands are located on the north side of Dundas Street East, east of Hurontario Street (see **Context Map** on the next page). Based on the in-effect planning policy framework, the Site is situated within the Downtown Mississauga Urban Growth Centre (in accordance with Schedule 1, A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2020), a Mobility Hub – Gateway, the proposed Dundas Major Transit Station Area (in accordance with the proposed Peel Regional Official Plan, Schedule Y7 – Major Transit Station Areas), the City of Mississauga's Downtown Intensification Area and the Downtown Cooksville Character Area, the Downtown Cooksville Focus Area (in accordance with the Hurontario / Main Street Master Plan, 2011), and the Cooksville Focus Area (in accordance with the Dundas Connects Master Plan, 2018).

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## AREA CONTEXT

55 DUNDAS STREET EAST,  
PART OF BLOCK A PLAN 637,  
CITY OF MISSISSAUGA, REGIONAL MUNICIPALITY OF PEEL

93 DUNDAS STREET EAST,  
PART OF BLOCK E PLAN 637 TORONTO; PART OF LOT 15 CONCESSION 1 NDS,  
CITY OF MISSISSAUGA, REGIONAL MUNICIPALITY OF PEEL

## LEGEND

- Subject Lands
- Dundas MTSA Boundary
- BRT Station
- HuLRT Station
- MiWay Route 1 (Dundas)
- MiWay Route 2 (Hurontario)
- MiWay Route 4 (Sherway Gardens)

Cooksville GO station



Scale NTS  
January 21, 2022



When considered collectively, the in-effect policy framework identifies the Subject Lands as an appropriate location for higher density, compact, mixed-use, transit-supportive development to occur. This is strengthened (see **Context Map** on the previous page) by the Site's frontage on Dundas Street (a recognized local Corridor and future location of the Dundas Bus Rapid Transit ('BRT') network), its location within walking distance (544 metres) of the Cooksville GO Station, its location within walking distance of a future HuLRT Station) and its location within walking distance of existing street-level transit services operating in the surrounding area. The Site is also within walking distance of various services, amenities and facilities to meet daily needs and support Cooksville as a healthy, complete, 15-minute community. We note that the local policy framework, including the Hurontario / Main Street Master Plan, Cooksville Mobility Hub Study, Dundas Connects Master Plan and the City's Zoning By-law identify the Subject Lands as an appropriate and desirable location for high-density, mixed-use, pedestrian-oriented, 'tall' development to occur.

We have reviewed the draft Mississauga Official Plan Amendment ('draft Amendment'), dated December 23, 2021, and offer the following comments.

The draft Amendment proposes a policy requirement (Policy 12.4.5) and a building height schedule (Map 12-4.2) for lands within the Downtown Cooksville Character Area. Collectively, the proposed policy and Map 12-4.2 direct that the greatest heights are to be directed to lands near the Cooksville GO Station and along Hurontario Street, north of Agnes. Furthermore, maximum permitted building heights are to be in accordance with Map 12-4.2: Downtown Cooksville Character Area Minimum to Maximum Building Heights. Based on this, the Subject Lands are identified as an appropriate location for building heights of 3 to 18 and 3 to 16 storeys, respectively. It is our opinion that this building height range should be modified given the Subject Lands are directly in front of and within walking distance of existing and planned transit services, the Site is situated in an area where intensification is to be directed and there is an inconsistent applicable of building heights. For instance, building heights of up to 29 storeys are permitted on lands to the north, along the Cooksville Creek, within Downtown Cooksville.

The draft Amendment proposes refinements to the City's urban design policies and local road pattern as demonstrated on proposed Map 12-4.3: Downtown Cooksville Street Types and Figure 9: Street Types in the Downtown Cooksville Character Area. Collectively, these proposed modifications are concerning. In particular, we are concerned that the proposed urban design policies will require a height variation of 3 to 5 storeys where more than one structure is to be provided, a 45 degree angular is to be provided as measured from the shared property line with surrounding low and medium density residential areas, and Jaguar Valley Drive is to be provided as a new 'B' Street. 'B' Streets are proposed to be residential in nature and development is to provide for 'generous setbacks from the street' (Policy 12.4.6.2.2.1.b). When considered collectively, the above-noted policy directions will impact the development potential of the Subject Lands and the ability to support the development vision for Dundas Street as a pedestrian-oriented, active, complete main street.





The draft Amendment presents refined land use policies, in particular policies pertaining to lands designated as 'Mixed Use'. The Subject Lands are currently designated 'Mixed Use' by the Mississauga Official Plan. Policy 12.4.8.2.1 which requires that existing office, retail and service commercial floor space is to be replaced is concerning. The Subject Lands are an ideal location for higher density, compact, mixed-use development to occur. We are also concerned by Policy 12.4.8.2.3.a which states that new development is to provide various convenient, easily accessible office, retail and service commercial uses. Given the Subject Lands' current condition as local retail plazas containing a variety of uses, the policy requirement to replace existing floor space and the policy requirement to provide office space will adversely impact the ability to provide a mixture of residential and non-residential uses on the lot, while also providing for new employment opportunities and a range of new housing opportunities. Additionally, the requirement to provide non-residential uses including office may not represent current or market trends. We request that consideration be given to modifying this policy requirement or granting a site-specific exception.

We are encouraged by Policy 12.4.9.1 which proposes modified height permissions of up to 18 storeys on the lands municipally addressed as 55 Dundas Street East. We are concerned however by other provisions of this amended site-specific policy which requires three floors of non-residential uses to be provided. This is unnecessarily restrictive and may adversely impact development potential. Exclusion of the Subject Lands from this policy requirement is appropriate given the Site is able to provide for a wide range of non-residential uses, including uses that open onto and address the Dundas Street public realm. Furthermore, the Subject Lands are appropriately situated to facilitate a compact, higher density development with a refined, high-quality architectural design. We request that site-specific Policy 1 be modified or a site-specific exclusion be granted.

Finally, we highlight that the draft Amendment seeks to implement Downtown Fairview, Cooksville and Hospital Built Form Standards ('Standards'). We understand that these Standards, which serve as urban design guidelines, are to further implement the proposed built form and Character Area policies of the Mississauga Official Plan. We are concerned that the draft Standards which provide detailed design guidelines for developments occurring in the Downtown Fairview, Cooksville and Hospital Character Areas are restrictive and do not facilitate the best utilization of land, resources and infrastructure nor the creation of Cooksville as a vibrant, complete, 15-minute community.

In summary, we are concerned about the proposed Amendment and request that modifications be considered. Thank you for the opportunity to provide these comments. Our Client wishes to be included in the engagement for the Downtown Fairview, Cooksville and Hospital Policy Review initiative and wishes to be informed of updates and future meetings.

We look forward to being involved. Please feel free to contact the undersigned if there are any questions.





**GLEN SCHNARR & ASSOCIATES INC.**  
URBAN & REGIONAL PLANNERS, LAND DEVELOPMENT CONSULTANTS

5.6

Yours very truly,

GLEN SCHNARR & ASSOCIATES INC.

Glen Broll, MCIP, RPP  
Partner

cc. Owner



January 21, 2022

Karin Phuong, Project Lead – Planner  
City of Mississauga  
City Planning Strategies Division  
300 City Centre Drive  
Mississauga, Ontario

**Re: Trillium Health Partners' (THP) Response to Draft Policies  
Downtown Fairview, Cooksville and Hospital Character Areas  
File: CD.03-DOW**

Dear Ms. Phuong,

On November 22<sup>nd</sup>, 2021 Trillium Health Partners (**THP**) received draft policies of the *Downtown Fairview, Cooksville and Hospital Policy Review – Draft Official Plan Amendment and Built Form Standards* as part of City of Mississauga Planning and Building Department's initial engagement for the policy review. THP appreciates the opportunity to continue engaging with City staff and provide input to the development of policies that will shape both the Mississauga Hospital and surrounding areas.

THP notes the proposed policies are intended to set out a vision and key guiding principles for each of the three Character Areas, including the Downtown Hospital Character Area (**DHCA**), with new policies introduced for building heights, land uses, transportation connections, and urban form. The package also included five maps highlighting draft changes to Schedules of the Mississauga Official Plan.

THP's Planning Consultant, DIALOG, has undertaken a review of the draft policies of the DHCA received on November 22<sup>nd</sup>, 2021. We note the draft policies attached to the Agenda for the Planning & Development Committee (**PDC**) meeting of January 24<sup>th</sup>, 2022 contain further updates from those previously circulated as well as a more robust draft Built Form Standards document. THP will undertake a review of those updated draft policies in due course and looks forward to continuing conversations with City staff on them following this meeting.

### **The New Mississauga Hospital Project**

THP is the owner and operator of the Mississauga Hospital, a significant health care provider for both local and regional populations for more than 62 years. The aging infrastructure of the Mississauga Hospital located at 100 Queensway West is no longer able to meet 21<sup>st</sup> century community needs, and the pandemic has further underlined the urgent need for THP to move forward with plans to expand and modernize its facilities so that it can deliver the care this community needs.

The redevelopment of the Mississauga Hospital envisions a full replacement of the existing hospital, creating a modern health care facility that meets the growing and changing health care needs of the community, with over 950 beds.

On December 16<sup>th</sup>, 2021, THP submitted a Site Plan Control Application (**SP 22-2 W7**) for the new 24-storey hospital tower (inclusive of mechanical penthouse and rooftop elements/projections) at 100 Queensway West and the adjacent Camilla Care lands located at 2250 Hurontario Street. This was THP's second development application, following an earlier Site Plan Control Application (**SP 21-156 W7**) to facilitate approvals for the development of a new 8-storey parking structure.

### **Previous THP Response Submissions to City**

THP previously submitted comments to City staff in support of the draft policy review on December 3<sup>rd</sup>, 2020 and January 25<sup>th</sup>, 2021. These submissions are included in **Appendix A** for context. Broadly speaking, in the first response letter, THP requested:

- Language to reflect the importance of the hospital facility within the community;
- Maintenance of existing flexible standards within Institutional OP designation;
- Incorporation of additional permitted land uses within the existing Institutional exception ("I-2") zone; and
- Application of Institutional zoning standards to 2250 Hurontario St. (in light of ongoing acquisition process).

As part of the second response letter, THP responded to two specific queries from City staff and continued to request inclusion of high-density residential as a permitted land use within the Institutional zone. The specific responses to City staff queries included:

- Acknowledgement of the integral role open and accessible green and parkland-type spaces will play in future development of the Subject Site, but no intention for a stand-alone parkland parcel at the hospital site; and
- An update to City staff on the acquisition and rezoning process related to 2250 Hurontario St, clarifying THP intent to permit hospital uses and institutional zoning standards on that property as well as part of the new hospital in-patient tower.

### **Review of Downtown Hospital Draft Policies**

For this review, THP's response to draft policies are limited to those proposed for the DHCA in November 2021. We will articulate any additional comments on the further revised policies and completed draft Built Form Standards as part of subsequent engagement opportunities with City staff. THP requests that the proper names of "Trillium Health Partners" and the "Mississauga Hospital" be used throughout the document where relevant.

- ***Plans & Policies related to 2250 Hurontario St***

We note for City staff that the above-mentioned development application for the New Mississauga Hospital Project includes both 100 Queensway West and 2250 Hurontario Street. An Agreement of Purchase and Sale (**APS**) was reached between THP and the owners of 2250 Hurontario St. in late-2021, with an anticipated closing in Spring 2022. As our project continues to move forward, and as the policy review continues to progress in parallel, THP notes for City staff that the municipal approvals process for the New Mississauga Hospital Project is inclusive of both properties.

- **Introduction (12.5.1) & Vision (12.5.2)**

THP supports the continued policy direction of making the Mississauga Hospital the focal point of a walkable and transit-oriented DHCA. THP agrees with the concept of a health district surrounding the Mississauga Hospital supported by a broad range of uses. The new draft Vision section creates a new emphasis on a health district clustered around the Hospital, with supporting uses such as retail and service commercial uses, research and development, health facilities, offices, parks and residential uses. This is consistent with comments submitted by THP to the City on December 3<sup>rd</sup>, 2020.

Under the first guiding principle of the vision, THP requests the City consider the following additional language: "Support health care services – promote a range of health care services and supportive uses **that create a healthier community**;"...

- **Floor Space Index (FSI) / Map 12-5**

THP notes 100 Queensway West is identified as part of Special Site 7 (a designation which should also encompass 2250 Hurontario Street). THP notes no specified FSI has been identified for 100 Queensway West, consistent with our request for a continuation of flexible standards within our Institutional designation. THP notes for City staff that the current municipal approvals process for the New Mississauga Hospital Project is inclusive of both 100 Queensway West and 2250 Hurontario Street. THP also notes that the FSI information is labeled for information only and will instead be implemented through the City's Major Transit Station Area (**MTSA**) Official Plan. THP requests to be notified of further opportunities to engage with City and Regional staff on relevant policies of the City's broader amendment related to MTSA's to conform to the Region's Official Plan Amendment.

- **General (12.5.3) – Figure 1**

THP's mandate is to protect the future growth potential on the Subject Site and surrounding properties with regards to the permissibility of uses that will support healthcare delivery objectives. Consistent with our correspondence of January 25<sup>th</sup>, 2021, THP agrees that a balanced and integrated land use mix is essential for a successful health district/cluster and envisions that open and accessible and green spaces will be an integral part of the DHCA. However, THP is not currently considering a stand-alone parkland parcel at the hospital site as this requirement would limit the future flexibility of the site. THP requests removal of the conceptual identification of a "Future Public Open Space" in Figure 1 over the western half of 100 Queensway West.

- **Building Height (12.5.4) – Map 12-5.2**

Consistent with our first comment above, THP requests that Map 12-5.2 be updated to align the height restrictions on 2250 Hurontario St with the adjacent

100 Queensway West property. In light of development application **SP22-2 W7**, both 100 Queensway West and 2250 Hurontario Street should have a blue hatch pattern to indicate “Development applications in process”, as per the draft legend of the map.

- **Urban Form (12.5.5)**

THP will provide more comprehensive responses to matters related to urban form once we review the updated Built Form Standards document (not previously provided) in more detail. As a general note, however, THP requests that urban form standards are written with appropriate flexibility and/or allowances for exemptions for institutional uses recognizing the unique programming needs for such uses and the impact those may have on built form. For example, THP notes that draft policies **12.5.5(a)** and **12.5.5(c)** do not retain reference to “institutional” frontage (as per Figure 8.9.1 of the Hurontario/Main Street Corridor Master Plan). THP requests relevant updates to include mention of “institutional use” as part of an active building frontage in policy 12.5.5(c) (e.g. “institutional lobbies”). For other draft built form standards (such as draft policies 12.5.5(e) minimum building separation distances, 12.5.5.3 podium stepbacks and 12.5.5.4 maximum tower floor plate size), THP requests flexible language and/or exemptions for institutional uses.

- **Transportation (12.5.7)**

THP requests clarification from City staff regarding the draft requirement for public easements for pedestrian connections on private land. THP would like to further understand the definition of “pedestrian connection” and if the intention of this draft policy is to have easements registered on title to secure such connections.

- **Special Sites (12.5.9)**

THP notes the designation of 100 Queensway West as part of Area B (now Area C in the further revised draft policies). THP notes for City staff that the current municipal approvals process for the New Mississauga Hospital Project is inclusive of both 100 Queensway West and 2250 Hurontario Street.

Regarding the proposed land uses under **12.5.9.7.3**, THP has been working closely with City staff in the Development and Design Division (**DDD**) to identify appropriate land use permissions for both sites. THP requests that the CPS Team coordinate with the DDD Team to align the draft land use permissions with those emerging under discussion as part of our ongoing municipal approvals.

In addition, and further to our previous correspondence dated January 25<sup>th</sup>, 2021, THP notes the draft policies acknowledge that the Mississauga Hospital site is surrounded by residential apartment dwellings, while the broader DHCA is predominantly residential and largely designated as “Residential High Density”. The Subject Site is located within an Intensification Area; these areas are further guided by policies in the Mississauga Official Plan that support redevelopment and

higher densities and mixed uses serviced by multi-modal transportation (100 Queensway West and 2250 Hurontario Street are located immediately west of the future Metrolinx Hurontario Light Rail Transit (LRT) – Queensway Station). From a planning perspective, the inclusion of “Residential High Density” would allow the site to achieve its full development potential within the immediate vicinity of a key higher-order transit node and be further integrated to a broader and complete community supportive of a connected health care system.

- **Queensway/Hurontario Gateway**

THP notes that the draft policies for DHCA no longer include identification of the Queensway and Hurontario Street intersection as a place-making gateway (see **Figure 8.9.6** of the existing Hurontario/Main Street Corridor Master Plan). As part of our ongoing development applications, THP has been working closely with City staff to position the new proposed hospital tower as an urban landmark that frames the southernmost entrance to the City’s Downtown areas. THP would support the continued designation of Hurontario and Queensway as a gateway intersection that links the City’s main north-south transit corridor and its east-west recreation corridor along Queensway. In particular, THP supports the stated potential for an urban plaza or gateway feature located at this intersection and requests such language be retained in the updated policies.

## Conclusion

We thank you again for the opportunity to provide our input into the development of policies that will shape both the Mississauga Hospital and surrounding areas. THP supports the City’s ambition to enable development aligned with the mission of THP to create a “new kind of health care for a healthier community” and is excited for the transformational opportunity this draft policy review represents for the New Mississauga Hospital Project and the potential it creates for an emerging health district. We look forward to working closely with you to meet the community’s health care needs through the Downtown Fairview, Cooksville and Hospital Policy Review. We would be happy to continue this conversation with you and are open to a follow-up meeting to discuss this item further should you deem it helpful to better understand our rationale and position.

Sincerely,

*Andrew Matheson*

Andrew Matheson  
Associate / Senior Urban Planner, DIALOG  
Amatheson@dialogdesign.ca

cc.  
David Longley, THP  
Matthew Kenney, THP  
Shawn Kerr, THP  
Steve Silva, THP



Antonio Gomez-Palacio, DIALOG  
Corey Horowitz, DIALOG





January 24, 2022

GSAI File: 1319-001

*(Via Email)*

Chairman and Members of Planning & Development Committee  
c/o Megan Piercey, Legislative Coordinator  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

RE: **Downtown Fairview, Cooksville and Hospital Policy Review**  
**Equity Three Holdings Inc.**  
**3085 Hurontario Street, City of Mississauga**

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Glen Schnarr & Associates Inc. ('GSAI') are the planning consultants to Equity Three Holdings Inc. (the 'Owner') of the lands municipally known as 3085 Hurontario Street, in Cooksville, in the City of Mississauga (the 'Subject Lands' or 'Site'). On behalf of the Owner, we are submitting this Comment Letter in relation to the ongoing City of Mississauga Downtown Fairview, Cooksville and Hospital Policy Review initiative ('Initiative').

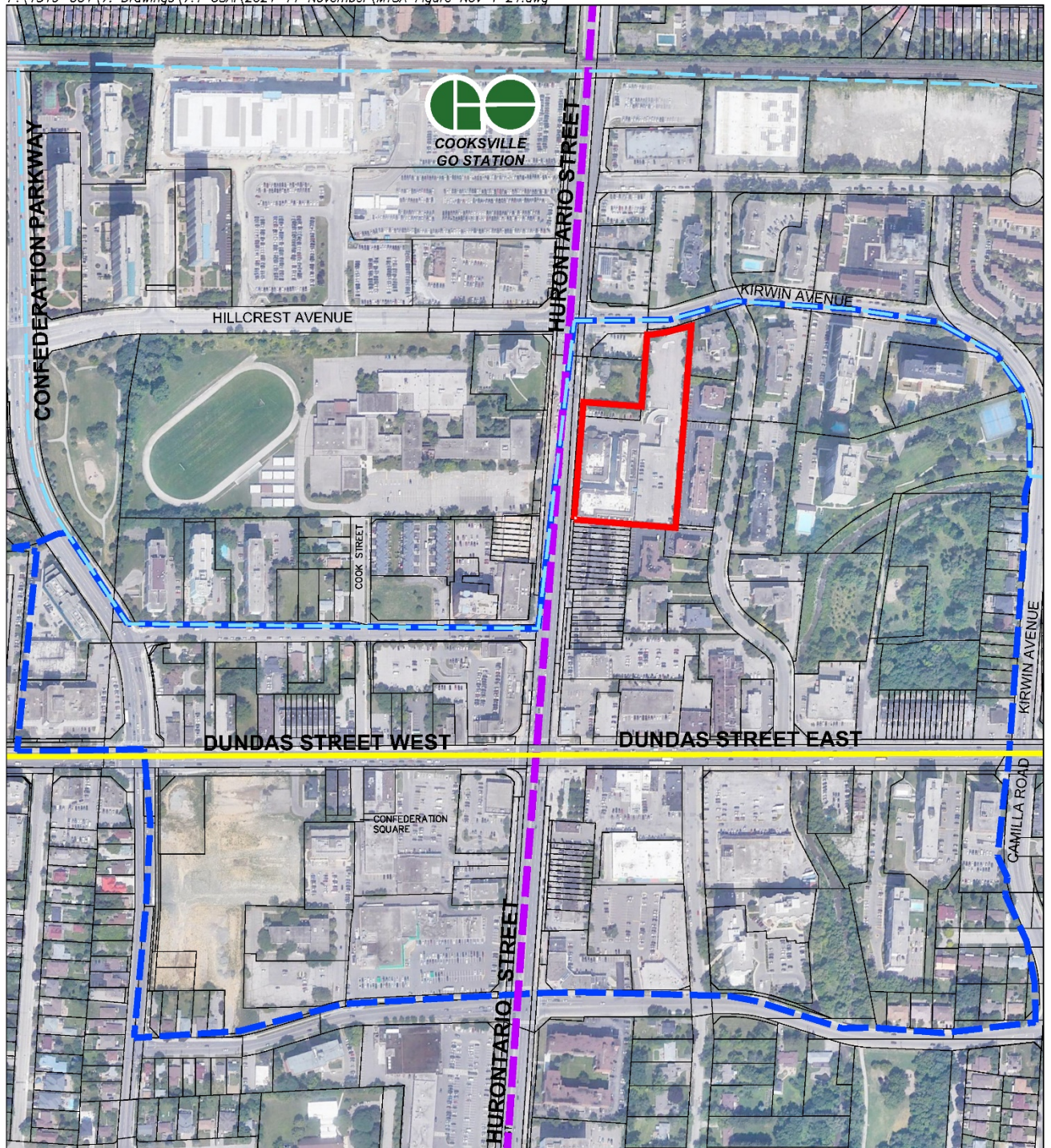
GSAI has been participating in the Initiative, as well as the concurrent City of Mississauga Official Plan Review and the Region of Peel Municipal Comprehensive Review (referred to as 'Peel 2051+') initiatives. We understand that when complete, the City's Downtown Fairview, Cooksville and Hospital Policy Review Initiative will culminate in an Official Plan Amendment ('OPA') that will modify the policy framework permissions for lands within the Downtown Fairview, Cooksville and Hospital communities, including the Subject Lands.

The Subject Lands are located on the east side of Hurontario Street, south of Kirwin Avenue (see **Context Map** on the next page). Based on the in-effect planning policy framework, the Site is situated within the Downtown Mississauga Urban Growth Centre (in accordance with Schedule 1, A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2020), a Mobility Hub – Gateway (in accordance with the Peel Regional Official Plan), the proposed Dundas Major Transit Station Area (in accordance with the proposed Peel Regional Official Plan, Schedule Y7 – Major Transit Station Areas), the City of Mississauga's Downtown Intensification, the Downtown Cooksville Character Area, the Downtown Cooksville Focus Area (in accordance with the Hurontario / Main Street Master Plan, 2011), and the Cooksville Focus Area (in accordance with the Dundas Connects Master Plan).

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**CONTEXT MAP**  
**3085 HURONTARIO STREET**  
CITY OF MISSISSAUGA, REGIONAL MUNICIPALITY OF PEEI

**LEGEND**

-  Subject Lands
-  Dundas Major Transit Station Area
-  Cooksville GO Major Transit Station Area
-  MiWay - Route 1 (Dundas)
-  MiWay - Route 2 (Hurontario)



Scale 1:5000  
November 1, 2021





When considered collectively, the in-effect policy framework identifies the Subject Lands as an appropriate location for higher density, compact, mixed-use, transit-supportive development to occur. This is strengthened (see **Context Map** on the previous page) by the Site's frontage on Hurontario Street (a recognized Regional Intensification Corridor, a recognized local Corridor and future location of the Hurontario Light Rail Transit ('HuLRT') network), its location within walking distance (220 metres) of the Cooksville GO Station, its location within walking distance of a future HuLRT Station, its location within walking distance (220 metres) of the planned Dundas Bus Rapid Transit ('BRT') network, and its location within walking distance of existing street-level transit services operating in the surrounding area. The Site is also within walking distance of various services, amenities and facilities to meet daily needs and support Cooksville as a complete, healthy, 15-minute community. We note that the local policy framework, including the Hurontario / Main Street Master Plan, Cooksville Mobility Hub Study and Dundas Connects Master Plan identify the Subject Lands as an appropriate and desirable location for high-density, mixed-use, pedestrian-oriented, 'tall' development to occur.

We note that the Subject Lands are subject to an active development application (City File OZ-OPA 21-11). As further demonstrated in the accompanying submission materials that are under technical review by City, Regional and Agency Staff, the Owner is seeking permission to redevelop the Site for a compact, pedestrian-oriented, transit-supportive, mixed use development. This is to be achieved through the provision of three (3) high-quality built forms (Buildings 1, 2 and 3) of varying heights, a mixture of uses, amenity spaces and dwelling units of varying sizes and configurations. Building 1 is situated in the southwest quadrant of the Site and is a 30-storey structure rising above a 7-storey podium. The proposed podium, which steps back, is to open onto and address the Site's Hurontario Street frontage. The proposed podium is to also include a range of grade-related non-residential uses with direct pedestrian connections, a selection of 2-storey, live-work units, amenity areas, a residential lobby and residential units. Building 2 is proposed in the southeast quadrant of the Site and is to have a 33-storey and 35-storey tower component that rise above a shared 9-storey podium. Finally, Building 3 is proposed in the northeast quadrant of the Site and is a 9-storey residential structure.

Overall, the proposed development has been organized around a central, landscaped outdoor courtyard, a series of pedestrian pathways, landscaped open spaces and a safe, comfortable, grade-related continuous retail frontage along the Site's Hurontario streetscape. The proposed structures provide for sufficient building separation distances and have incorporated stepbacks above a podium level to facilitate pedestrian-oriented built forms, maximize access to direct sunlight and facilitate a refined design. Overall, the proposed development has been planned and designed to conform to the Provincial, Regional and local policy frameworks of directing growth to an appropriate location that makes best utilization of land, resources and infrastructure to support Downtown Cooksville as a healthy, vibrant, complete, 15-minute community.

We have reviewed the draft Mississauga Official Plan Amendment ('draft Amendment'), dated



December 23, 2021, and offer the following comments.

The draft Amendment proposes policy requirement (Policy 12.4.5) and a building height schedule (Map 12-4.2) for lands within the Downtown Cooksville Character Area. Collectively, the proposed policy and Map 12-4.2 direct that the greatest heights are to be directed to lands near the Cooksville GO Station and along Hurontario Street, north of Agnes. Furthermore, maximum permitted building heights are to be in accordance with Map 12-4.2: Downtown Cooksville Character Area Minimum to Maximum Building Heights. As such, the Subject Lands are identified as an appropriate location for building heights of 3 to 25 storeys. It is our opinion that this building height range should be modified to permit heights up to 35 storeys on the Subject Lands given the Site is directly in front of and within walking distance of existing and planned transit services, it is situated in an area where intensification is to be directed, there is an absence of sensitive uses, parks or natural features that would be negatively impacted and there is an inconsistent applicable of building heights. More specifically, we note that building heights of up to 29 storeys are permitted on lands to the east and west within Downtown Cooksville community. Additionally, lands to the north of Downtown Cooksville have permitted heights of up to 50 storeys in instances.

Similarly, the draft Amendment directs that where multiple structures are proposed on a lot, a height variation of 3 to 5 storeys is to be achieved (Policy 12.4.6.1.2) and that podiums are to have heights of 3 to 6 storeys (Policy 12.4.6.1.3). These policies are concerning and should be revised. More specifically, the appropriate height variation between structures on a lot and the appropriate height of podiums should be determined through the development review process.

The draft Amendment proposes refinements to the local road pattern as demonstrated on proposed Map 12-4.3: Downtown Cooksville Street Types and Figure 9: Street Types in the Downtown Cooksville Character Area. Collectively, the proposed Map 12-4.3 and Figure 9 direct that a new local road, identified as a 'C' Street is to be provided along the Subject Lands' southern property line to provide a mid-block connection between Hurontario Street and Jaguar Valley Drive and to facilitate 'vehicular access to on-site service, loading, parking and garbage storage areas'. Furthermore, the segment of Kirwin Avenue directly in front of the Subject Lands is identified as a 'B' Street (see image on the next page). 'B' Streets are proposed to be residential in nature and development is to provide for 'generous setbacks from the street' (Policy 12.4.6.2.2.1.b). We note that the draft Amendment does not provide sufficient detail regarding the anticipated ultimate Right-of-Way widths, alignments or cross-sections of these proposed roads.

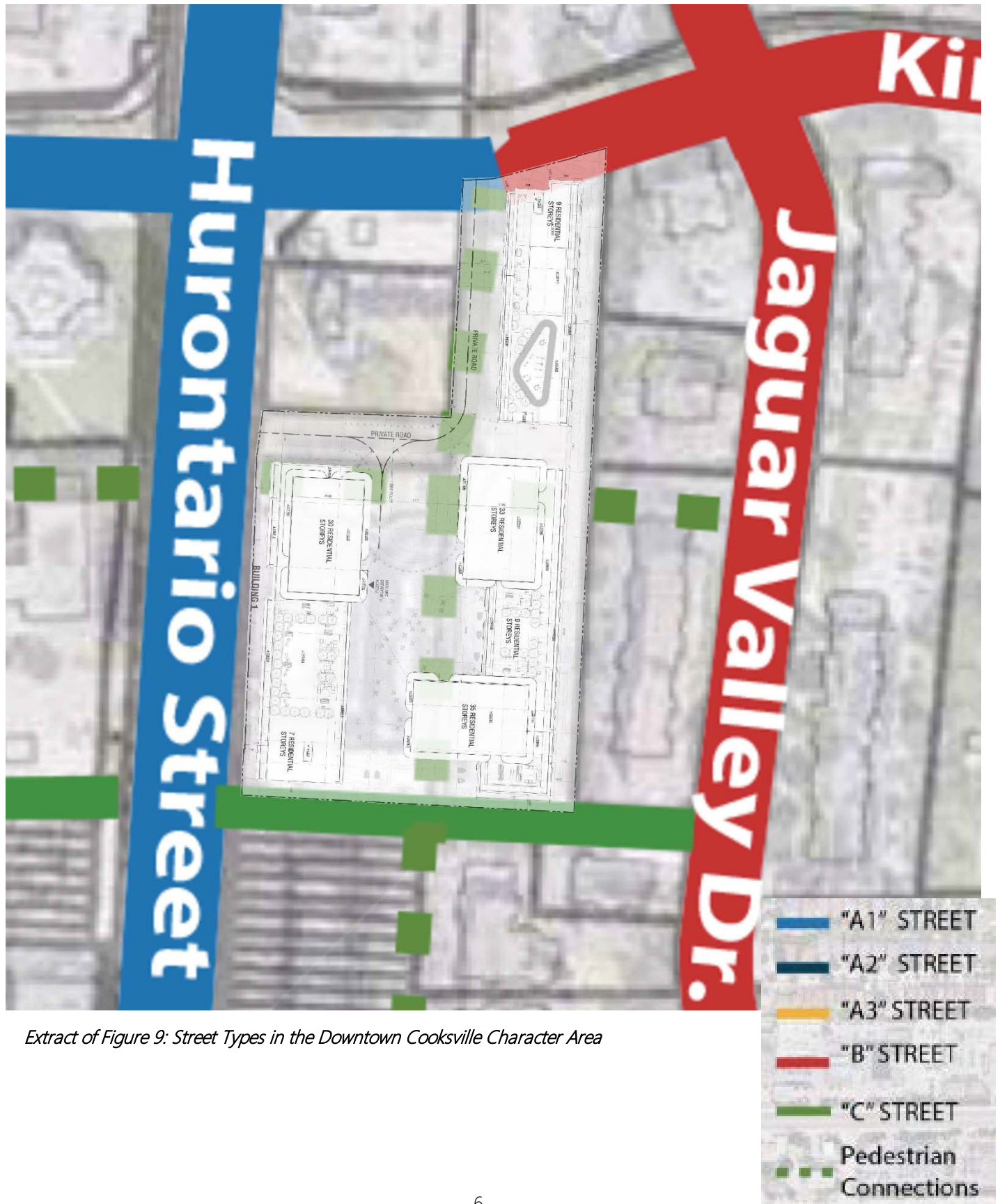
As currently contemplated, the positioning of Street 'C' and the proposed identification of Kirwin Avenue as a 'B' Street will significantly and adversely impact the development potential of the Subject Lands. It will also negatively impact the current development concept under consideration (City File OZ-OPA 21-11) by City, Regional and Agency Staff. More specifically, provision of Street 'C' in the location identified would result in a sizable land taking and the removal of proposed pedestrian networks, landscaped open spaces and a refined, mixed-use built form. Similarly, the



provision of Kirwin Avenue as a 'B' Street may require additional road widening and would result in the need for a revised development concept. We request that further detail be provided and the potential provision of new public roadways be negotiated as part of the site-specific development review process.

As further demonstrated in the image on the next page, the proposed Amendment seeks refinements to the community-wide pedestrian network. More specifically, Figure 9 of the proposed Amendment directs that a series of pedestrian connections are to be provided across the Subject Lands and other lands in order to provide safe, comfortable and convenient access across the Downtown Cooksville community. Provision of pedestrian connections in the desired locations would bisect the Subject Lands and significantly and adversely impact the development potential of the Subject Lands. Specifically, provision of the proposed pedestrian connections in the desired locations would result in significant reductions to the proposed built form and alter the proposed, optimal site design. These proposed connections would also require additional land takings and the requirement for long-term public access easements to be secured. We note that the proposed east-west connection would require modifications to an existing rental complexes along Jaguar Valley Drive – which would be contrary to the City's rental protection policy directions.

Based on the above, we request that the provision of pedestrian connections be removed and instead the provision of pedestrian pathways that support safe, comfortable and convenient movements across a Site be supported.



*Extract of Figure 9: Street Types in the Downtown Cooksville Character Area*





The draft Amendment presents refined urban design policies including Policy 12.4.6.1.1. Policy 12.4.6.1.1.a) states that a 45 degree angular plane is to be applied from the property line adjacent to low and medium residential areas in order to provide for appropriate transition. In this instance, this proposed policy may implement a requirement for a 45 degree angular plane to be applied, as measured from the Subject Lands' eastern property line. In our opinion, the residential areas immediately east are high-density in nature and demonstrate that Downtown Cooksville is already a compact, urban community. Application of an angular plane is inappropriate and will significantly and adversely impact the development potential of the Subject Lands. We request that this policy be modified to exclude the Subject Lands.

The draft Amendment presents refined land use policies, in particular policies pertaining to lands designated as 'Mixed Use'. The Subject Lands are currently designated 'Mixed Use' by the Mississauga Official Plan. While the active development application seeks to re-designate the Site to 'Residential High Density', the proposed Mixed Use policy (Policy 12.4.8.2.1) which states that existing office, retail and service commercial floor space is to be replaced is concerning. The Subject Lands are an ideal location for higher density, compact, mixed-use development to occur. Given the Subject Lands' current condition as a local retail plaza, this policy requirement that existing office, personal and service commercial floor space be replaced will adversely impact the ability to provide a mixture of residential and non-residential uses on the lot, while also providing for new employment opportunities and a range of new housing opportunities. We request that this policy requirement be removed as market demands have changed. In our opinion, the need for specified non-residential uses should be determined by market trends and the findings of a Market Impact Study.

In Section 12.4.9.9 of the draft Amendment, a new area-specific policy (Site 9) is proposed. This proposed Site 9 policy states that a minimum of three (3) floors of non-residential uses will be required for buildings on lands designated Mixed Use and Residential High Density. We request that the Subject Lands be exempt from this policy. Exclusion of the Subject Lands from this policy requirement is appropriate given the proposed development has been planned and designed to facilitate a compact, higher density development with a refined, high-quality architectural design. Additionally, a range of non-residential uses are proposed and will be provided in a manner that accommodates a refined design, an optimal site design and is based on market demands.

Finally, we highlight that the draft Amendment seeks to implement Downtown Fairview, Cooksville and Hospital Built Form Standards ('Standards'). We understand that these Standards, which serve as urban design guidelines, are to further implement the proposed built form and Character Area policies of the Mississauga Official Plan. We are concerned that the draft Standards which provide detailed design guidelines for developments occurring in the Downtown Fairview, Cooksville and Hospital Character Areas are restrictive and do not facilitate the best utilization of land, resources and infrastructure nor the creation of Cooksville as a vibrant, complete, 15-minute community.

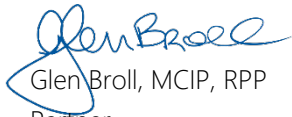


In summary, we are concerned about the proposed Amendment and request that modifications be considered. Thank you for the opportunity to provide these comments. Our Client wishes to be included in the engagement for the Downtown Fairview, Cooksville and Hospital Policy Review initiative and wishes to be informed of updates and future meetings.

We look forward to being involved. Please feel free to contact the undersigned if there are any questions.

Yours very truly,

**GLEN SCHNARR & ASSOCIATES INC.**



Glen Broll, MCIP, RPP  
Partner

cc. Owner

Frank Doracin

Councillor Damerla



January 24, 2022

GSAI File: 054-006

*(Via Email)*

Chairman and Members of Planning & Development Committee  
c/o Megan Piercey, Legislative Coordinator  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

RE: **Downtown Fairview, Cooksville and Hospital Policy Review**  
**Pro-M Capital Partners Inc.**  
**2434 – 2490 Shepard Avenue, City of Mississauga**

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Glen Schnarr & Associates Inc. ('GSAI') are the planning consultants to Pro-M Capital Partners Inc. (the 'Owner') of the lands municipally known as 2434, 2442, 2466, 2472, 2484, 2484 and 2490 Shepard Avenue, in the City of Mississauga (the 'Subject Lands' or 'Site'). On behalf of the Owner, we are submitting this Comment Letter in relation to the ongoing City of Mississauga Downtown Fairview, Cooksville and Hospital Policy Review initiative ('Initiative').

GSAI has been participating in the Initiative, as well as the concurrent City of Mississauga Official Plan Review and the Region of Peel Municipal Comprehensive Review (referred to as 'Peel 2051+') initiatives. We understand that when complete, the City's Downtown Fairview, Cooksville and Hospital Policy Review Initiative will culminate in an Official Plan Amendment ('OPA') that will modify the policy framework permissions for lands within the Downtown Fairview, Cooksville and Hospital communities, including the Subject Lands.

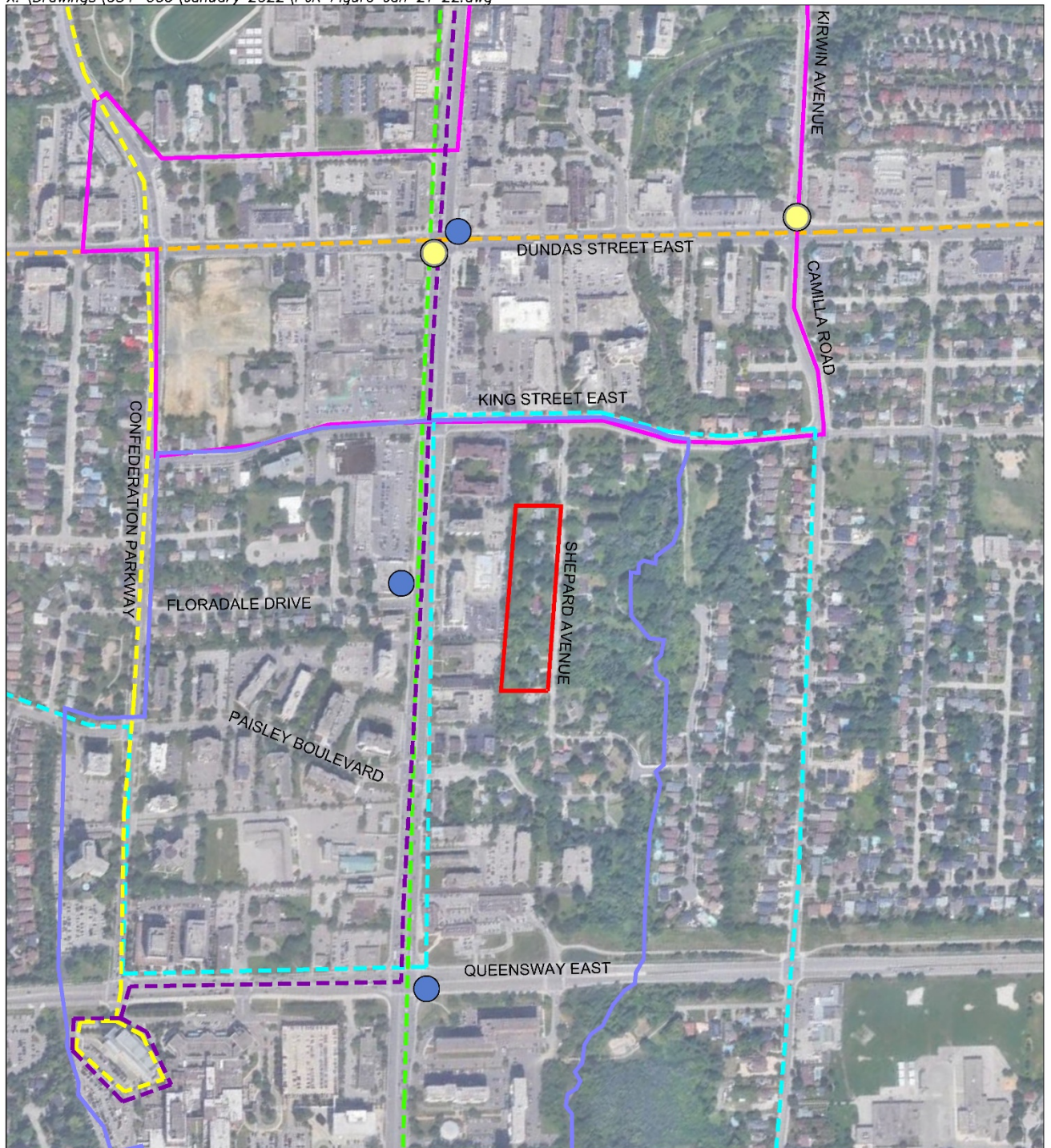
The Subject Lands are located on the west side of Shepard Avenue, north of Paisley Boulevard East and south of King Street East (see **Area Context** on the next page). Based on the in-effect planning policy framework, the Site is situated within the Downtown Mississauga Urban Growth Centre (in accordance with Schedule 1, A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2020), the proposed Queensway Major Transit Station Area (in accordance with the proposed Peel Regional Official Plan, Schedule Y7 – Major Transit Station Areas), the City of Mississauga's Downtown Intensification Area and the City's Downtown Hospital Character Area. It is also adjacent to the Downtown Hospital Focus Area (in accordance with the Hurontario / Main Street Master Plan, 2011).

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## AREA CONTEXT

2434 - 2490 SHEPARD AVENUE,  
CITY OF MISSISSAUGA, REGIONAL MUNICIPALITY OF PEEL

## LEGEND

- |  |                                      |  |               |
|--|--------------------------------------|--|---------------|
|  | Subject Lands                        |  | BRT Station   |
|  | Dundas MTSA Boundary                 |  | HuLRT Station |
|  | Queensway MTSA Boundary              |  |               |
|  | MiWay Route 1 (Dundas)               |  |               |
|  | MiWay Route 2 (Hurontario)           |  |               |
|  | MiWay Route 4 (Sherway Gardens)      |  |               |
|  | MiWay Route 28 (Confederation)       |  |               |
|  | MiWay Route 103 (Hurontario Express) |  |               |



Scale NTS  
January 21, 2022



When considered collectively, the in-effect policy framework identifies the Subject Lands as an appropriate location for higher density, compact, mixed-use, transit-supportive development to occur. This is strengthened (see **Area Context** on the previous page) by the Site's location within distance of the planned and funded Dundas Bus Rapid Transit ('BRT') network, its location within walking distance of future HuLRT Stations and its location within walking distance of existing street-level transit services operating in the surrounding area. The Site is also within walking distance of various services, amenities and facilities to meet daily needs and support Cooksville as a healthy, complete, 15-minute community.

We have reviewed the draft Mississauga Official Plan Amendment ('draft Amendment'), dated December 23, 2021, and offer the following comments.

The draft Amendment proposes refinements to the City's Downtown Hospital Character Area policy framework. In particular, the draft Amendment seeks to implement revised building height permissions, new pedestrian connections and revised urban design policies. These are concerning and modifications are requested.

Figure 1 of the draft Amendment identifies the desired locations of new pedestrian connections and public parkland. As such, a new pedestrian connection is identified on the Subject Lands in order to provide a mid-block connection between Hurontario Street and Shepard Avenue. Provision of this pedestrian connection would bisect the Subject Lands and negatively impact the development potential. We request a modification be made to facilitate contextually appropriate pedestrian connections, evaluated on a site-by-site basis.

The draft Amendment also seeks to implement a building height schedule, identified as Map 12-5.2: Downtown Hospital Character Area Minimum to Maximum Building Heights. Based on this, the Subject Lands are identified as an appropriate location for buildings 3 to 8 storeys in height. In our opinion, this permitted height requires modification as lands immediately adjacent along Hurontario Street have increased height permissions. Furthermore, as a Site within a proposed MTSA and within the Downtown Mississauga Urban Growth Centre, the suggested building heights are inconsistent with the Provincial, Regional and local policy directions to accommodate compact, higher density, mixed-use, transit-supportive development in these areas.

The draft Amendment proposes refinement to the area-specific urban form policies (Section 12.5.5). These proposed policies are concerning given the development potential of the Subject Lands will be significantly and adversely impacted given the requirement for application of a 45 degree angular plane, height variation of 3 to 5 storeys where multiple buildings are proposed and maximum floor plate allowances. We are also concerned by the identification of Shepard Avenue as a 'B' Street which may require additional land dedication. . We highlight that the policy requirement to comply with a 45 degree angular plane as measured from the property line adjacent to low and medium residential areas is unnecessarily restrictive and will hinder the ability for well-



designed, compact, higher density, transit-supportive development.

The draft Amendment also presents refined area-specific policies, in particular Site 1. The Subject Lands are and continue to be subject to Site 1 policies of the Downtown Fairview Character Area. Overall, the revised Site 1 policy (Policy 12.5.9.1) is encouraging and we support to removal of reference to land use designations. Modification is requested to provision d which identifies where the highest heights are to be situated. In our opinion, site design, massing and scale considerations should be evaluated during the development review process to facilitate a well-designed, appropriately massed and scaled development is provided.

We are also encouraged and support that the draft Amendment re-designates the Subject Lands to 'Residential High Density'. We support this re-designation.

Finally, we highlight that the draft Amendment seeks to implement Downtown Fairview, Cooksville and Hospital Built Form Standards ('Standards'). We understand that these Standards, which serve as urban design guidelines, are to further implement the proposed built form and Character Area policies of the Mississauga Official Plan. We are concerned that the draft Standards which provide detailed design guidelines for developments occurring in the Downtown Fairview, Cooksville and Hospital Character Areas are restrictive and do not facilitate the best utilization of land, resources and infrastructure nor the creation of Cooksville as a vibrant, complete, 15-minute community.

In summary, we are concerned about the proposed Amendment and request that certain modifications be considered. Thank you for the opportunity to provide these comments. Our Client wishes to be included in the engagement for the Downtown Fairview, Cooksville and Hospital Policy Review initiative and wishes to be informed of updates and future meetings.

We look forward to being involved. Please feel free to contact the undersigned if there are any questions.

Yours very truly,

**GLEN SCHNARR & ASSOCIATES INC.**

  
 Glen Broll, MCIP, RPP  
 Partner





# Downtown Fairview

12.3 Downtown Fairview Character Area, Mississauga Official Plan

Summary of Key Policy Changes

2022-02-02



## Vision

Downtown Fairview will be a distinct **walkable** community that provides a **transition** to lower heights and densities from the Downtown Core. With more shops and amenities along Hurontario Street, Downtown Fairview will be an attractive place where residents can access their daily needs within a short distance from their homes.

**Mixed use development** located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard will continue to serve the retail and commercial needs of the local community. The area along Mississauga Valley Boulevard will continue to be defined by existing towers in the park and **beautiful green spaces**, with improved connections to trails and to Cooksville Creek.

An **expanded pedestrian network** will connect new development with nearby amenities and residential areas, and improve access to transit, parks and open spaces.

002

## #001

Posted by X on **02/27/2022** at **5:21pm** [Comment ID: 39] - *Agree: 0, Disagree: 0*  
I agree and support more mixed use development, with more walkable shops, etc.

## #002

Posted by X on **02/27/2022** at **5:22pm** [Comment ID: 40] - *Type: Suggestion*  
*Agree: 0, Disagree: 0*

For those who drive, there should be vertical, accessible public parking in the area.



# Guiding Principles

## Attractive and Walkable



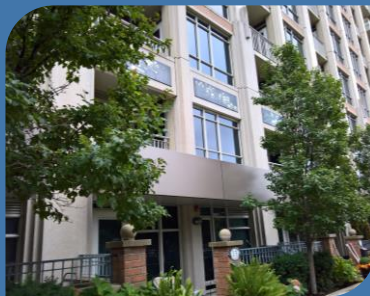
Accessible public spaces for all, with enhanced connections for walking and cycling

## Mix of Shops, Restaurants, Café and Service Establishments



An attractive, walkable environment along Hurontario Street with a vibrant mix of shops, restaurants, cafes and service establishments

## Mix of Housing Form and Tenure



A mix of housing forms and tenure with a range of housing options that meet the needs of a diverse community

## Transition in Building Height



Heights and densities that are appropriate to the planned context

- Greatest heights will be located at Hurontario Street and Elm Drive West
- Transition to low <sup>003</sup> building heights and densities for new developments south along Hurontario Street and east towards Mississauga Valley Boulevard

## #003

Posted by X on **02/27/2022** at **5:24pm** [Comment ID: 41] *Type: Suggestion*

*Agree: 0, Disagree: 0*

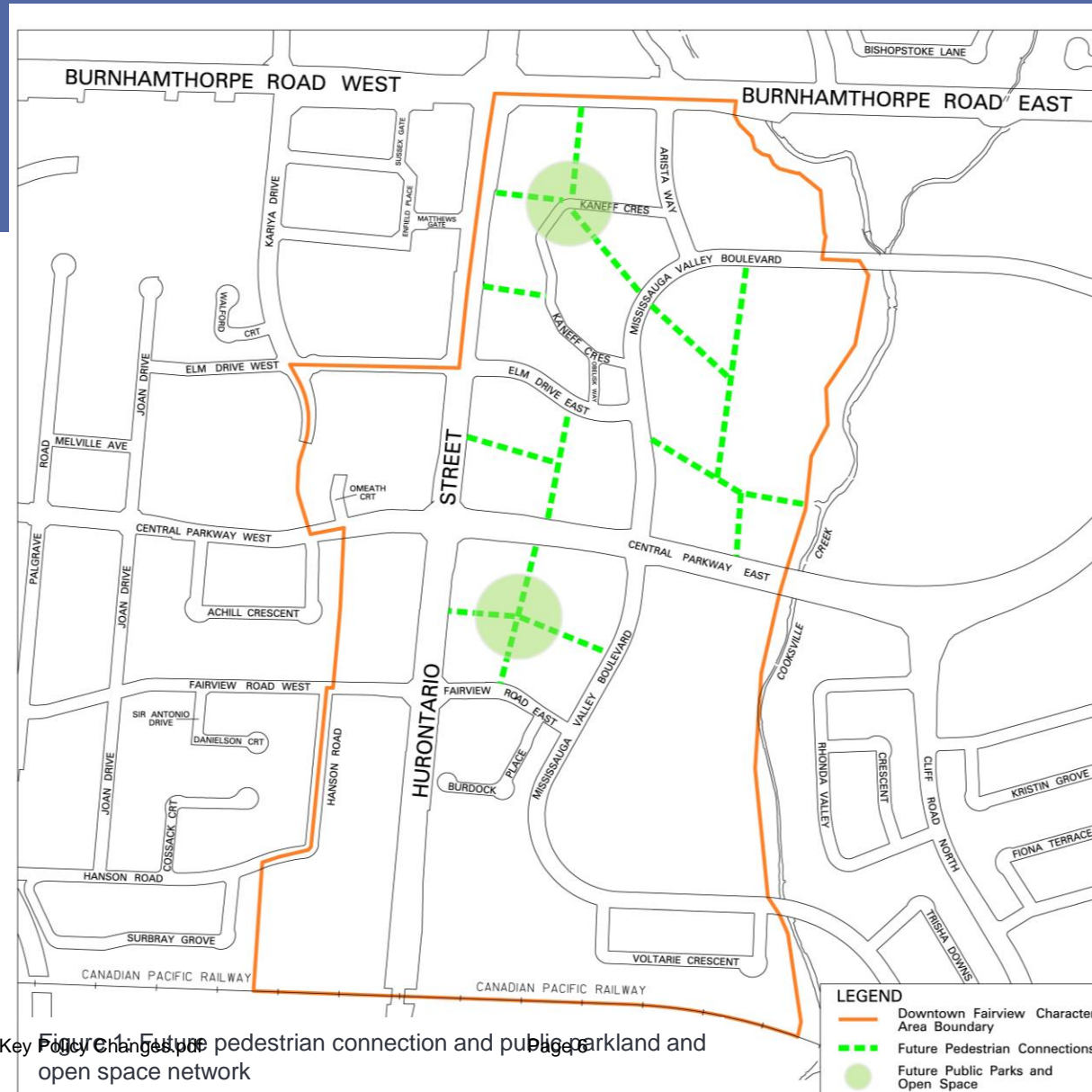
One of the most beautiful aspects of Mississauga is its horizon - the sunrise and sunsets in this area (i.e., driving down Hurontario) are breathtaking. Efforts should be made to support this continued unobstructed view.

# Future Public Parkland and Pedestrian Connections

## Network of Public Parkland and Open Space

- High quality and well-designed public parkland and open space that is green, safe and attractive
- Supports a range of social and recreational activities
- The City will determine the location, configuration and size of the parkland block(s) and requirement for land dedication
- Opportunity to obtain parkland through purchase by the City
- Opportunity to further increase public open space through new developments and public easements, where appropriate

Figure 1: Future pedestrian connection and public parkland and open space network



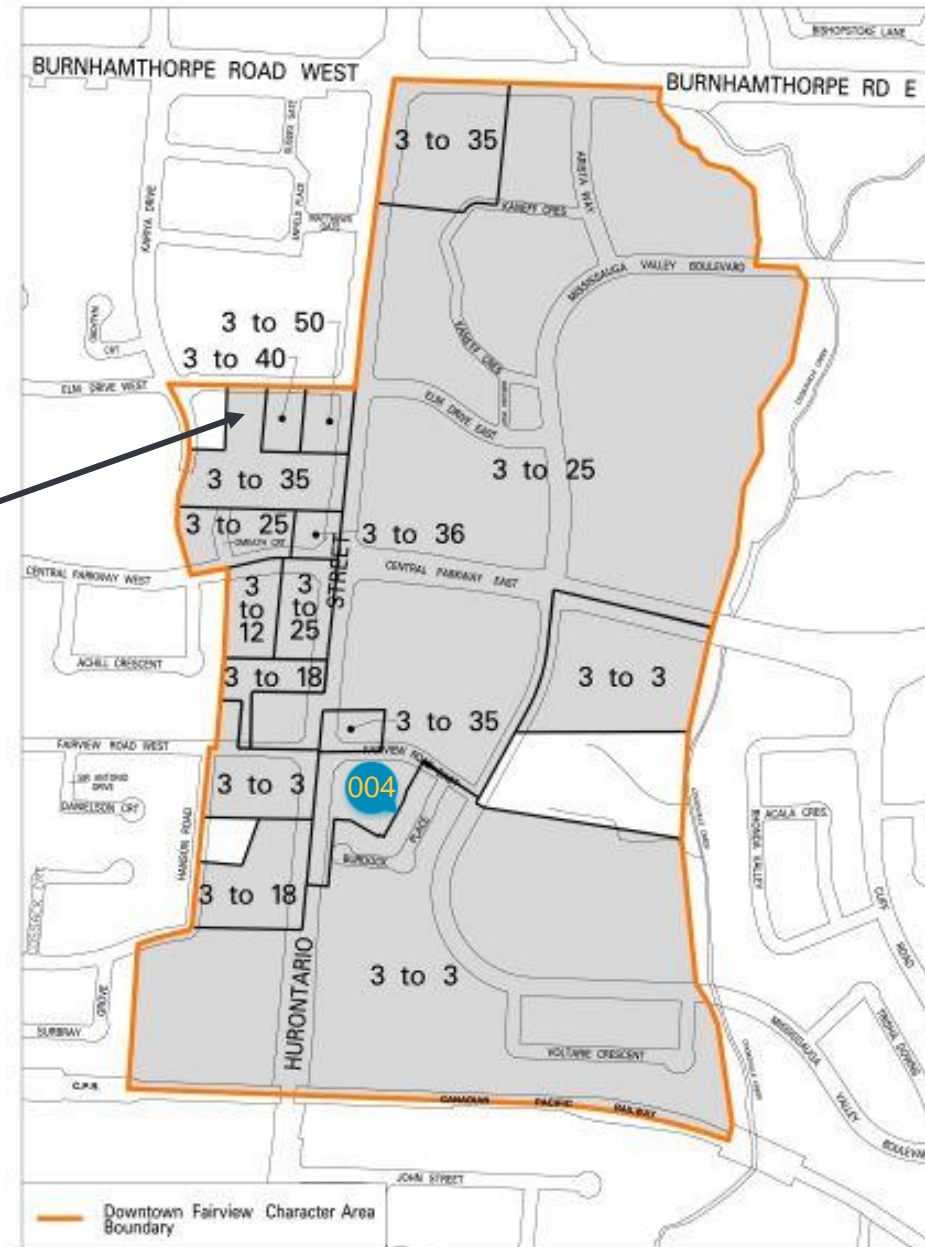
## Pedestrian Connections

- Pedestrian connections to Hurontario LRT, transit routes/stops, trails and parks and open spaces
- Public easements will be required where pedestrian connections are proposed on private lands



# Building Height

- Greatest heights located in the existing and planned buildings at Elm Drive West and Hurontario Street
- Building heights will not exceed the maximum limits as shown on Map 12-3.2



## #004

Posted by X on **02/16/2022** at **8:13pm** [Comment ID: 30] - *Type:*

*Question*

*Agree: 3, Disagree: 0*

There is no clear explanation on why the south east corner of Hurontario and Fariview has an extremely sharp transition from a high density building to an already established community. This area should be re-classified to 3 to 3 like already established buildings in the area.

## General Policies

### New Buildings:

- Transition in height from the property line adjacent to low and medium density residential areas
- Minimum separation distance, generally 30 metres for taller buildings
- Add visual interest by varying the use of massing and materials
- Create visual interest with the use of high quality materials and architectural detailing
- Variations in height where more than 1 tower is proposed (generally variations of 3-5 storeys)
- Tall buildings will incorporate podiums, between 3 – 6 storeys
- For tall buildings, the tower above the podium will have a limited floor plate size
- Development will incorporate appropriate setbacks between the edge of the podium and tower portion of the building

006

005



## #005

Posted by X on **02/27/2022** at **5:27pm** [Comment ID: 42] - *Type: Suggestion*

*Agree: 0, Disagree: 0*

The buildings should be built with different materials and styles (i.e., not the same tall, glass buildings that have proliferated the skyline of Mississauga thus far). To the extent possible, it should introduce interesting and diverse architecture in Mississauga

## #006

Posted by X on **03/15/2022** at **5:03pm** [Comment ID: 59] - *Type:*

*Suggestion*

*Agree: 0, Disagree: 0*

Height transition and shadows are a superficial concern. We need tighter regulations on build quality and less on built form.

# Urban Form

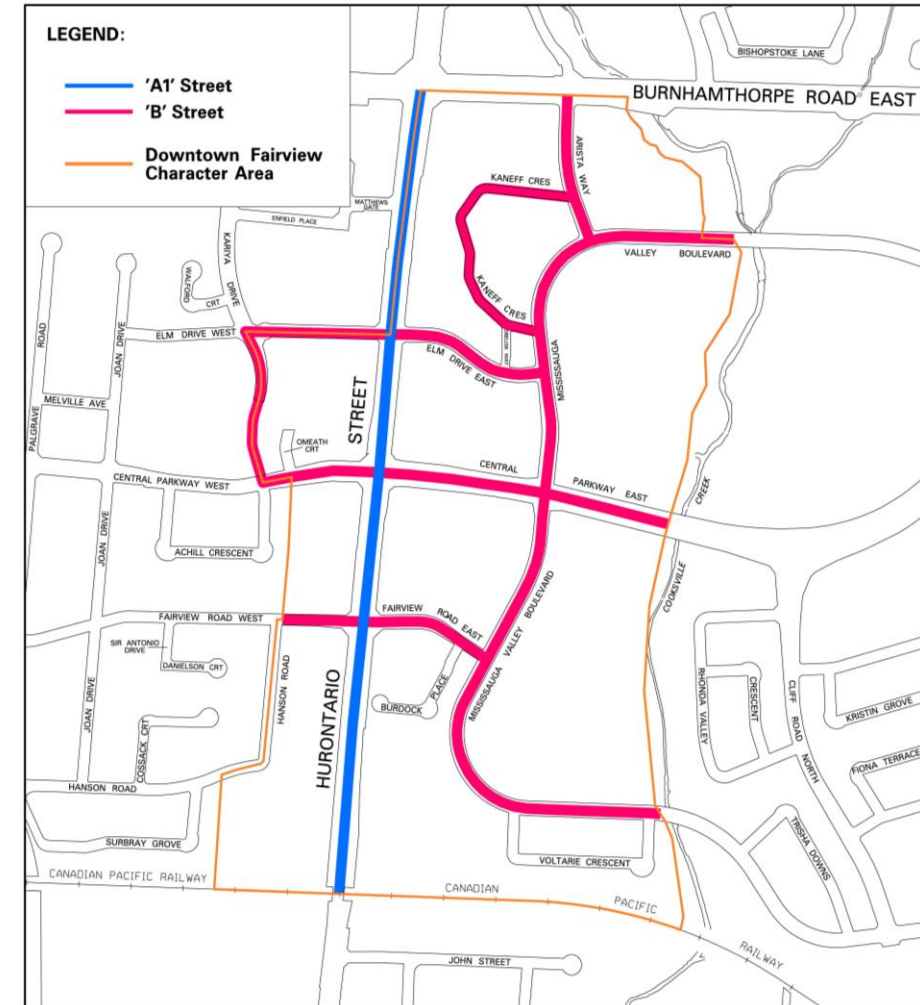
5.6

## Development along 'A' Streets:

- Promote a mainstreet character with smaller retail units
- Frame and animate streets and public spaces with storefronts and prominent entrances
- Consistent streetwall
- Periodic building indentations should be provided as relief to long building walls e.g., by integrating entry forecourts and plazas
- Design retail and service commercial units at street corner locations with animated storefronts that wrap the corner
- Setbacks from the street to accommodate e.g., landscaping, street furniture, wayfinding, pedestrian scaled lighting and outdoor patios
- Integrated design character between private land and the public boulevard

## Development along 'B' Streets:

- Consistent streetwall
- Periodic building indentations should be provided as relief to long building walls e.g., by integrating entry forecourts and plazas
- Generous setbacks from the street to accommodate e.g., terraces and landscaping
- High quality seamless interface and transition between the public sidewalk and principle building entrance to ensure adequate separation, definition and privacy



Map 12-3.3 Downtown Fairview Street Types

### Street Types

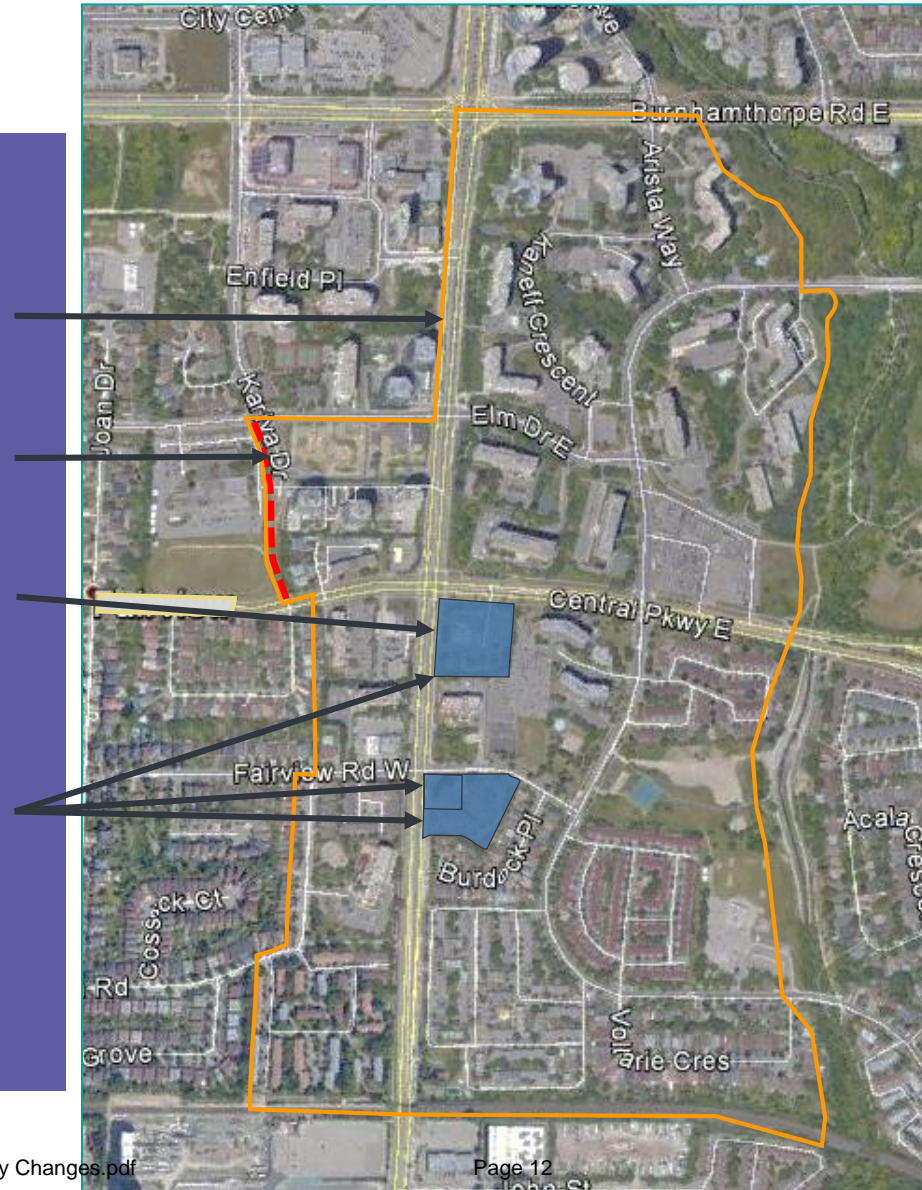
'A' Streets are considered primary arteries with buildings that integrate at-grade retail and service commercial uses.

'B' Streets will be primarily residential in character.



# Land Use

- New development along Hurontario Street will require at grade retail and service commercial uses
- Addition of Kariya Drive as a minor collector and future minor collector road to the long term road network
- New Exempt Site policy to also permit motor vehicle commercial use on the southeast corner of Hurontario Street and Central Parkway East
- Re-designate lands from Motor Vehicle Commercial and Convenience Commercial to Mixed Use



## Mixed Use

- Redevelopment that results in a loss of office, retail and service commercial floor space will not be permitted
- The planned function of the non-residential component will be maintained or replaced as part of the redevelopment

## Office

- Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office is retained or replaced through redevelopment





# Downtown Cooksville

12.4 Downtown Cooksville Character Area, Mississauga Official Plan

Summary of Key Policy Changes

2022-02-02



## Vision

Downtown Cooksville will be a **walkable, mixed use** community with interesting places for culture and art. Hurontario and Dundas streets will <sup>007</sup> be **animated** with storefronts and other active uses that support local businesses.

The area immediately surrounding the intersection of Hurontario and Dundas (Four Corners) will be developed with a **vibrant mainstreet focus** that includes a mix of uses and a **human-scaled** built form. The Cooksville GO Station area will be a focal point for **transit oriented** development with a concentration and mix of residential, community, office, retail and service commercial uses.

#007

Posted by X on **03/15/2022** at **5:01pm** [Comment ID: 58] - *Type:*

*Suggestion*

*Agree: 0, Disagree: 0*

Please eliminate parking minimums to help reduce housing prices and encourage pedestrian and transit oriented communities.



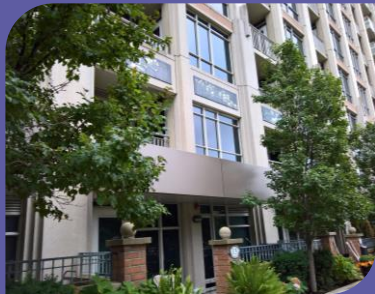
# Guiding Principles

## Create a Vibrant, Mixed Use Community



A mix of local and unique businesses along Hurontario and Dundas streets, with spaces for educational, cultural and recreational uses, a vibrant public realm, and a network of connected parks and open spaces

## Plan for More Housing and People



Provide new housing and jobs with the greatest heights and densities located at the Cookville GO Station

## Achieve a Walkable, Connected Community



A pedestrian-oriented mainstreet character along Hurontario and Dundas streets, improved pedestrian and cycling infrastructure, public art and public realm

## Plan for High Quality Transit

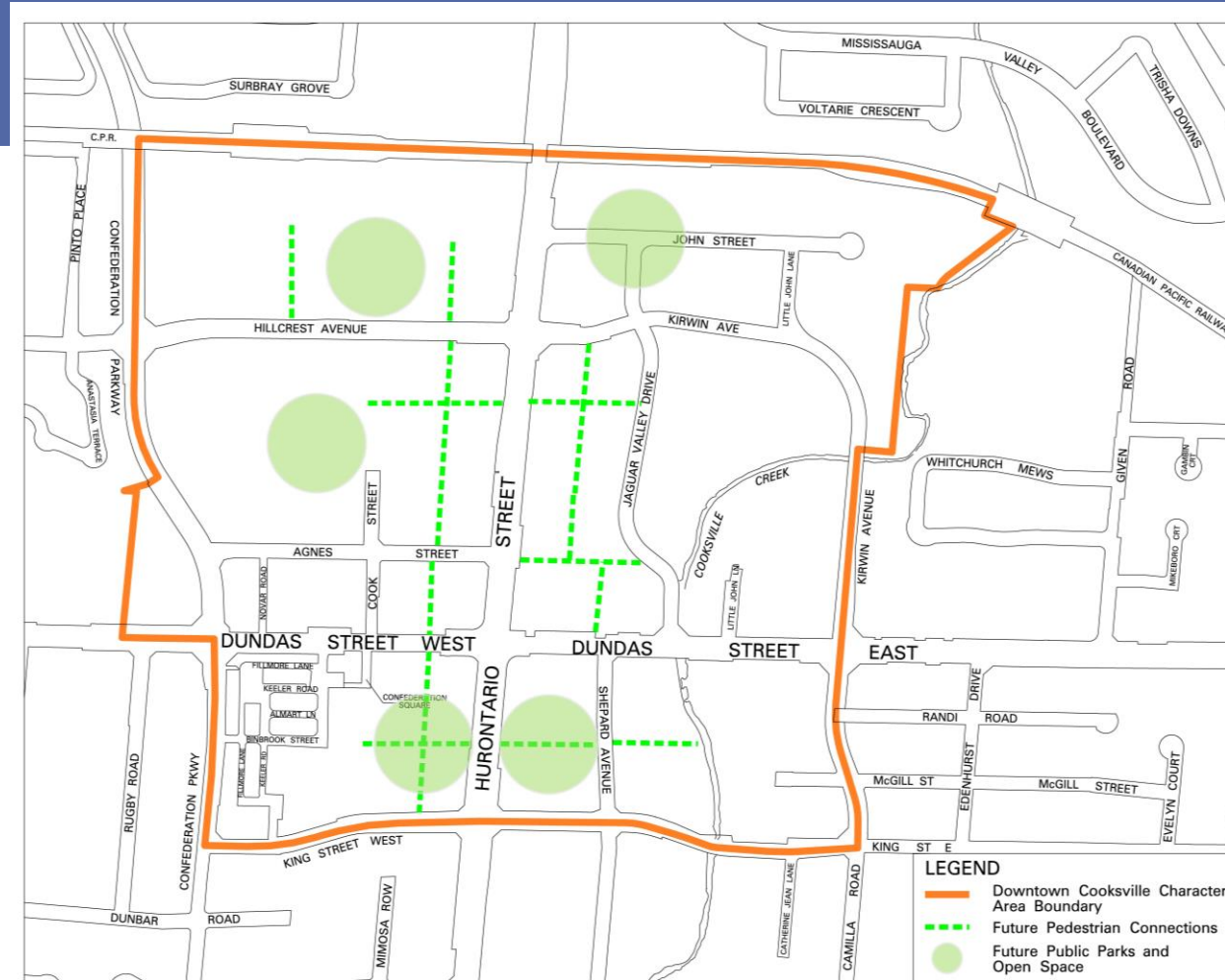


Provide convenient and efficient transit with seamless connections to local and higher-order transit service

# Future Public Parkland and Pedestrian Connections

## Network of Public Parkland and Open Space

- High quality and well-designed public parkland and open space that is green, safe and attractive
- Supports a range of social and recreational activities
- The City will determine the location, configuration and size of the parkland block(s) and requirement for land dedication
- Opportunity to obtain parkland through purchase by the City
- Opportunity to further increase public open space through new developments and public easements, where appropriate



## Pedestrian Connections

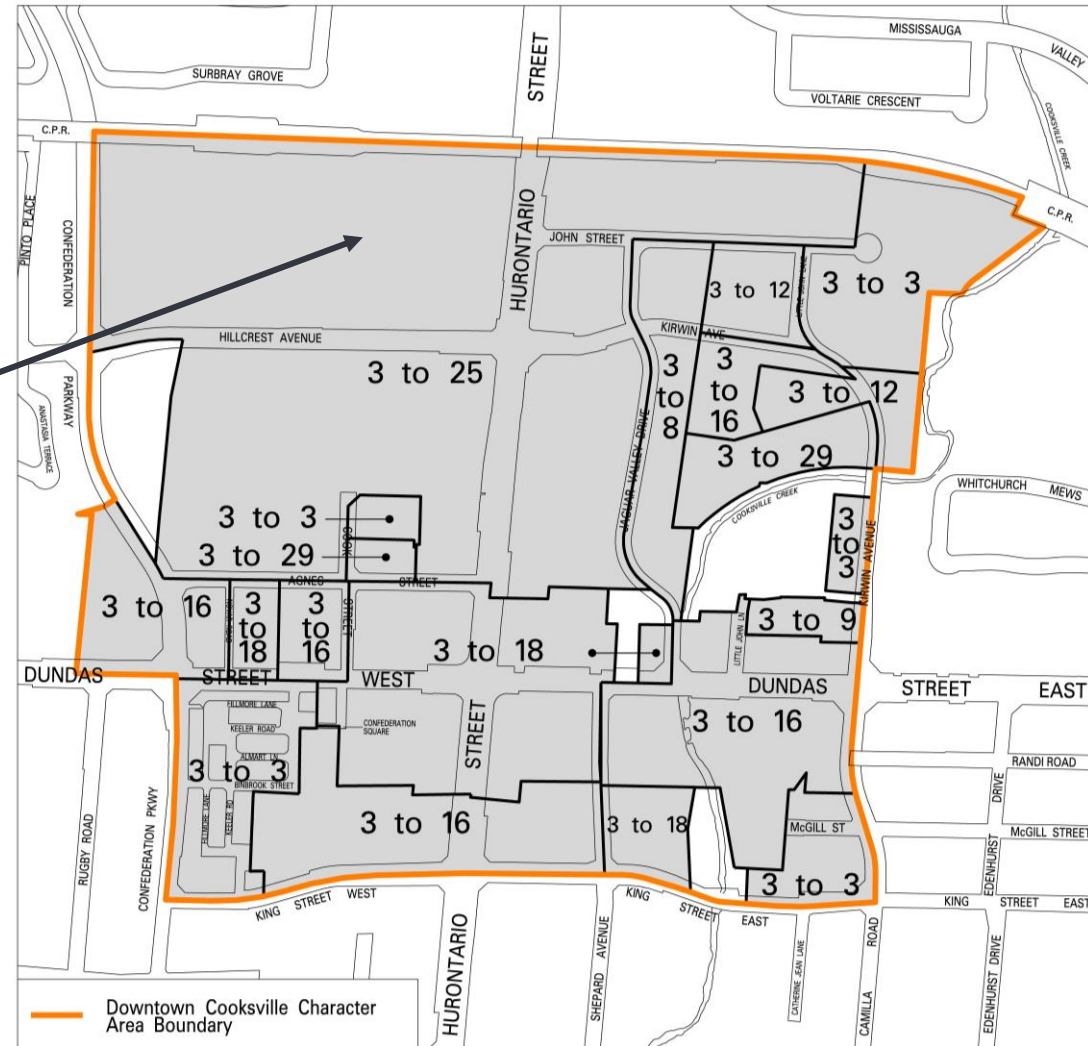
- Pedestrian connections to Hurontario LRT, transit routes/stops, trails and parks and open spaces
- Public easements will be required where pedestrian connections are proposed on private lands

Figure 1: Future pedestrian connection and public parkland and open space network



# Building Height

- Greatest heights located at Cooksville GO Station and along Hurontario north of Agnes Street
- Building heights will not exceed the maximum limits as shown on Map 12-4.2



## General Policies

### New Buildings:

- Transition in height from the property line adjacent to low and medium density residential areas
- Minimum separation distance, generally 30 metres for taller buildings
- Add visual interest by varying the use of massing and materials
- Create visual interest with the use of high quality materials and architectural detailing
- Variations in height where more than 1 tower is proposed (generally variations of 3-5 storeys)
- Tall buildings will incorporate podiums, between 3 – 6 storeys
- For tall buildings, the tower above the podium will have a limited floor plate size
- Development will incorporate appropriate setbacks between the edge of the podium and tower portion of the building



# Urban Form

5.6

## Development along 'A' Streets:

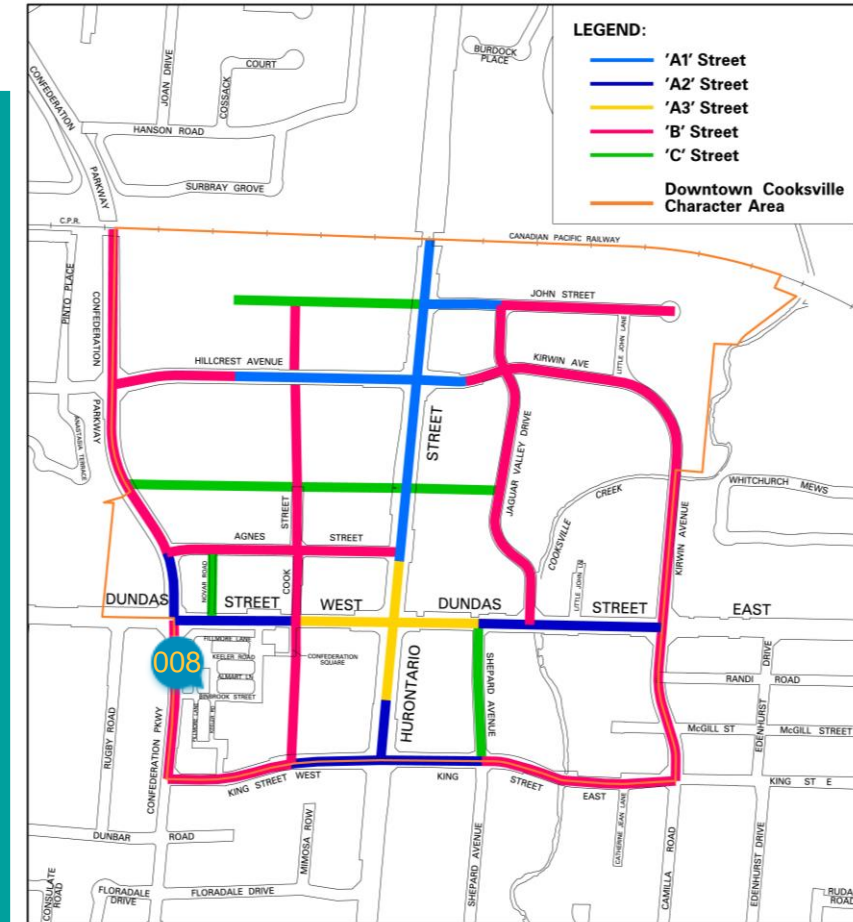
- Promote a mainstreet character with smaller retail units
- Frame and animate streets and public spaces with storefronts and prominent entrances
- Consistent streetwall
- Periodic building indentations should be provided as relief to long building walls e.g., by integrating entry forecourts and plazas
- Design retail and service commercial units at street corner locations with animated storefronts that wrap the corner
- Setbacks from the street to accommodate e.g., landscaping, street furniture, wayfinding, pedestrian scaled lighting and outdoor patios
- Integrated design character between private land and the public boulevard

## Development along 'B' Streets:

- Consistent streetwall
- Periodic building indentations should be provided as relief to long building walls e.g., by integrating entry forecourts and plazas
- Generous setbacks from the street to accommodate e.g., terraces and landscaping
- High quality seamless interface and transition between the public sidewalk and principle building entrance to ensure adequate separation, definition and privacy

## Development along 'C' Streets:

- Consolidate service, parking and loading to serve a number of buildings
- Minimize visual impact of the parking, loading and service areas from the street
- Provide a secondary entrance for pedestrian access, where appropriate



Map 12-4.3 Downtown Cooksville Street Types

### Street Types

'A' Streets are considered primary arteries with buildings that integrate at-grade retail and service commercial uses.

'B' Streets will be primarily residential in character.

'C' Streets are intended to support a pedestrian environment and will also provide for vehicular access to on-site service, loading, parking and garbage storage areas.

Printed 03/21/2022

## #008

Posted by X on **02/17/2022** at **1:51pm** [Comment ID: 31] - *Type: Question*

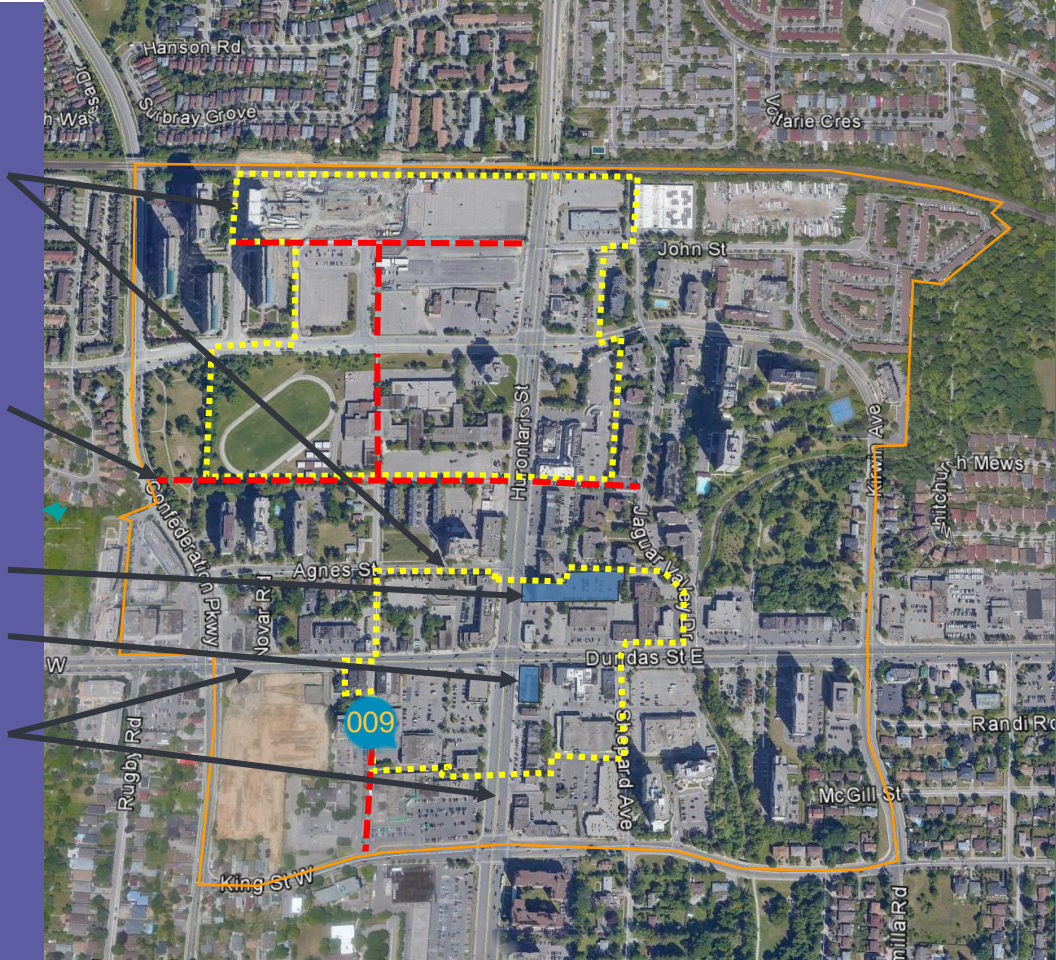
*Agree: 0, Disagree: 0*

Why isn't this section of Confederation Parkway an "A" street? There is a commercial plaza on the southwest corner of Confederation Pkwy and Dundas, and it's my understanding that ground level retail is planned for the townhouses on the east side of Confederation Pkwy (if they ever get built), so it's not clear why this section is categorized as a "B" street.



# Land Use

- Greater concentration and mix of uses – 3 floors of non-residential uses near 2 key locations:
  - Cooksville GO Station
  - Hurontario and Dundas Street intersection
- Complete the street network with new streets and extensions of existing streets
- Re-designate parcel with existing office building from Mixed Use to Office
- Re-designate to Public Open Space
- New development along Hurontario and Dundas Streets will require at grade retail and service commercial uses



## Mixed Use

- Redevelopment that results in a loss of office, retail and service commercial floor space will not be permitted
- The planned function of the non-residential component will be maintained or replaced as part of the redevelopment

## Office

- Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office is retained or replaced through redevelopment



#009

Posted by X on **02/05/2022** at **7:41pm** [Comment ID: 20] - *Type:*

*Question*

*Agree: 0, Disagree: 0*

What is the advantage of extending cook street?

Won't it just create more intersections and increase gridlock?

There doesn't seem to be much space for a vehicle road unless buildings are knocked down or it's a 1 lane street.



# Downtown Hospital

12.5 Downtown Hospital Character Area, Mississauga Official Plan

Summary of Key Policy Changes

2022-02-02



# Vision

Downtown Hospital will be developed as a **health district**, anchored by the hospital with a broad range of uses **clustered** around the Trillium Mississauga Hospital, supporting more retail and service commercial uses, research and development, health facilities, offices, parks and residential uses.

**Transit-oriented development** along Hurontario and Queensway will ensure transit investments are supported with greater access to housing, jobs and services.

# Guiding Principles

## Support Health Care Services



A range of health care services, supportive uses and research and development

## Walkable and Transit-oriented



Additional pedestrian connections to Hurontario Street and Queensway, and street related retail and service commercial uses along these corridors



# Future Public Parkland and Pedestrian Connections

## Network of Public Parkland and Open Space

- High quality and well-designed public parkland and open space that is green, safe and attractive
- Supports a range of social and recreational activities
- The City will determine the location, configuration and size of the parkland block(s) and requirement for land dedication
- Opportunity to obtain parkland through purchase by the City
- Opportunity to further increase public open space through new developments and public easements, where appropriate

## Pedestrian Connections

- Pedestrian connections to Hurontario LRT, transit routes/stops, trails and parks and open spaces
- Public easements will be required where pedestrian connections are proposed on private lands

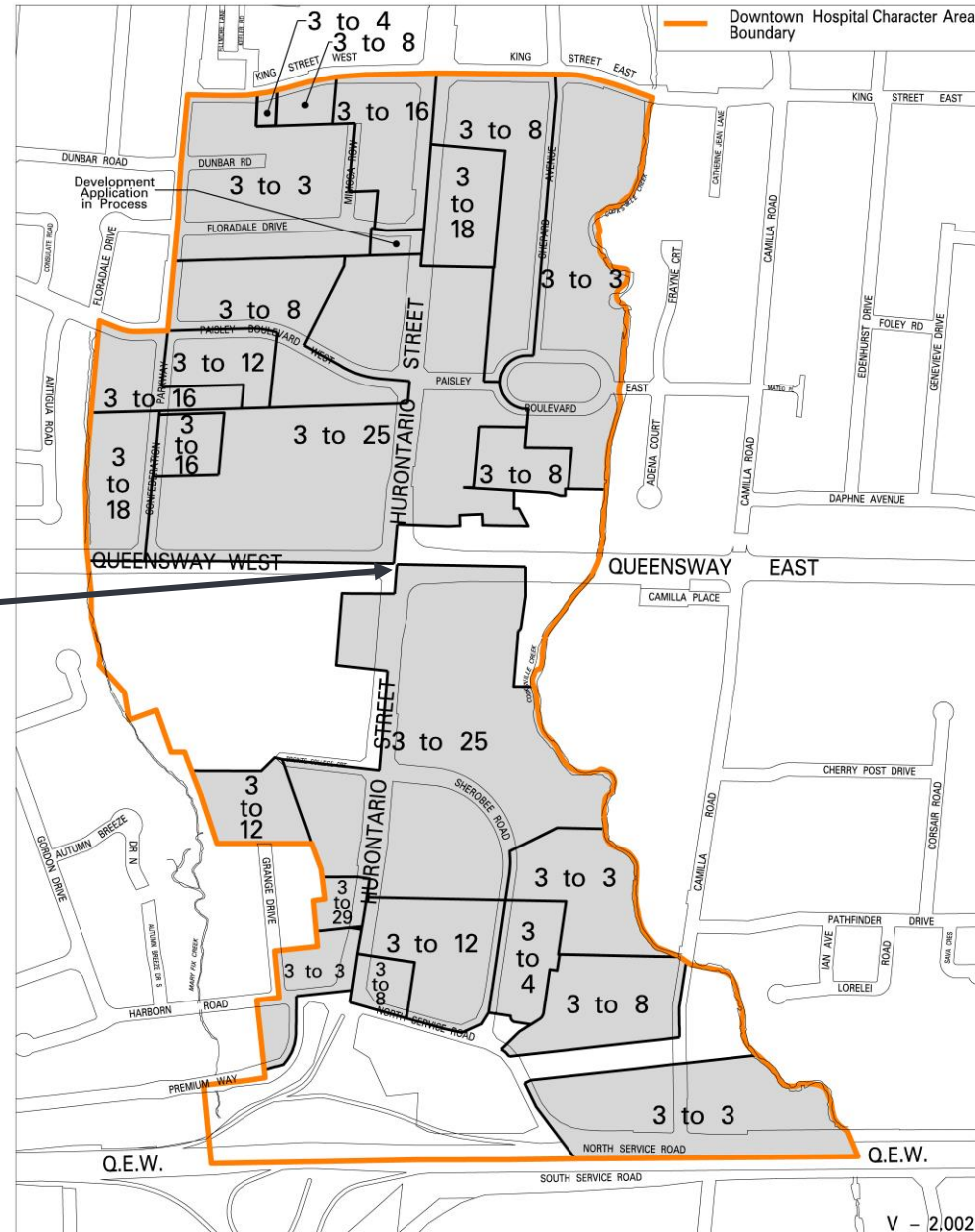


Figure 1: Future pedestrian connection and public parkland and open space network



# Building Height

- Greatest heights located at the rapid transit stop at Queensway and Hurontario Street
- Building heights will not exceed the maximum limits as shown on Map 12-5.2



## General Policies

### New Buildings:

- Transition in height from the property line adjacent to low and medium density residential areas
- Minimum separation distance, generally 30 metres for taller buildings
- Add visual interest by varying the use of massing and materials
- Create visual interest with the use of high quality materials and architectural detailing
- Variations in height where more than 1 tower is proposed (generally variations of 3-5 storeys)
- Tall buildings will incorporate podiums, between 3 – 6 storeys
- For tall buildings, the tower above the podium will have a limited floor plate size
- Development will incorporate appropriate setbacks between the edge of the podium and tower portion of the building



# Urban Form

5.6

## Development along 'A' Streets:

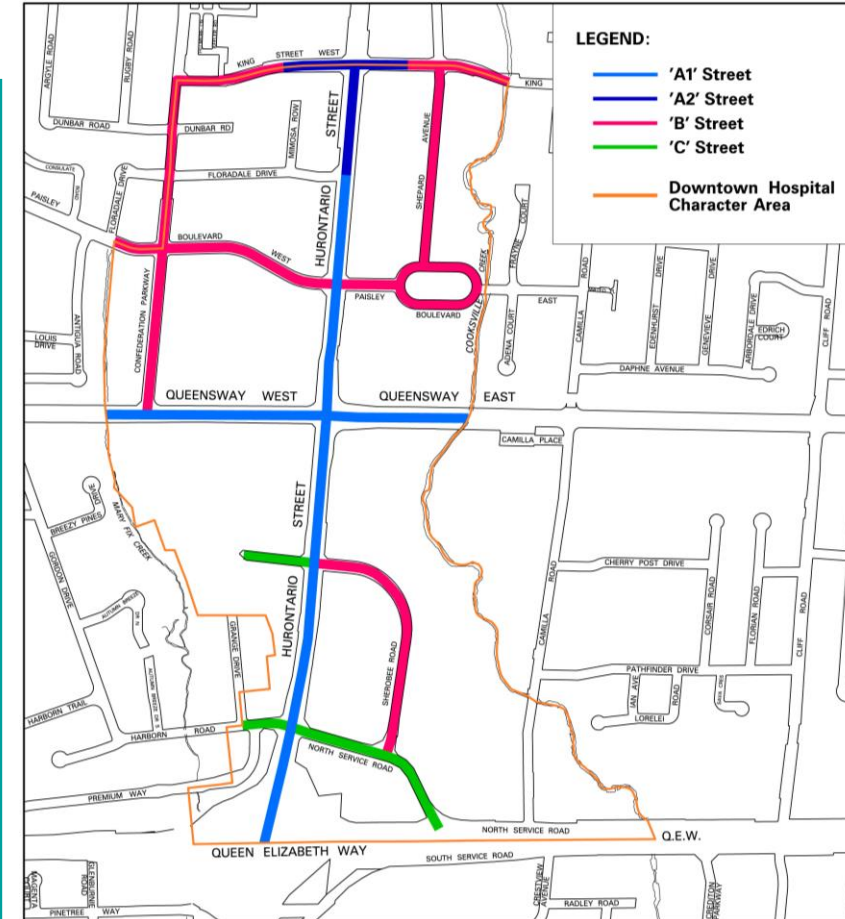
- Promote a mainstreet character with smaller retail units
- Frame and animate streets and public spaces with storefronts and prominent entrances
- Consistent streetwall
- Periodic building indentations should be provided as relief to long building walls e.g., by integrating entry forecourts and plazas
- Design retail and service commercial units at street corner locations with animated storefronts that wrap the corner
- Setbacks from the street to accommodate e.g., landscaping, street furniture, wayfinding, pedestrian scaled lighting and outdoor patios
- Integrated design character between private land and the public boulevard

## Development along 'B' Streets:

- Consistent streetwall
- Periodic building indentations should be provided as relief to long building walls e.g., by integrating entry forecourts and plazas
- Generous setbacks from the street to accommodate e.g., terraces and landscaping
- High quality seamless interface and transition between the public sidewalk and principle building entrance to ensure adequate separation, definition and privacy

## Development along 'C' Streets:

- Consolidate service, parking and loading to serve a number of buildings
- Minimize visual impact of the parking, loading and service areas from the street
- Provide a secondary entrance for pedestrian access, where appropriate



Map 12-5.3 Downtown Hospital Street Types

### Street Types

'A' Streets are considered primary arteries with buildings that integrate at-grade retail and service commercial uses.

'B' Streets will be primarily residential in character.

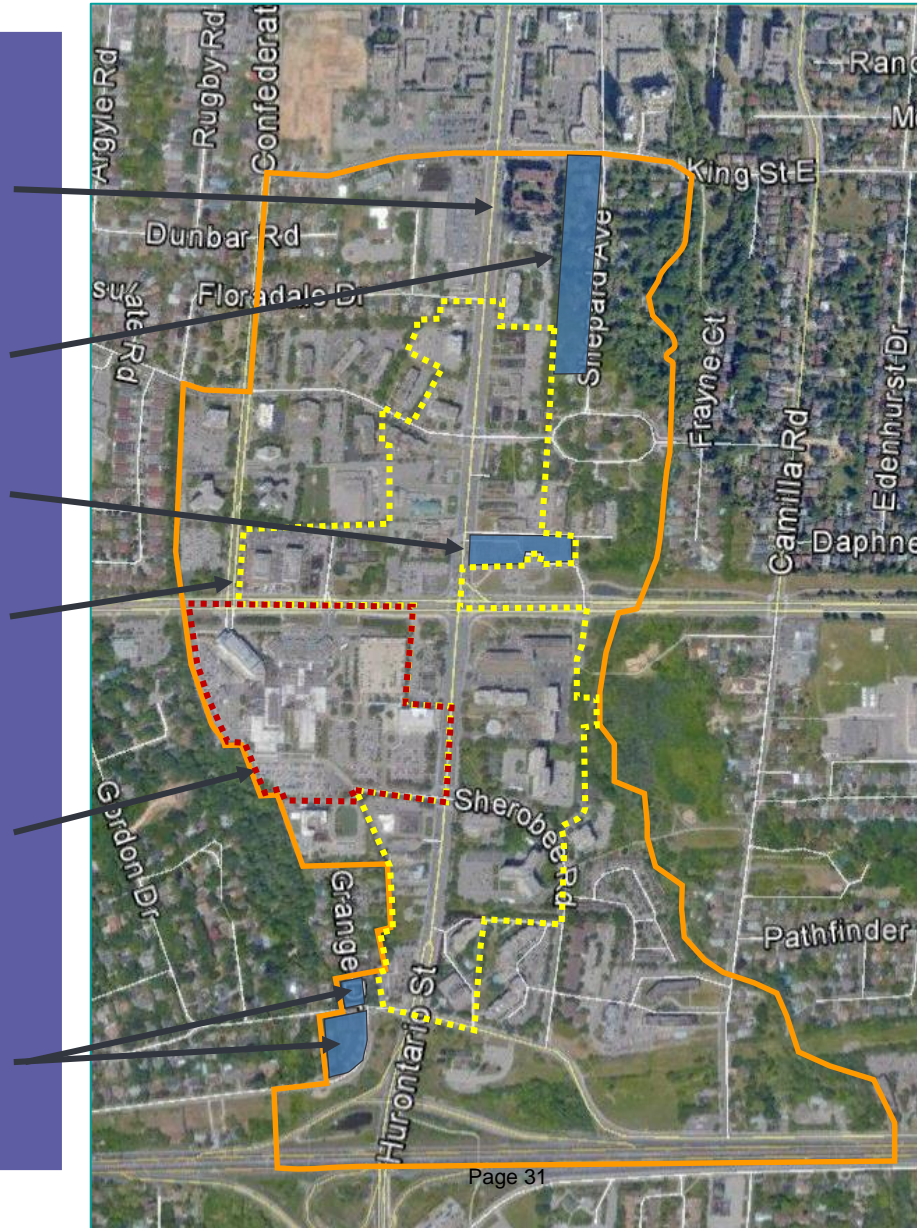
'C' Streets are intended to support a pedestrian environment and will also provide for vehicular access to on-site service, loading, parking and garbage storage areas.

Printed 03/21/2022



# Land Use

- At grade retail and service commercial uses for new development along Hurontario Street, King Street (new Cook Street and Hurontario Street) and Queensway
- Re-designate lands from Residential Medium Density to Residential High Density
- Re-designate lands from Convenience Commercial to Mixed Use
- New Special Site policy for a greater concentration and mix of uses - a minimum of 3 floors of non-residential uses adjacent to Hurontario Street and Queensway West
- New Special Site policy that permits a range of other uses for the Mississauga Hospital (e.g., conference centre, special needs housing, overnight accommodation, offices, restaurants)
- Re-designate lands from Residential Low Density II to Residential Medium Density



## Mixed Use

- Redevelopment that results in a loss of office, retail and service commercial floor space will not be permitted
- The planned function of the non-residential component will be maintained or replaced as part of the redevelopment

## Office

- Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office is retained or replaced through redevelopment

# Summary of Downtown Fairview, Cooksville and Hospital Built Form Standards

Date: 2022 -02-02



# Downtown Fairview, Cooksville and Hospital Built Form Standards

**Built Form Standards complement the Official Plan policies. They also express the City's expectations for good design, and the importance of a high quality built form and public realm in creating vibrant, walkable communities.**

## General Standards

**General architectural and built form urban design standards that apply to Downtown Fairview, Cooksville and Hospital and provide detail and direction on what is expected from the built form of new development.**

## Character Areas & Street Types

**Categorizes streets in each of the areas by types that correspond to the uses and character of the areas. Additional standards for new buildings are provided depending on which street the building fronts.**

## General Built Form Standards

### Ground Floor Setbacks

Ground floor setbacks vary depending on the required uses on the ground floor.

### Podium Stepback

Podium heights will be between 3 to 6 storeys with a minimum 3 - 6 metre stepback between the podium face and tower, depending on the site context.

### Floor Plate Sizes

Floor plate size will be limited depending on the height of the building

### Tower Separation

Towers will be separated by at least 30metres.

### Building Articulation

The podium levels must be designed to appear to be broken down into multiple parts and varying the massing and materials.

### Building Top

The top levels of the building should be stepped back to create visual relief at the top of the building.

### Height Variation

In a development with multiple towers, no two towers can be the same height.

### Main Street Character

Design buildings to improve the overall pedestrian experience such as with streetwall continuity, facade compositions, etc.

### Choice of Materials

Construct building exteriors from durable, natural material and deploy a hierarchy of materials.

### Building Corners and Sides

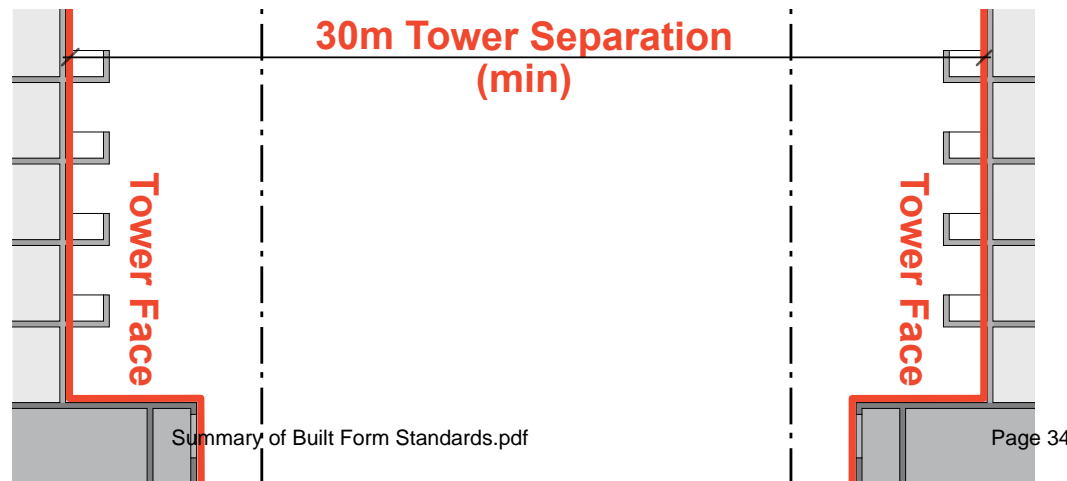
Design building located at prominent corners with upgraded elevations and design features to create a focal point.

### Parking Structure

In general, development will locate structured parking and vehicular access to minimize impacts on the property and on surrounding properties.

### Service, Parking and Loading

Service, parking and loading should be coordinated on sites by providing consolidated locations.



Tower separations and setbacks are measured from the front edge of the tower or podium face. Tower separation distance should be at least 30m.

# Character Areas & Street Types

5.6

## A-Streets

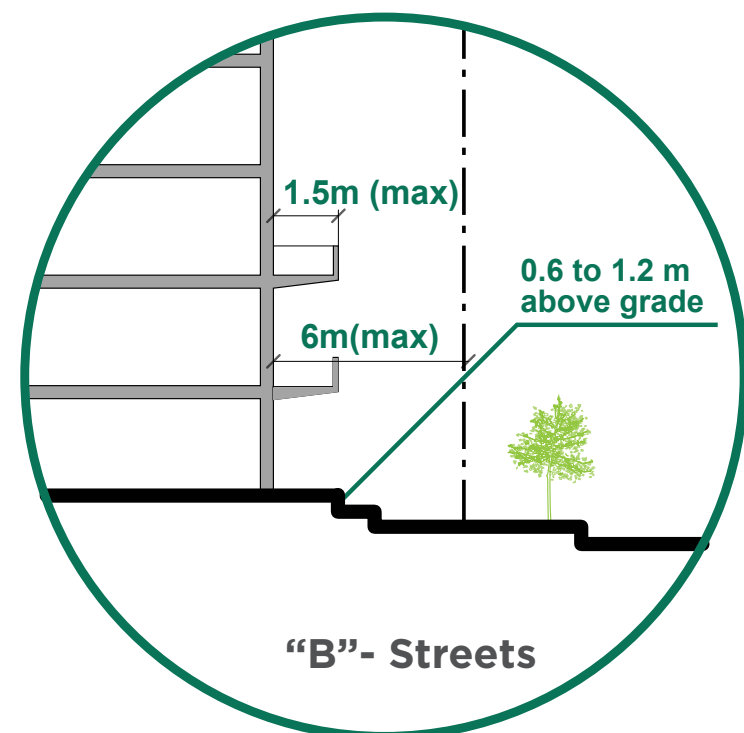
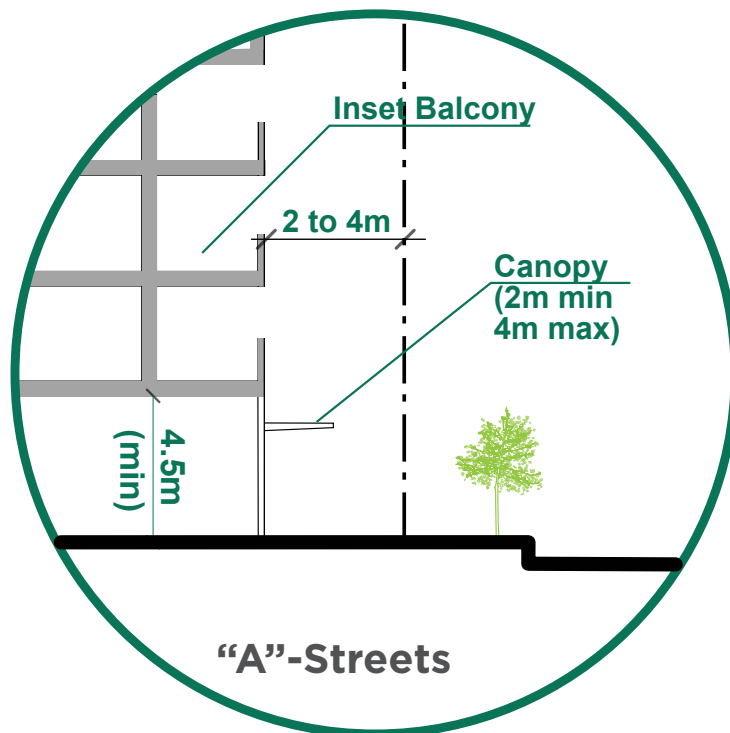
A-Streets are considered to be primary commercial arteries and must incorporate at grade commercial and/or retail uses at grade. The design of A-Streets is critical to develop a lively urban environment that fosters active uses and a pleasant pedestrian environment.

## B-Streets

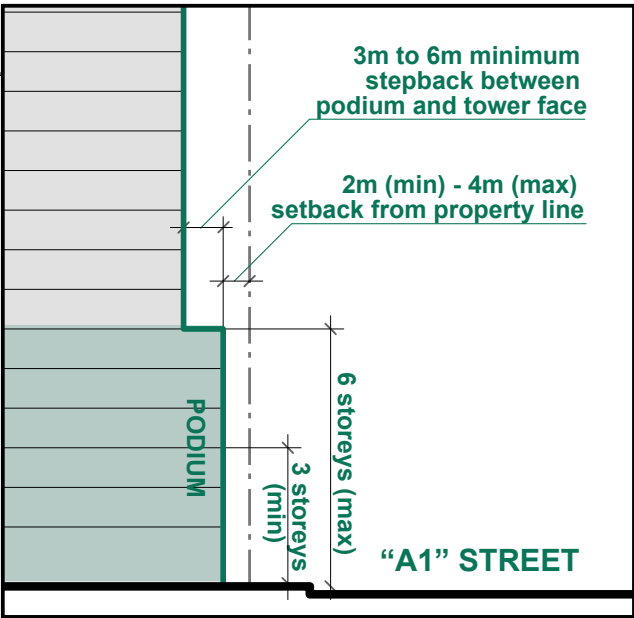
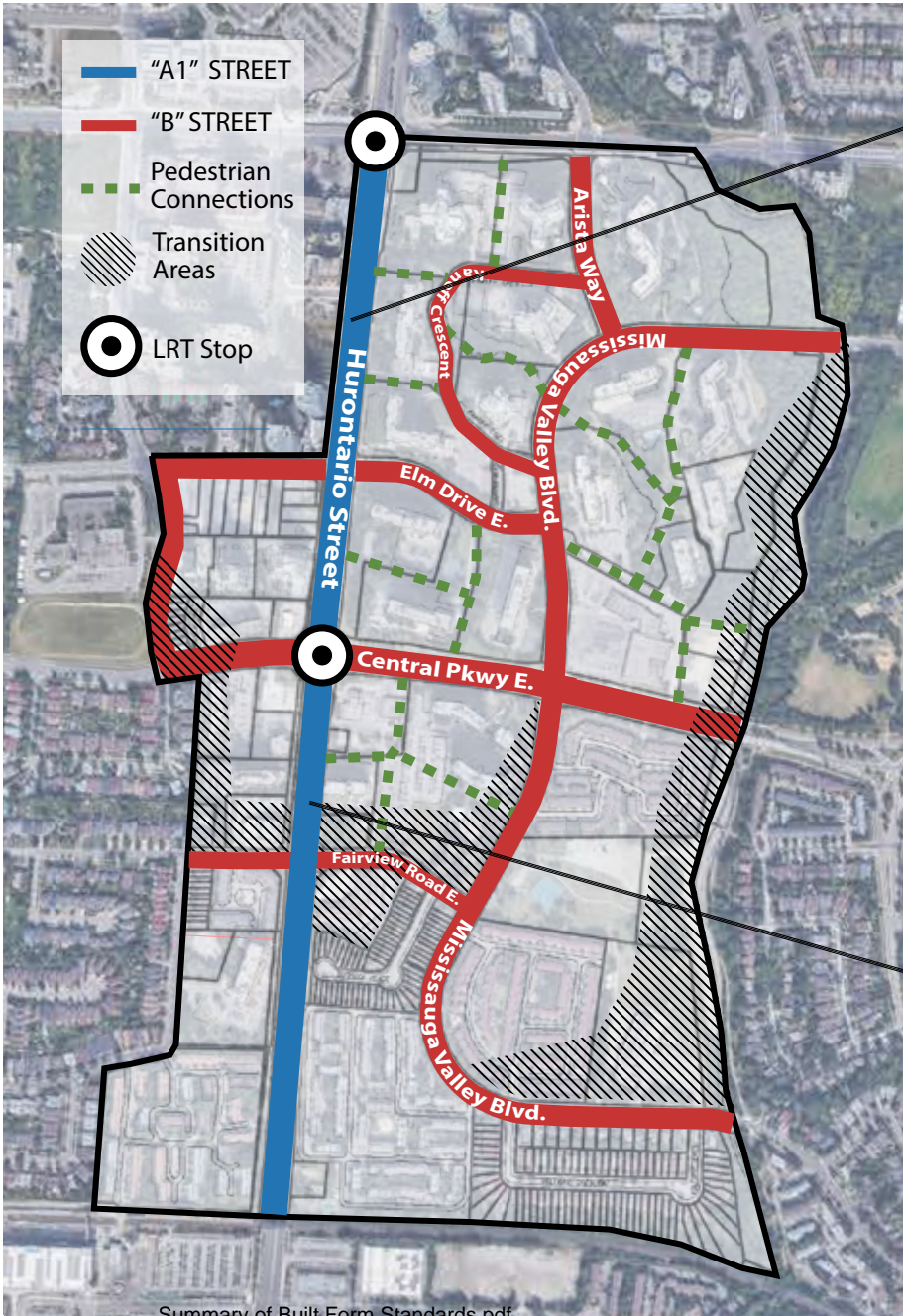
B-Streets are secondary streets and are more commonly residential in nature. When the ground floor does not have retail uses, entrances should be raised from the sidewalk and greater setbacks and landscaping should be designed to create a buffer between the public and the private realms and maintain the residential characteristic of these streets.

## C-Streets

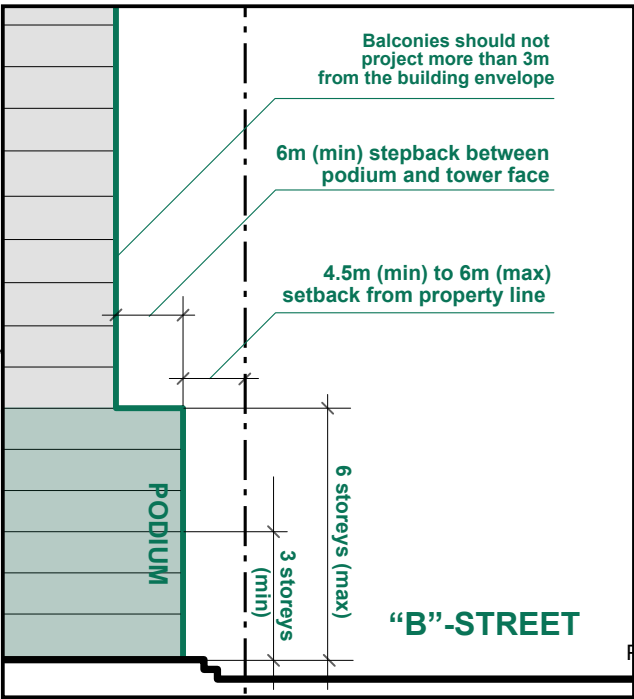
C-Streets are tertiary streets that connect A and B Streets. In contrast to 'A' and 'B' Streets, C-Streets provide development blocks with access for deliveries, garbage pick-up, service and loading, including vehicular access to structured and off-street parking within development sites. 'C' Streets are also intended to support a pedestrian environment by integrating a high standard of urban design to support street activity.



# Downtown Fairview Street Types



Frontage on an "A1" Street with the required podium height, setbacks and stepback.



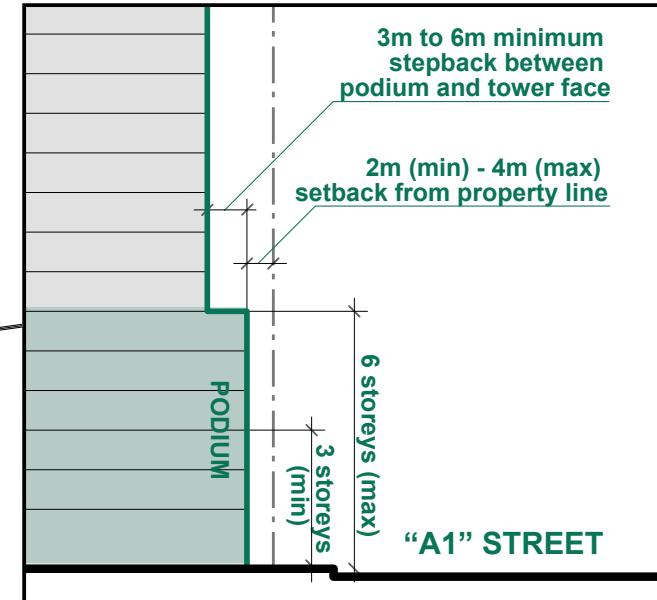
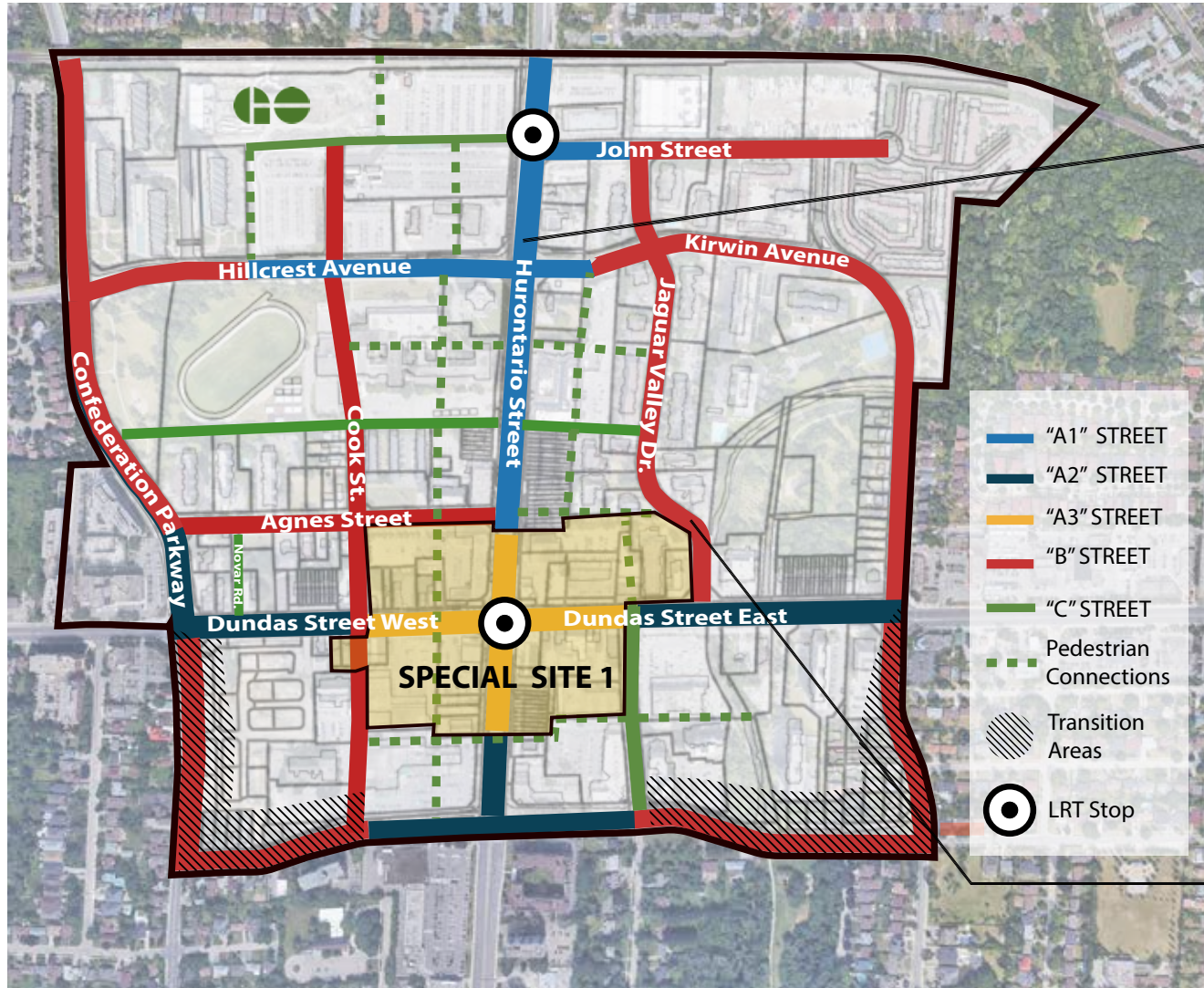
Frontage on a "B" Street with the required podium height, setbacks and stepback.



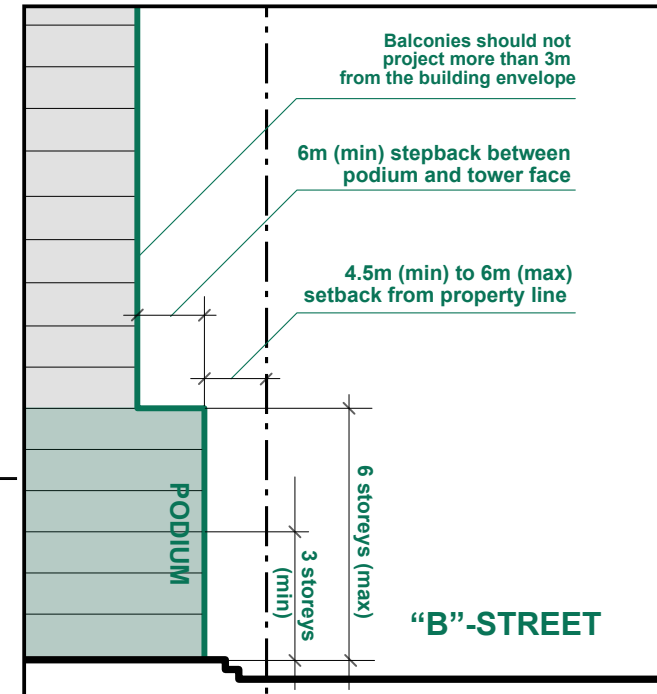
# Downtown Cooksville Street Types

5.6

(Continued on next page)



Frontage on an "A1" Street with the required podium height, setbacks and stepback.



Frontage on a "B" Street with the required podium height, setbacks and stepback.

Printed 03/21/2022

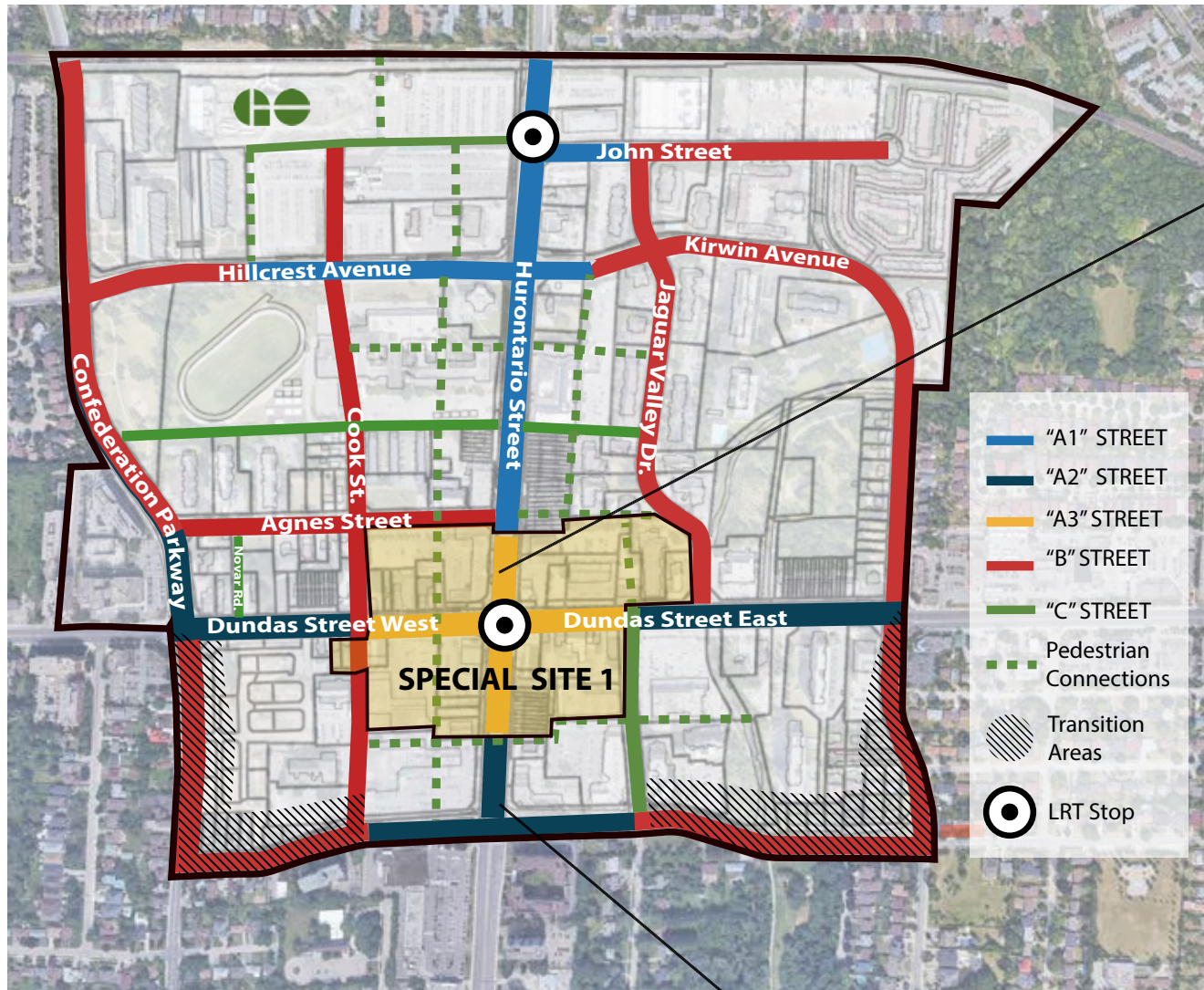
Street Types in the Downtown Cooksville Character Area



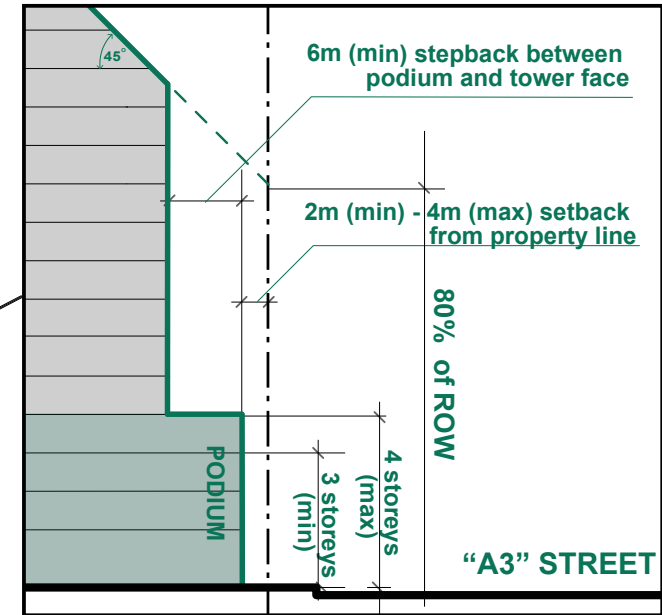
# Downtown Cooksville Street Types

5.6

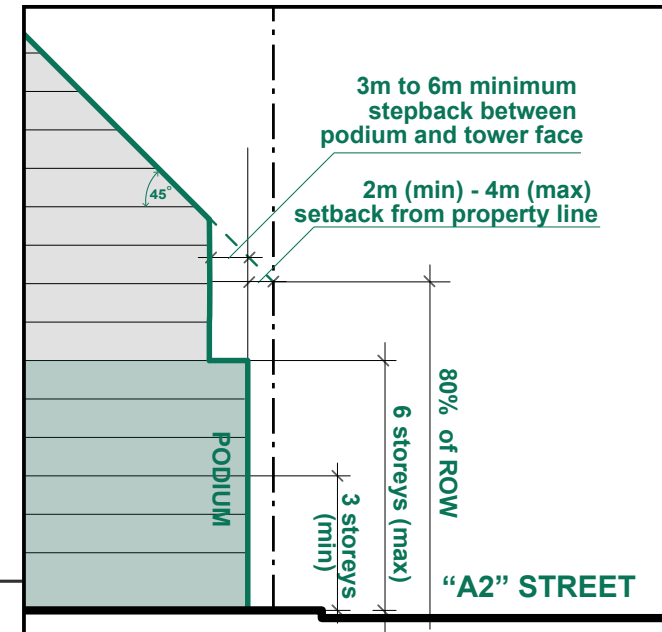
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Street Types in the Downtown Cooksville Character Area



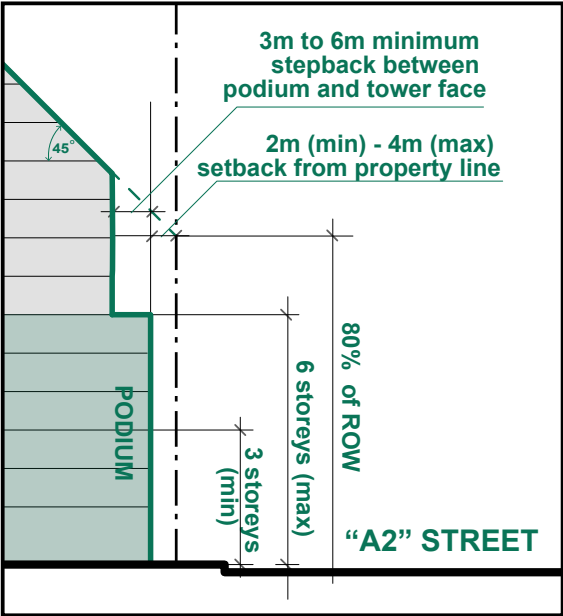
Frontage on an "A3" Street with the required podium height, setbacks and stepback.



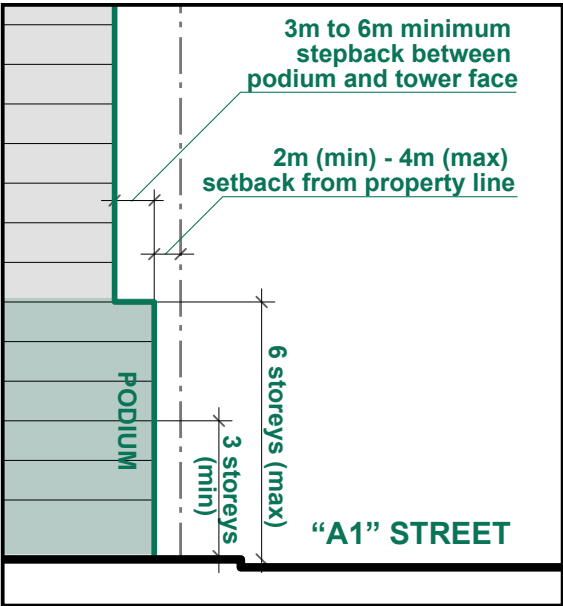
Frontage on an "A2" Street with the required podium height, setbacks and stepback.

Printed 03/21/2022

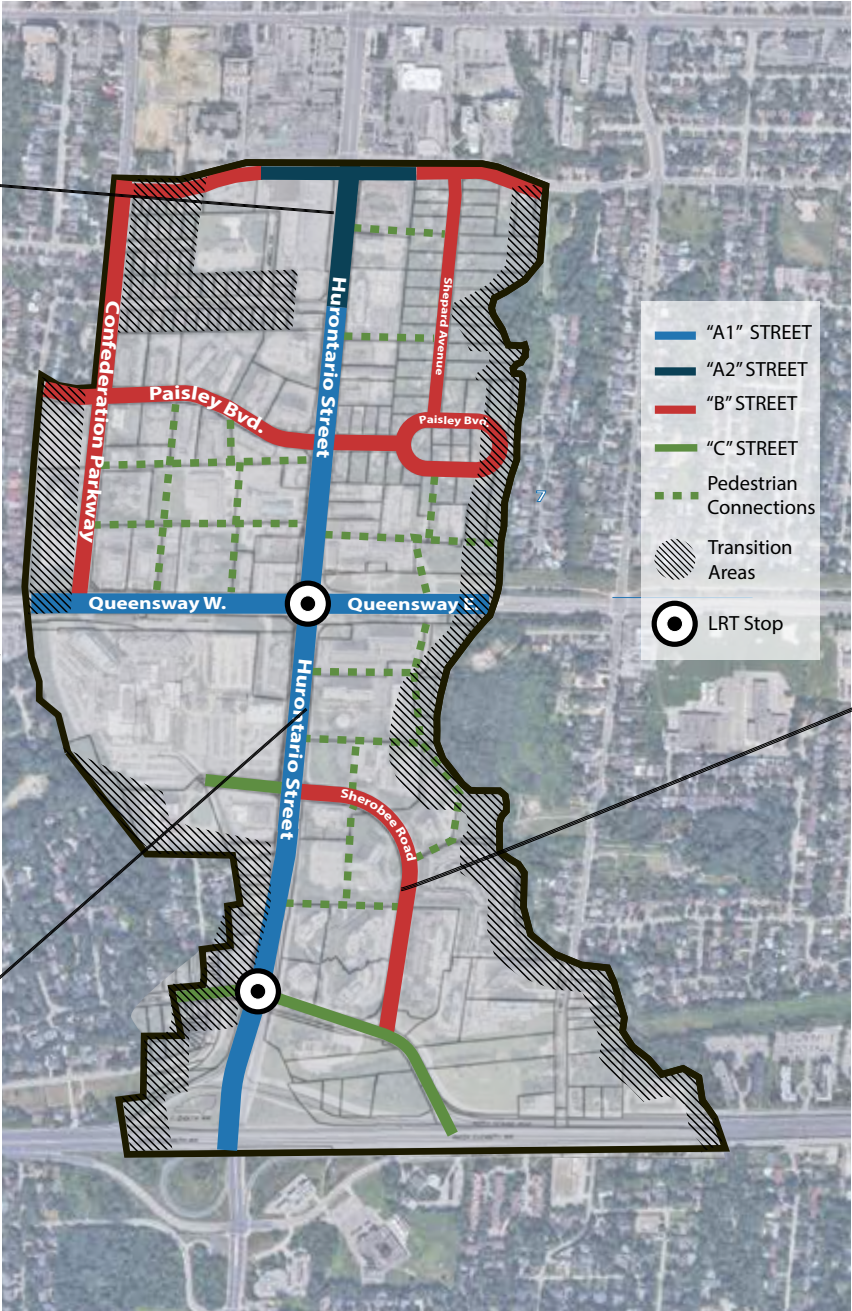
# Downtown Hospital Street Types



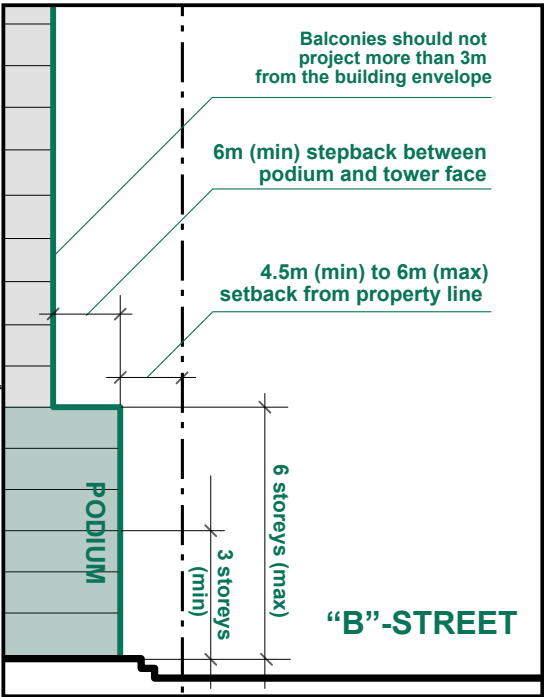
Frontage on an "A2" Street with the required podium height, setbacks and stepback.



Frontage on an "A1" Street with the required podium height, setbacks and stepback.



Street Types in the Downtown Hospital Character Area

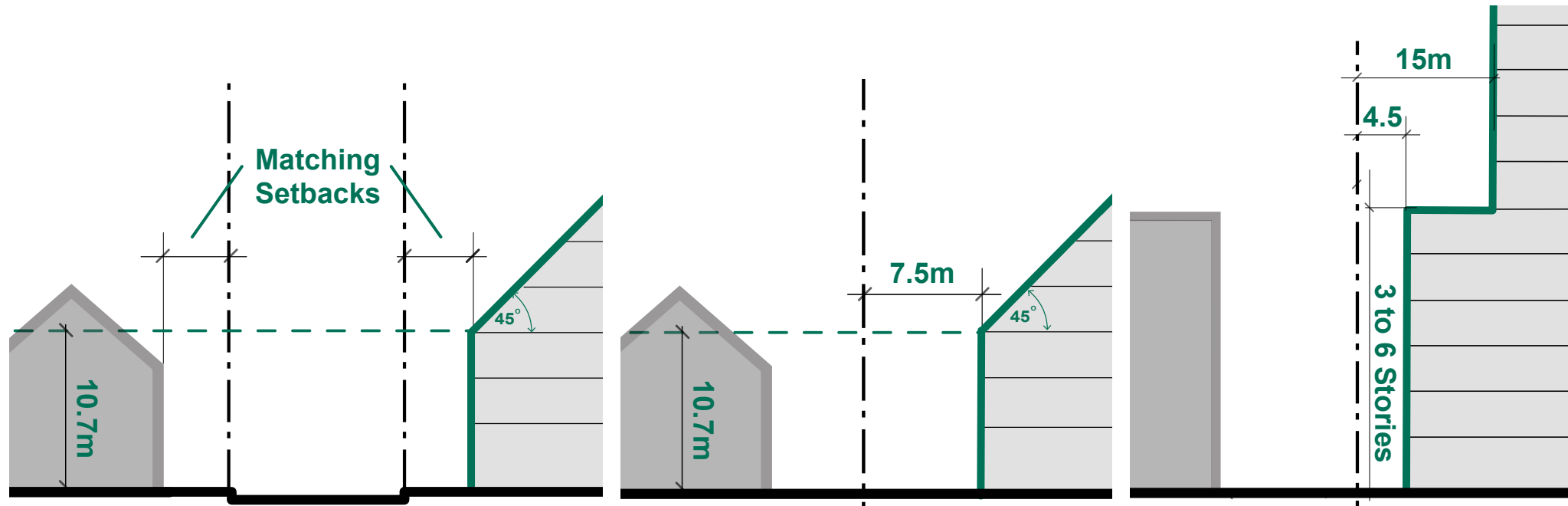


Frontage on an "B" Street with the required podium height, setbacks and stepback.



# Transition from Established Neighbourhoods

To create an appropriate built-form and to protect established neighbourhoods, parks and open spaces, the following standards identify setback and angular plane provisions for new developments to create adequate transitions. The following standards should be used wherever appropriate and could replace the other street frontage standards if deemed necessary by City staff or the applicant.



Transition from an established residential area separated by a road

Transition from the rear property line abutting an established residential area or an open space.

Transition from the rear property line abutting an apartment building in an established neighbourhoods.



Project No. 21265

March 9, 2022

**Via Email**

City of Mississauga, City Planning Strategies

Planning and Building

**Attn: Karin Phuong, Planner**

300 City Centre Drive, 8th floor

Mississauga, Ontario L5B 3C1

**Re: *Downtown Fairview, Cooksville and Hospital Policy Review – Draft  
Official Plan Amendment and Built Form Standards***

**File: *CD.03-DOW***

***65-71 Agnes Street, Mississauga (Cooksville)***

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We are the planning consultants for 65 Agnes Inc. on behalf of 65 Agnes LP, the registered owner (the “owner”) of the properties municipally known as 65-71 Agnes Street (the “subject site”), which is within the boundary of the lands subject to the City-initiated Policy Review (the “policy review”).

On behalf of the owner, we are writing to provide our preliminary comments with respect to the staff report and supporting information presented at Planning and Development Committee (PDC) held on January 24, 2022, including the draft Official Plan Amendment and other draft documents, as well as at the City-led public meeting held on February 16, 2022. We characterize these comments as preliminary insofar as we are currently reviewing the proposed amendment and documents in more detail, in relation to our client’s development plans for the site, and may bring forward additional comments for consideration.

In general, the owner supports the City’s concept of a “15-minute city” for Cooksville and the objective of promoting a policy framework for the area that is intended to foster a complete and mixed-use community that seeks to utilize and integrate high order transit. It is a vision that the owner shares and believes that any redevelopment of the subject site can assist by providing many of the key elements that have been initially conceptualized in the materials presented by City staff at the two meetings.

Given the subject site’s proximity to the Dundas Street and Hurontario Street intersection, which provides service to two future major transit services: the Hazel McCallion Line (Hurontario LRT(light rail transit) line) along Hurontario Street and the Dundas BRT (bus rapid transit) line along Dundas Street, as well as the site’s

proximity to the existing Cooksville GO Transit rail station (approximately 900 metres away), the site represents an excellent opportunity for residential intensification that can contribute to the creation of a complete community within the Cooksville area. The subject site is also designated *Residential High Density* by the City's Official Plan, which permits apartment uses in a high density form. In this regard, the subject site can provide a meaningful high density residential development that is consistent with this planned function and that will be compatible with the surrounding area.

It is our opinion that greater height and density be encouraged on the subject site to facilitate growth and support the planned structure and function of the area, as well as support the higher order transit services the serve the community. However, in reviewing the draft Official Plan Amendment, as it relates to the subject site, we feel the proposed policy direction would limit the intensification opportunities on the subject site and surrounding area. Specifically, we express concern with the proposed height limitation of 25 storeys on the subject site, as shown on proposed Map 12-4.2: Downtown Cooksville Character Area Minimum to Maximum Heights. As described above, the site is located within Cooksville, which is rich with transit offerings. Limiting the height to 25 storeys would not constitute optimization of the land that is within walking distance of three major transit services. It is good planning to intensify sites such as the subject site to make efficient use of the significant investment into transit infrastructure. In this regard, it is noted that the site immediately east of the subject site, on the east side of Cook Street, is proposed to have a maximum height limit of 29 storeys. It is our opinion, notwithstanding the approval for a 28-storey apartment building on this site, that there is no planning rationale as to why 29 storeys is supportable and approved on lands immediately east of the subject site, while the subject site (and other surrounding properties) is limited to 25 storeys.

Additionally, in reviewing the proposed policies of Section 12.4.6 (Urban Form) of the draft Official Plan Amendment, it is unclear if proposed Policy 12.4.6.1.1(a) requires a 45-degree angular plane be taken from properties that are low or medium density residential or properties that are designated *Residential Low* or *Medium Density* as shown on Map E-1 (Part of Schedule 10 – Land Use Designations) of the Mississauga Official Plan. As it relates to the subject site, the properties to the immediate north are currently developed with low density residential dwellings; however, they are designated *Residential High Density* by the Official Plan and can be expected to redevelop over time. We request the rewording of this proposed policy to provide more clarity.

Also, while proposed Policy 12.4.6.1.3 states "Tall buildings will incorporate podiums that are generally a minimum of three storeys and a maximum of six storeys", we are of the opinion that it is preferable not to prescribe limits for the





heights of podiums. Rather we suggest wording that encourages an appropriate and comfortable street wall condition, which would allow for site-specific considerations in determining the appropriate heights of podiums.

Thank you for your consideration of this submission. The owner and their consultant team support the vision of creating a vibrant mixed-use community that will see positive and longstanding impact.

We request notice of any reports and/or decision of this matter by the Planning and Development Committee or by City Council.

Yours very truly,

**Bousfields Inc.**



David Huynh, MCIP, RPP

DH/jobs

cc. Umair Waseem, 65 Agnes Inc. on behalf of 65 Agnes LP

## Karin Phuong

---

**From:** DiBerto, Dorothy <Dorothy.DiBerto@cvc.ca>  
**Sent:** Thursday, March 24, 2022 2:28 PM  
**To:** Christian Binette; Luisa Galli; Karin Phuong  
**Cc:** Hosale, Lisa  
**Subject:** RE: [External] RE: REVIEW: Updated Official Plan Amendment - Downtown Fairview, Cooksville and Hospital

Hi Christian,

Lisa passed this along to me for review – thank you for taking the time to consider our comments.

In general, the policy modifications look good but I have added some very minor comments/edits in **green** below:

Thanks again,

I'm working remotely. The best way to reach me is by email, mobile phone or Microsoft Teams.

### Dorothy Di Berto, RPP

Senior Manager, Planning and Development Services | Credit Valley Conservation

905-670-1615 ext 232 | M: 416-558-2053

[dorothy.diberto@cvc.ca](mailto:dorothy.diberto@cvc.ca) | [cvc.ca](http://cvc.ca)



**Credit Valley  
Conservation**



[View our privacy statement](#)

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**From:** Christian Binette <[Christian.Binette@mississauga.ca](mailto:Christian.Binette@mississauga.ca)>  
**Sent:** Wednesday, March 16, 2022 12:58 PM  
**To:** Hosale, Lisa <[Lisa.Hosale@cvc.ca](mailto:Lisa.Hosale@cvc.ca)>  
**Cc:** DiBerto, Dorothy <[Dorothy.DiBerto@cvc.ca](mailto:Dorothy.DiBerto@cvc.ca)>; Luisa Galli <[Luisa.Galli@mississauga.ca](mailto:Luisa.Galli@mississauga.ca)>; Karin Phuong <[Karin.Phuong@mississauga.ca](mailto:Karin.Phuong@mississauga.ca)>  
**Subject:** [External] RE: REVIEW: Updated Official Plan Amendment - Downtown Fairview, Cooksville and Hospital

**[CAUTION]** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe. If in doubt contact [help211@cvc.ca](mailto:help211@cvc.ca)

Hi Lisa,

I hope you are doing well!

Following up on our discussion in February, we are proposing several changes to the draft Official Plan Amendment (OPA) for Downtown Fairview, Cooksville and Hospital Character Areas. You can see the proposed changes related to Cooksville Creek and the associated floodplain below.

Can you please review and let us know if you have any follow-up comments by next **Friday, March 25<sup>th</sup>**? I am happy to clarify any questions you may have by phone at (905) 615-3200, ext. 5753.

I have attached the January 24<sup>th</sup> Planning and Development Committee (PDC) Information Report with Appendices containing the draft OPA and Built Form Standards to the email for your reference. Please note that we will be moving forward with an updated draft Official Plan Amendment and Built Form Standards at a statutory public meeting in the spring of this year – tentatively scheduled for May 9<sup>th</sup>.

### **Draft Official Plan Amendment Changes**

1. We are proposing a new general policy in Chapter 12: Downtown to apply to development that is within the floodplain in the 3 Character Areas. This new policy will help reinforce the importance of assessing impacts arising from development on the floodplain, and addressing any flood hazards to the satisfaction of the City of Mississauga and the Conservation Authority. It also builds on existing environmental policies contained in Chapter 6 of Mississauga Official Plan – in particular policies 6.3.50 to 6.3.54. The proposed policy is shown in red text below.

Prior to development within the floodplain, the development proponent will assess impacts, identify flood management measures and mitigate flood hazards to the satisfaction of the City and the appropriate Conservation Authority.

This is a good policy – no concerns with the wording just wondering if Chapter 12 includes a brief preamble to introduce Cooksville Creek (assuming so as this policy wording seems to elaborate on Cooksville Creek, but just confirming).

2. We are also proposing additional language in the Introduction section of each of the 3 Character Areas in Chapter 12: Downtown that emphasizes the importance of the Cooksville Creek to these communities (shown in red text below).

- Downtown Fairview

The Cooksville Creek is a significant natural heritage feature that runs north-south through Downtown Fairview and drains into Lake Ontario. Opportunities to enhance the ecological features of existing natural habitats and the urban forest will be encouraged.

- Downtown Cooksville

The Cooksville Creek is a significant natural heritage feature in Downtown Cooksville. Cooksville Creek serves as a wildlife corridor and linkage to other features within and beyond Downtown Cooksville. This corridor is also used for passive recreation with opportunities for restoration, rehabilitation and hazard mitigation along the watercourse.

- Downtown Hospital (The PDC Report contained wording for the Cooksville Creek in Downtown Hospital that is proposed to remain and is shown in black text for reference)

I note that the word 'heritage' is missing from the final wording so if this poses a problem then we can let it go. Just looking for consistent terminology throughout the OP. Typically the term is 'natural heritage feature' but if 'natural feature' is being used more commonly in Mississauga Plan that's fine.

Cooksville Creek is a significant natural feature and portions of the creek have been impacted by channelization associated with flood and erosion control and storm water management. **Enhancements to the ecological features of existing natural habitats and the urban forest will be encouraged.**

The open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, additional parkland and improvements to existing parks are planned.

Thank you,

Christian



**Christian Binette, MUP**

Planner, Planning Programs

T 905-615-3200 ext.5753

[christian.binette@mississauga.ca](mailto:christian.binette@mississauga.ca)

[City of Mississauga](#) | City Planning Strategies  
Planning and Building Division

Please consider the environment before printing.



March 23, 2022

City Planning Strategies  
Planning and Building  
300 City Centre Drive, 7<sup>th</sup> Floor  
Mississauga, ON, L5B 3C1

Attn: Karin Phuong, Project Manager, City Planning Strategies

**Re: Comments in Response to the Downtown Fairview, Cooksville and Hospital Policy Review - Draft Official Plan Amendment and Built Form Standards**

Wards: 4 & 7

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Dear Ms. Phuong,

On behalf of Kaneff Group, I am pleased to submit comments in response to the Draft Official Plan Amendment and Built Form Standards that have been prepared as a component of the Downtown Fairview, Cooksville and Hospital Policy Review. While we generally support the goals and objectives of the new policy framework, we have significant concerns with some of the implementing draft policies that could inhibit further growth and intensification along the Hurontario Corridor and throughout the Downtown Character Areas. As currently proposed, we are concerned that the draft policy framework will prove to be an impediment to implement our mutual vision and objective to support intensification and broaden the market supply of purpose-built rental housing in the City of Mississauga.

Kaneff Group has maintained a long-standing, steadfast presence, and reputation within the City of Mississauga and GTA as a community builder with a commitment to build and maintain rental housing. We are the registered owners of the following properties located within Downtown Mississauga:

- 3575 Kaneff Crescent
- 3355 Hurontario Street
- 2300 Confederation Parkway
- 2170 Sherobee Road
- 2177 Sherobee Road
- 2211 Sherobee Road

5 of our 6 properties within Downtown Mississauga were developed as purpose built rental projects and have been under Kaneff ownership for over 40 years. Combined, these properties comprise a total of 1,231 purpose built rental units and a residential GFA over 104,000 square metres (1,120,000 square feet). Currently, we have an OPA/ZBA application under review for our lands located at the north-west corner of Mississauga Valley Boulevard and Elm Drive East to permit a new 33 storey purpose built rental apartment building







consisting of 322 new rental units. On January 12, 2022, we had a DARC meeting with City staff to review our proposal for our property located at 2300 Confederation Parkway to permit a new 40 storey mixed-use development that would introduce over 570 new rental units and over 1,300 square metres of office/commercial space. We are also in the early stages of planning for the intensification of our two properties on Sherobee Road, which have great exposure along the Hurontario Corridor.

Our goal is to pursue the intensification of our existing sites in the short to mid-term, and to build, own and maintain new purpose-built rental buildings that we hope will help to address the housing affordability crisis and provide quality rental housing options to the existing and future residents of Mississauga. The height restrictions proposed along the Hurontario Corridor and throughout the Downtown Character Areas is a cause for concern that puts the viability of these purpose-built rental projects into question. As I'm sure you are aware, there are significant front-loaded costs incurred by a developer that wishes to pursue purpose-built rental over a market-based condominium development. The lack of municipal financial incentives and alternative development standards for rental projects makes it extremely difficult to qualify these proposals, which has been a persistent challenge facing municipalities and the development community. We are concerned that the draft policy framework would require us to rethink our business plan for purpose built rental.

According to the Region of Peel's Housing Master Plan, the current vacancy rate for rental apartments stands at just 1.2%, which indicates a tight market where the supply of units is far below the demand. On average, 5,300 new housing units are built in Peel annually, of which approximately only 170 are new rental units. Given this statistic, market rental units represent only 3.2% of all new housing starts in Peel on an annual basis.

We support the proposed direction and vision of the new policy framework to create 15-minute, transit-oriented, walkable complete communities. We believe that the intent of the policy direction to support more housing and taller buildings along rapid transit corridors and within MTSA's is a sustainable approach to planning and development that will undoubtedly support transit investment and projected population growth. With that said, Kaneff Group feels strongly that this policy direction is undermined by restrictive built form policies that unnecessarily limit building heights to 25 storeys or less along the Hurontario Corridor and throughout the Downtown Character Areas. There have been several examples of development applications that have recently been approved to permit building heights that far exceed 25 storeys. These applications have gone through extensive technical review and public consultation to ensure that they conform to provincial and municipal policy and represent good planning. If past applications for site-specific Official Plan Amendments have been supported by the municipality, there should absolutely be flexibility within the new policy framework for the Downtown Character Areas to consider greater building heights without the need for an Official Plan Amendment.

The Growth Plan places an emphasis on planning within Major Transit Station Areas that achieves the minimum density targets as identified by the municipality. All MTSA's located along Hurontario Street have been identified by the Region as "Primary Major Transit Station





Areas” which are capable of meeting or exceeding the minimum transit supportive density target. The Region has assigned a minimum density target of 300-400 people and jobs combined per hectare for these Primary MTSA's, which is the highest minimum density target anywhere in the Region of Peel. The Downtown Character Areas also form part of the City's Urban Growth Centre as identified by the Growth Plan, which are areas intended to accommodate significant population and employment growth. We are of the opinion that the policies of the Growth Plan and Draft Regional Official Plan are best achieved through the implementation of a policy framework that does not impede the potential for intensification along the Hurontario Corridor and within Primary MTSA's. We suggest that the City reconsider the limitations on height and built form that could inhibit the highest and best use for lands strategically located within an Urban Growth Centre, Primary MTSA's, and along an Intensification Corridor.

We are concerned that the new policy framework will bring into question our ability to contribute much needed purpose-built rental housing within Wards 4 and 7. Intensification supported by a diverse range and mix of housing options and higher order transit investment is the catalyst for creating 15-minute transit-oriented communities. We encourage the City of Mississauga to revisit the restrictions for maximum building heights that could limit the extent of intensification within MTSA's and hinder our ability to significantly contribute towards the market supply of purpose-built rental housing in the City of Mississauga.

To conclude, we have summarized a few recommendations for consideration:

1. Increase maximum building heights along the Hurontario Corridor. New buildings along the Hurontario Corridor should be able to achieve heights ranging from 35 - 40 stories, which has been demonstrated in recent decisions.
2. Provide flexibility in the policy that buildings may exceed the maximum height without an Official Plan Amendment provided appropriate justification is included with an application for a Zoning By-law Amendment.
3. Increase the maximum height assigned to our property located at 2300 Confederation Parkway from 18 storeys to 35 storeys. After further consideration and consultation with City staff, we recognize that a reduction in building height from the 40 storey's we had initially contemplated is necessary. The attached Angular Plane Study illustrates that a building height of 35 storey's is very well achievable by replicating the existing angular plane that currently separates the low-density neighbourhood from our existing 17 storey apartment building. An increased maximum building height would support additional purpose-built rental units and create an opportunity to incorporate much needed office/retail space to support the Trillium Hospital redevelopment.

We appreciate the opportunity to provide our comments on the Draft Official Plan Amendment and Built Form Standards for the Downtown Character Areas. We would like to formally request to be notified of any key milestones or decision associated with this City initiated OPA moving forward.





Sincerely,

A handwritten signature in black ink, appearing to read 'K. Freeman'.

**Kevin Freeman, MCIP, RPP**

*Director of Planning & Development  
Kaneff Group*

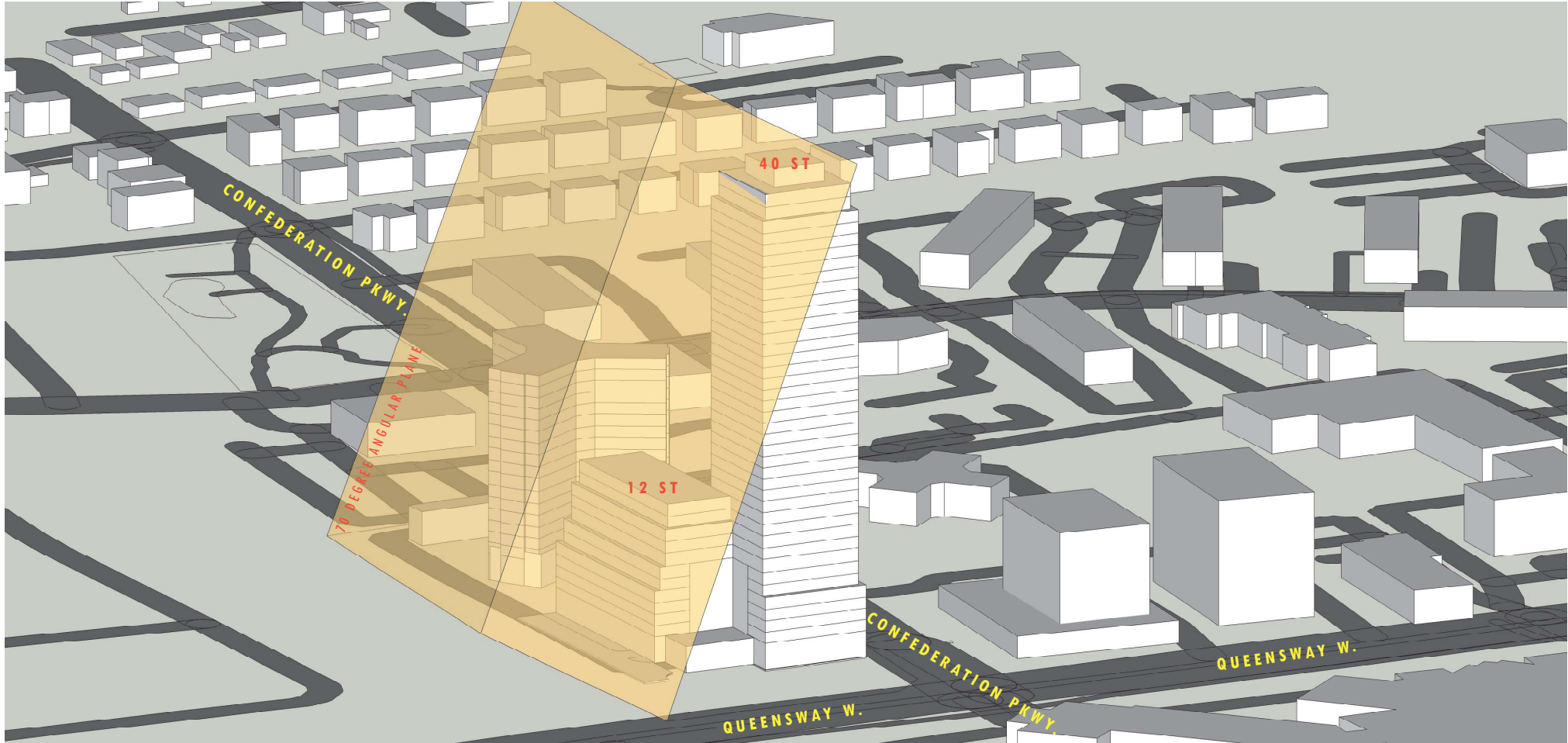
\*On behalf of the Kaneff Leadership Team











RICHMOND Architects Ltd.

55 EGLINTON AVENUE EAST  
SUITE 802  
TORONTO, ON M4P 1G8

T: 416 961 1567

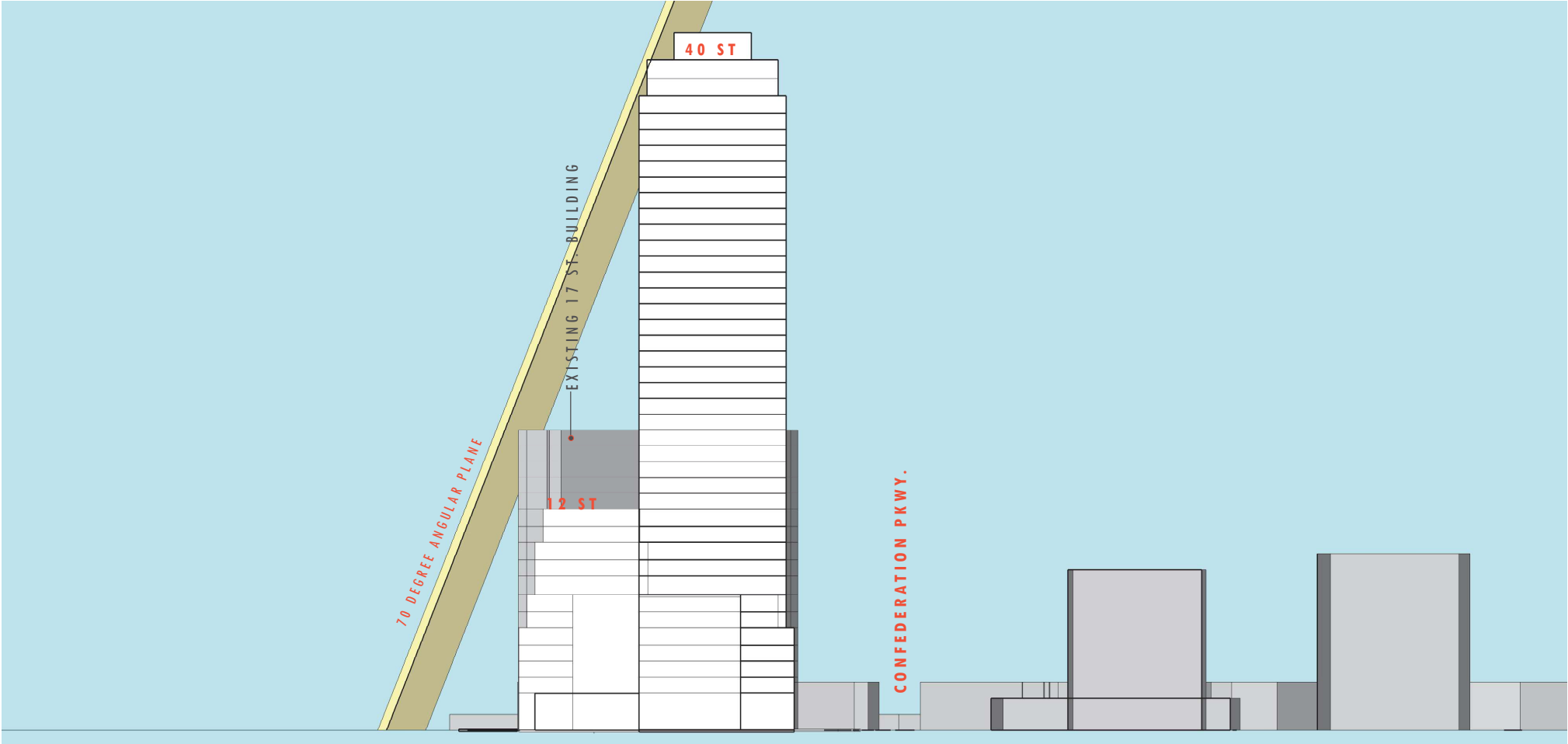
RICHMONDARCH.COM

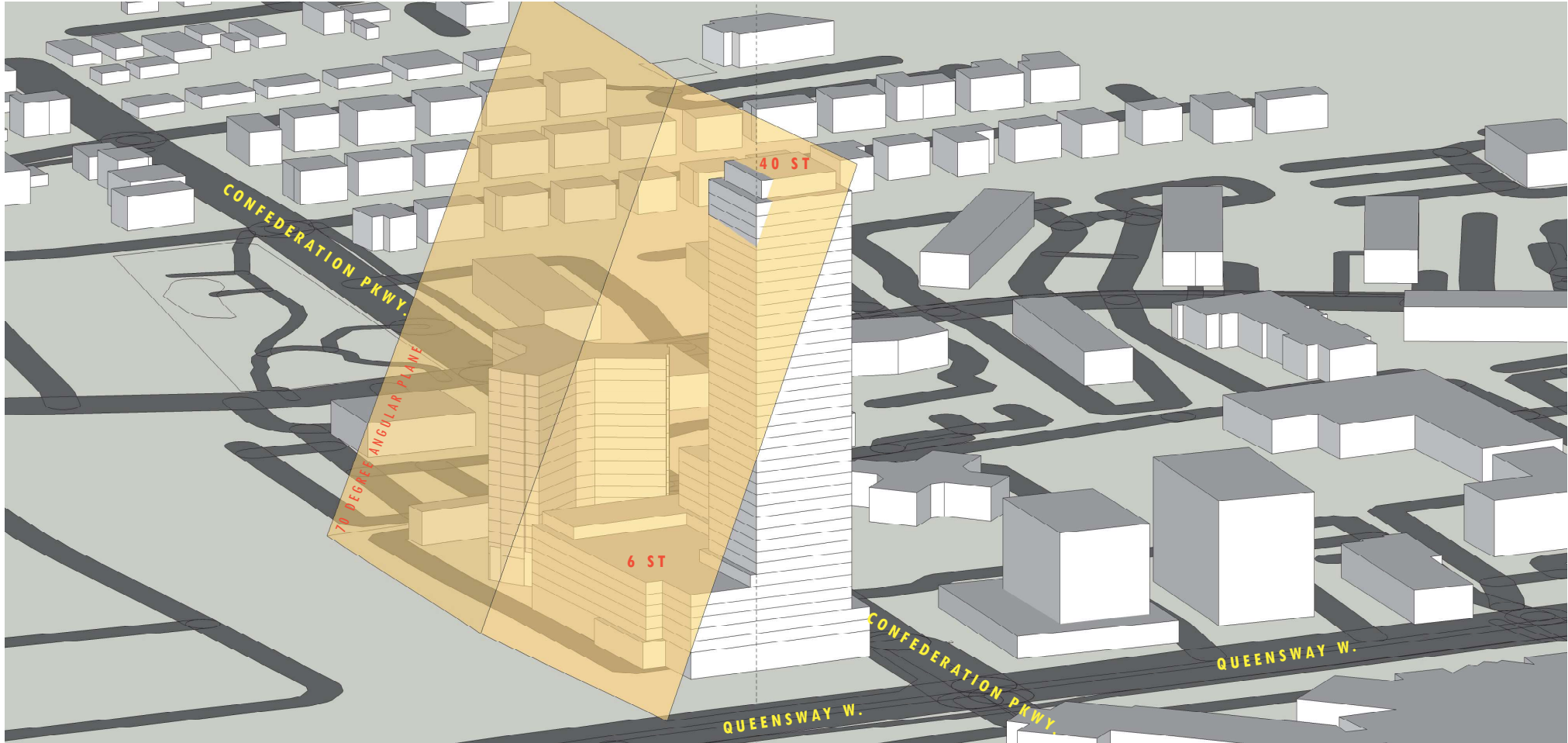
#### OPTION -1-70° ANGULAR PLANE

70 DEGREE ANGULAR PLANE

PROPOSED MIXED USE DEVELOPMENT 2300 CONFEDERATION PARKWAY, CITY OF MISSISSAUGA







RICHMOND Architects Ltd.

55 EGLINTON AVENUE EAST  
SUITE 802  
TORONTO, ON M4P 1G8

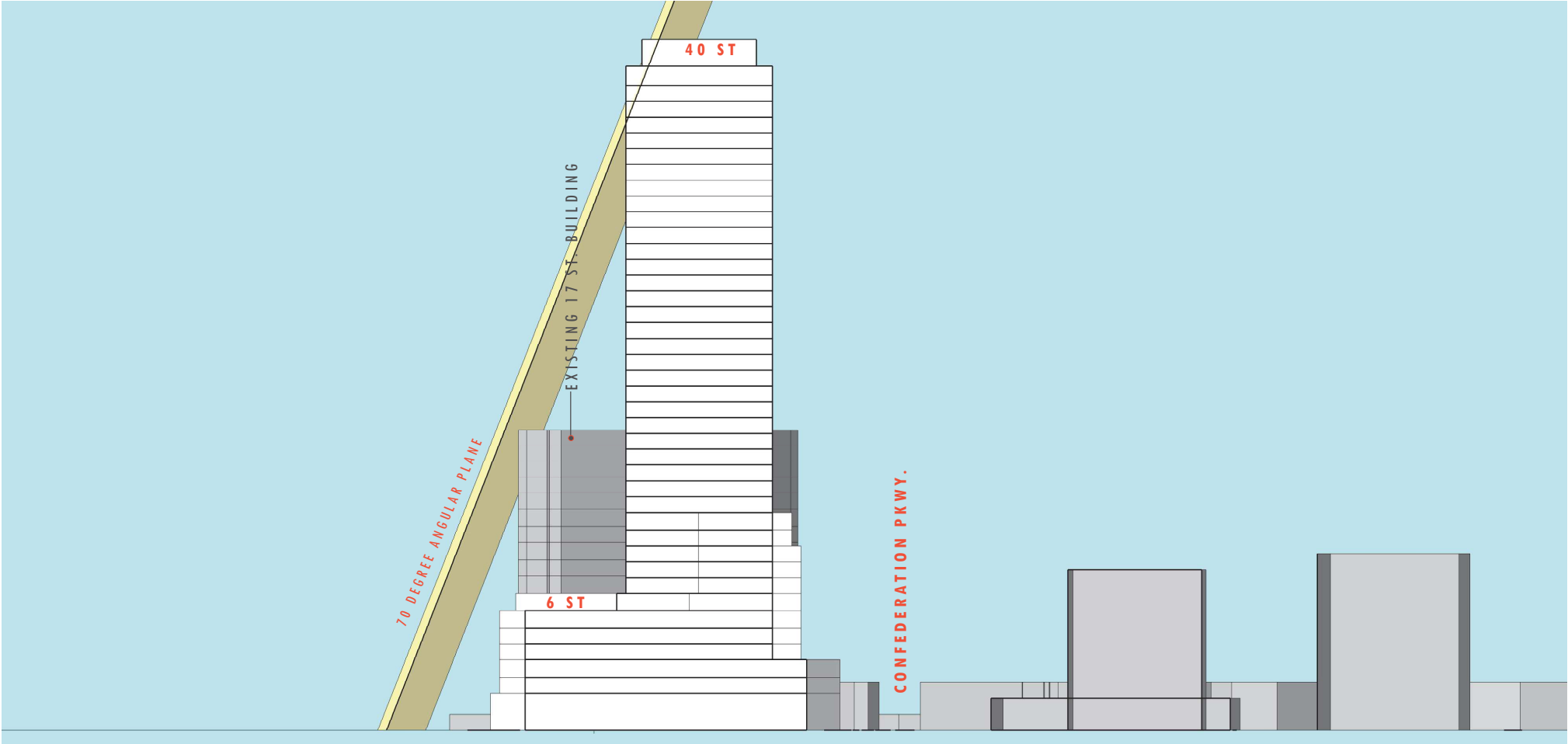
T: 416 961 1567

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#### OPTION -2-70° ANGULAR PLANE

70 DEGREE ANGULAR PLANE

PROPOSED MIXED USE DEVELOPMENT 2300 CONFEDERATION PARKWAY, CITY OF MISSISSAUGA



May 4, 2022

File No.: 589429-1

Sent Via Email ([karin.phuong@mississauga.ca](mailto:karin.phuong@mississauga.ca))

Karin Phuong, Planner  
City of Mississauga  
Planning and Building Department  
City Planning Strategies Division  
300 City Centre Drive  
Mississauga, ON L5B 3C1

Dear Ms. Phuong:

**Re: Downtown Fairview, Cooksville and Hospital Policy Review- Draft Official Plan Amendment and Built Form Standards**  
**Our Client: Bronte College**  
**Our Client's Property: 88 Bronte College Court, Mississauga**

Further to our meeting with you and your colleagues on April 26, 2022, we are submitting this letter in response to the draft Downtown Fairview, Cooksville and Hospital Policy Review- Draft Official Plan Amendment and Built Form Standards. As you know, we are the solicitors for Bronte College which is the owner of property municipally known as 88 Bronte College Court in the City of Mississauga ("Subject Property"). Our client operates a private school at this location. The Subject Property is located immediately to the south of 100 Queensway West, which is the location of the Mississauga Hospital. Our client also owns lands municipally known as 2156 Hurontario Street. Both of these sites are located within the Downtown Hospital Policy Review Area.

On February 2, 2022, we submitted a letter to Mayor Crombie and City Council advising of our client's general concern regarding the potential impact of the proposed Downtown Hospital Policies on the Subject Property. (See letter attached). We subsequently met with Mayor Crombie, Councillor Damerla and Planning staff to discuss our client's concerns regarding the proposed redevelopment of the adjacent hospital site, specifically in regard to the location of the proposed, above grade, parking garage immediately opposite the Bronte College student residence.

Having further considered the implications of the proposed redevelopment of the Mississauga Hospital site and the Draft Downtown Hospital Policy Review Official Plan Amendment, we met with you and your colleagues to discuss future policy direction for the Subject Property.

As discussed, it is our client's interest to remain at this location, subject to the Hospital site redevelopment incorporating acceptable mitigation solutions. While we appreciate the efforts of the Trillium Health Partners consulting team to respond to our client's issues regarding noise, air quality, light and shadowing, we remain concerned that the hospital redevelopment may result in the Collage needing to look at other options.

Fernanda Lopes & Associados ► Guevara & Gutierrez ► Paz Horowitz Abogados ► Sirote ► Adepetun Caxton-Martins Agbor & Segun ► Davis Brown ► East African Law Chambers ► Eric Silwamba, Jalasi and Linyama ► Durham Jones & Pinegar ► LEAD Advogados ► Rattagan Macchiavello Arocena ► Jiménez de Aréchaga, Viana & Brause ► Lee International ► Kensington Swan ► Bingham Greenebaum ► Cohen & Grigsby ► Sayarh & Menjra ► For more information on the firms that have come together to form Dentons, go to [dentons.com/legacyfirms](https://www.dentons.com/legacyfirms)

Accordingly, the current Policy Review is an opportunity to ensure that the Subject Property is being considered for its future highest and best use.

We are supportive of the Residential High Density designation on the Subject Lands, however, it is our position that the Subject Property has far greater height potential than the currently proposed maximum 12 storey height limit. This is based, in part, on the following:

- Bronte College Court currently serves only non-residential uses, allowing for intensification with no impact on stable, local, residential streets;
- The Subject Lands are located adjacent to the Mississauga Hospital site which is being redeveloped for a full range of uses, in addition to the hospital itself;
- Based on information currently available, the maximum height proposed on the Hospital site is equivalent to 35 to 40 residential storeys; and
- The Subject Lands are located to the northwest of a site, municipally known as 2114, 2124, 2130 Hurontario Street and 2095 – 2143 Grange Drive (the “Gordon Woods Condo” site), which is zoned for a maximum building height of 29 residential storeys.

Accordingly, it is our respectful submission that the maximum height for the Subject Lands should fall between the maximum height for the Hospital site and the approved height of 29 storeys on the Gordon Woods Condo site. Furthermore, our client has retained CORE Architects to conduct a review of the height and massing potential on the Subject Lands which, based on preliminary review, also supports a far greater potential building height on the Subject Lands. Please note that this review is being conducted in compliance with the Transition Provisions of the Built Form Standards proposed through the current Downtown Hospital Policy Review.

We look forward to further participation in the Downtown Fairview, Cooksville and Hospital Policy Review-Draft Official Plan Amendment and Built Form Standards process. Thank you again for meeting with us and continuing to work together to reach a mutually agreeable solution.

Yours truly,

**Dentons Canada LLP**



Mary Ellen Bench  
Counsel

MB/ap

cc: Marianne Cassin  
Christian Binette  
Li Chia



February 2, 2022

File No.: 589429-1

**SENT VIA E-MAIL [angie.melo@mississauga.ca](mailto:angie.melo@mississauga.ca)**

Mayor Crombie and Members of Council  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

Attention: Angie Melo, Legislative Coordinator

Dear Worship Mayor Crombie and Members of Council:

**Re: City Council Agenda February 2, 2022, Item 12.1 Planning and Development Committee  
Report 3-2022, PDC-00802022  
Downtown Fairview, Cooksville and Hospital Policy Review- Draft Official Plan Amendment  
and Built Form Standards  
Our Client: Bronte College  
Our Client's Property: 88 Bronte College Court, Mississauga**

We are the solicitors for Bronte College which is the owner of property municipally known as 88 Bronte College Court in the City of Mississauga ("Subject Property"). Our client operates a private school at this location. The Subject Property is located immediately to the south of 100 Queensway West, in the City of Mississauga ("Mississauga Hospital Site").

By way of this letter, we are submitting our client's concern regarding the potential impact of the proposed Downtown Hospital Policies on the Subject Property. Accordingly, kindly provide us with any further decisions and notifications with respect to this matter.

Yours truly,

**Dentons Canada LLP**

**Mary Ellen Bench**  
Counsel

MEB/ap



May 5, 2022

GSAI File: 1348-001

*(Via Email to: [megan.piercey@mississauga.ca](mailto:megan.piercey@mississauga.ca))*

Chairman and Members of Planning & Development Committee  
c/o Megan Piercey, Legislative Coordinator  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

**RE: Agenda Item 6.2 Downtown Fairview, Cooksville and Hospital  
Policy Review - Draft Official Plan Amendment and Built Form  
Standards  
Augend Investments Limited  
189 Dundas Street West, City of Mississauga**

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Glen Schnarr & Associates Inc. ('GSAI') are the planning consultants to Augend Investments Limited (the 'Owner') of the lands municipally known as 189 Dundas Street West, in Cooksville. The City is currently processing a rezoning/OPA under file OZ 21/009 W7 for a large-scale mixed-use development of these lands. For some time, we have been monitoring the Downtown Fairview, Cooksville and Hospital Policy Review Draft Official Plan Amendment and Built Form Standards on behalf of our client, including providing a letter dated January 21, 2022, and providing a deputation to the Planning and Development Committee on January 24, 2022.

As outlined in previous submissions, the Downtown Fairview, Cooksville and Hospital Policy Review Draft Official Plan Amendment will restrict the intensification and redevelopment of this site; in light of existing and future transit infrastructure which will service this area via the Cooksville GO, Hurontario LRT and proposed Dundas BRT. While the Dundas Connects Master Plan had identified our client's lands as appropriate for possible future inclusion within the Downtown Cooksville Area, the site is currently located just outside of the Downtown Cooksville boundary.

As stated in our deputation on to the Planning and Development Committee on January 24, 2022, we believe with current and pending transit infrastructure improvements and the recent surge of investment in the area, that the boundaries of the Downtown Cooksville area should be reviewed as part of the Cooksville Downtown Review and be reflected in the new Official Plan City Structure. Notwithstanding, the City seems set on maintaining the current boundaries which date back to the 1980s and pre-date the current provincial policy regime and numerous City initiated land use studies seeking to promote transit supportive redevelopment in Cooksville. Our client's lands represent a larger scale redevelopment opportunity within 800 m of all three transit forms and have the locational attributes that, in our opinion, contribute to the creation of a 15 Minute City.



We were advised by Jason Bevan, Director, City Planning Strategies, at the Planning and Development Committee meeting on January 24, 2022, that staff would take our comments back for review and consideration; however, no revision or considerations have been made to the draft OPA boundaries. Furthermore, GSAI have made earlier written submissions to staff on both this exercise, the MTSA Study and the Dundas Connects Master Plan Implementation that similarly reflect this position; none of which appear to be considered.

We have attached our letter dated January 21, 2022, herein for your further review and consideration as it is our opinion that our client's lands represent a larger scale redevelopment opportunity within 800m of all three transit forms and should logically be included within the Downtown Cooksville boundary. Again, we ask that Council and staff take our submission into consideration prior to bringing forward an OPA for Council adoption. Thank you and we appreciate the opportunity to participate in this important initiative.

Yours very truly,

**GLEN SCHNARR & ASSOCIATES INC.**

---

Jim Levac, MCIP, RPP  
Partner

*Copy: John Gagliano, Vito M. Valela, Madeline Nelson*

*Attached: Letter Re Jan 24 PDC Item 5\_3\_1.pdf*



January 21, 2022

GSAI File: 1348-001

*(Via Email to: [megan.piercey@mississauga.ca](mailto:megan.piercey@mississauga.ca))*

Chairman and Members of Planning & Development Committee  
c/o Megan Piercey, Legislative Coordinator  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

RE: **Downtown Fairview, Cooksville and Hospital Policy Review**  
**Augend Investments Limited**  
**189 Dundas Street West, City of Mississauga**

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Glen Schnarr & Associates Inc. ('GSAI') are the planning consultants to Augend Investments Limited (the 'Owner') of the lands municipally known as 189 Dundas Street West, in Cooksville. The City is currently processing a rezoning/OPA under file OZ 21/009 W7 for a large scale mixed use development of these lands. For some time we have been monitoring the various studies that will effect intensification and mixed use redevelopment in Cooksville, in light of existing and future transit infrastructure which will service this area via the Cooksville GO, Hurontario LRT and proposed Dundas BRT. While our client's lands are just outside of the Downtown Cooksville boundary, we were encouraged to see that the Dundas Connects Master Plan had identified these lands as appropriate for possible future inclusion within the Downtown Cooksville area as part of that study.

The intersection of Dundas and Hurontario Street has historically been the "epicentre" of Cooksville. In earlier iterations of Cooksville District policy documents dating back to the 1980s and up until present, the Cooksville Node boundary (now the Downtown Cooksville boundary) has remained unchanged. Historically this boundary has been confined within the CNR Tracks to the north, King Street to the south, Kirwin Avenue/Camila Road to the east and Confederation Parkway to the west. We believe with current and pending transit infrastructure improvements and the recent surge of investment in the area, that the boundaries of the Downtown Cooksville area should be reviewed as part of this ongoing study and are disappointed to see the draft OPA maintaining the boundaries dating back to the 1980s that pre-date the current provincial policy regime and numerous City initiated land use studies seeking to promote transit supportive redevelopment in Cooksville. GSAI have made earlier written submissions to staff on both this exercise and Dundas Connects that similarly reflect this position.

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The staff report speaks to the concept of creating a "15 Minute City" in Downtown Cooksville and GSAI participated in the recent webinar on this very topic co-ordinated by Ward 7 Councillor Dipika Damerla which was very well received. Drawing on the concept of the historical "four corners" of the Dundas/Hurontario Intersection being the epicentre of this community, we support the concept of a 15 Minute City centred on this area. Based on provincial planning objectives, the attached graphic visually depicts an 800 m radius around this intersection where mixed use redevelopment would benefit from an ideal trifecta of GO, LRT and future BRT transit. This will improve walkability and reduce auto dependency within this area such that it functions as a future mixed use node where increased density will help a more street related and pedestrian friendly forms of commercial development grow and thrive. We believe this reflects the vision of re-creating Cooksville as a 15 Minute City as outlined on page 4 of the staff report.

Our client's lands represent a larger scale redevelopment opportunity within 800 m of all three transit forms and have the locational attributes that, in our opinion, contribute to the creation of a 15 Minute City. We believe the inclusion of these lands, as well as potentially other sites outside the Node boundaries, warrant consideration for inclusion within the Downtown Cooksville area. While the staff report is an Information Report only, we see this as an appropriate opportunity to present our views to Council while our proposal is still in its early stages and the final Public Meeting on the forthcoming OPA has yet to take place. We trust Council and staff will take our submission into consideration and appreciate the opportunity to participate in this important initiative.

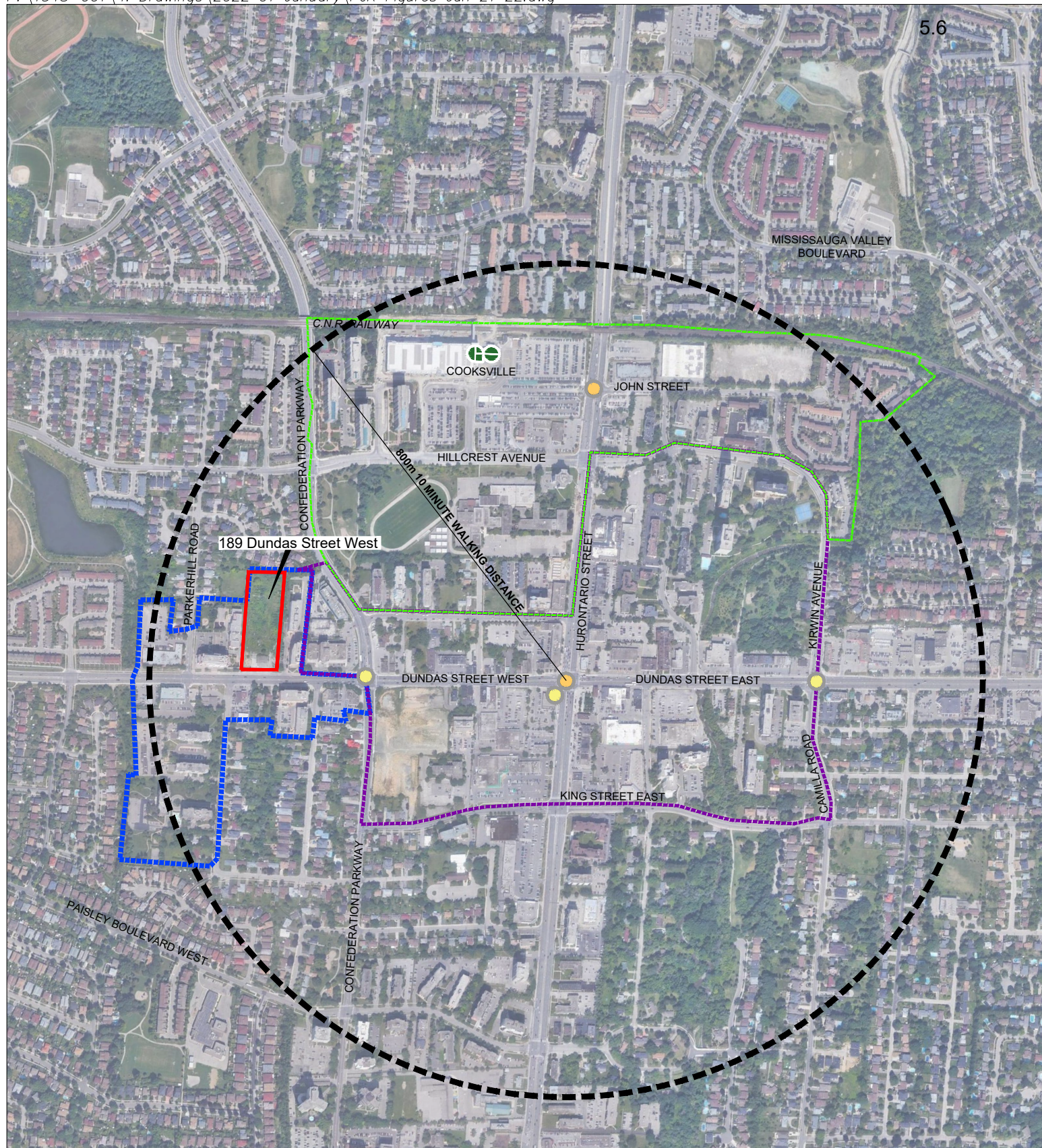
Yours very truly,

**GLEN SCHNARR & ASSOCIATES INC.**

Jim Levac, MCIP, RPP  
Partner

Copy: client





# FIGURE 1 SITE CONTEXT PLAN

189 Dundas Street West,  
City of Mississauga, Regional Municipality of Peel

## LEGEND

- Subject Property (189 Dundas Street West)
- Cooksville GO Station
- Proposed BRT Station
- Proposed LRT Station
- Confederation Parkway MTSA
- Dundas MTSA
- Cooksville GO MTSA



SCALE NTS  
JANUARY 21, 2022



# **Community Response to the Draft Official Plan Policies For Downtown Cooksville**

## **Cooksville Community Hub Working Group**

May 8, 2022

The Cooksville Community Hub Working Group is network of service providers, businesses and residents concerned about the wellbeing of the community and working to ensure this diverse, growing neighbourhood gets the support it needs to thrive.

For the Official Plan Policy review, the Cooksville Community Hub Working Group worked with City staff to host 3 community-led discussions about the Draft Official Plan Policies for the area. An in-person meeting for youth, an in-person meeting for all residents, and an online meeting were held between May 4<sup>th</sup> and May 7<sup>th</sup>. Translation and interpretation were provided in the 4 most common languages in Cooksville, English, Urdu, Arabic and Polish.

The Working groups collaborated with City staff to ensure an effective process, and were grateful for their ongoing help. Meetings included an overview of the planning process and the current Draft Official Plan Policies, which were vetted and revised by City staff to ensure accuracy. City staff participated virtually in the in-person meeting held on May 4<sup>th</sup> and participants were all provided with contact information and internet links to enable further participation in the City's process.

About 80 people attended the three meetings and their input is summarized here.

### **Key Issues**

Participation in the meetings was enthusiastic. Residents were eager to have input and add their perspectives. While most of the input focused on the Draft Official Plan Policies, some of it related to other policies and plans, which many participants felt should proceed in tandem with the OP so that new developments in Cooksville reflect the full range of intended changes.

There was positive response to the idea of renewal in Cooksville. There were also several areas where the community felt the plan, and associated policies and initiatives by the City and other governments, needed to take additional things into consideration to fully address the opportunities and challenges of the neighbourhood. Participants in the meetings felt those key issues should also be incorporated into the ongoing planning taking place for the area.

### **Affordable Housing**

Affordable housing was a high priority across all three meetings. Participants were worried that new development would be largely high-cost homes without enough rental and affordable units. There were also concerns that local improvements would result in increases in rents in existing buildings. While improvements were welcomed, participants were concerned that the new and improved Cooksville may not be a place they can afford to live, and that they would be gentrified out of their own community if there weren't steps taken to ensure an adequate supply of affordable homes.

Most participants were uncertain about how affordable housing could be ensured. On reviewing emerging City policies, they were eager to see tools like Inclusionary Zoning proceed quickly as a way to support this goal. One participant noted that some new developments in Port Credit included subsidies for 30% of the housing that was built in that community and hoped similar arrangements could be made in Cooksville.

### **Employment and Commercial Growth**

Participants welcomed the commitment to retaining and expanding retail space as a source of future jobs. They saw expanding local employment as another way to help ensure current residents could afford to stay in an improving community.

However, participants were also concerned about sustaining the diverse local retail environment. Many valued Cooksville's "mom and pop" stores and eclectic mix of retailers. People wanted to see mechanisms to help preserve existing retailers, and ensure that improvements didn't eliminate the affordable outlets for food and daily needs they currently rely on. They valued the diverse, culturally relevant shops that not only provided access to familiar goods but also helped create a sense of welcoming for the area's large immigrant community. As one participant noted "Small family run businesses are an important part of our culture and Mississauga's diversity, we should build around them." Some wondered if Cooksville would be an attractive destination if the current unique mix was replaced by the same big box brands located all across the region.

### **Community Space**

Participants frequently mentioned the importance of adding community facilities, including a new community centre and a community hub. They noted that the community is already short of community space, with few places to gather and few opportunities to build social connection. They felt that adding 8,000 new residents would further stretch the already insufficient supply of community space. A new recreation centre, such as the one long suggested for the TL Kennedy Secondary School site, was seen as a priority. A community hub was often mentioned as a way for the rapidly growing population to more easily access services and supports. Sports facilities were emphasized by youth, including a ball court, a pool and a skate park.

Some participants also noted the limited availability of faith spaces in the area, seeing faith spaces as a way to address spiritual needs but also as sites for community connection and supports for vulnerable members of the community.

### **Greenspace**

Considerable emphasis was put on greenspace. Residents were excited about the prospect of more parks and green spaces but wondered about specific plans for park design, to ensure that the space was well used and optimally beneficial. The space at Hurontario and Dundas was seen as an example of space that was not well planned or well used and they were eager to see new spaces developed with extensive community input to reflect local priorities and local knowledge. They saw active use, and "eyes on the park" as key priorities to ensure safe, inviting spaces.

Participants saw parks as recreational space but also as a way to improve connections among residents. They liked the idea of walking trails and connections between parks but wanted more detail. Some emphasized the need for green space and water infiltration strategies as tools to help address the water quality problems in Cooksville Creek, especially in the face of growing density. Others were eager for community gardens as well.

### **Public Amenities**

Participants felt that the plans for a growing population should be accompanied by expanding school space and health care facilities. There were concerns that local students would need bussing if new elementary school space was not part of the plan.

### **Connectivity**

People valued how walkable Cooksville currently is and appreciated the emphasis the plan put on walkability. They liked the addition of more walking trails, more pedestrian pathways and the new roads producing more connecting through the neighbourhood.

### **Transportation Planning**

There were concerns about traffic planning. People found Hurontario already too fast, too busy and unsafe, and worried that adding more people would make that situation worse if plans weren't in place to manage traffic better. Parking was a similar concern, with surface parking taking up considerable space and the resulting setbacks disconnecting people from local stores. People saw steps to address traffic and parking as both a safety issue and a civic improvement issue. There was support for more bike space, but also a desire to ensure it is in places where it will be well used. While people recognized that transit was expanding, they did not assume a switch to transit was automatic, and that car culture will remain and drive up traffic volumes unless new initiatives actively promoted modal shift.

### **Urban Design and Community Improvements**

While people welcomed improvements, there was a desire to see them focused on addressing local conditions. A number of people noted that Hurontario is already very windy and tall buildings would have to be designed in ways that didn't add to that problem. Many found the streetscape on Hurontario poorly designed and unappealing, often dirty, overly busy and unwelcoming. Many felt wider sidewalks, public plantings, space for patios and other initiatives that soften, humanize, clean and green the streetscape were desirable. People also felt lighting improvement and safety considerations were needed. As one participant noted that the plans add density that risks creating a "concrete jungle" and needs additional features to help make it a neighbourhood, and a "complete community".

### **Transitions**

While people welcomed improvements, the ongoing LRT construction made them wary of the risks involved in extensive redevelopment. They were eager to see initiatives that support local businesses and protect them from too much disruption. Comparisons were made to the impact of changes along St. Claire Avenue and Eglinton Avenue in Toronto.

### **Homelessness and Vulnerable People**

There was concern that there seemed to be little to address the acute homelessness issues affecting Cooksville. There are few facilities for people who are homeless to use now, and intensification and disruption will not improve those circumstances. Business owners noted that many homeless people appear to be in crisis and in need of shelters and supports, and the absence of both left them in fairly desperate circumstances which impacted the community and local business as well as homeless people themselves. Creating more affordable housing and establishing a local service hub were seen as elements of the solution but people felt more comprehensive planning around homelessness was needed.

### Collaboration

Participants felt that the overall principles were good, and in many ways appropriate to the current stage of the planning process, but those principles need to be developed and refined with active community participation and local engagement from residents and local businesses as the planning process moves forward. Some worried that the plan, at this stage, seemed to be a plan for anywhere, and not really a plan for this unique community.

Participants stressed that Cooksville's population is highly diverse, with many new immigrants who struggle with the challenges that come with recent immigration, including settlement issues and lower incomes, and the plan for Cooksville should reflect those needs and characteristics.

Participants underscored the importance of economic opportunity and the small local retailers that populate Cooksville's commercial areas that are a critical steppingstone to prosperity for newcomers in the neighbourhood, and their preservation should be part of the plan.

Participants recognized that these weren't all issues for the Official Plan, or even for the City of Mississauga to solve, and there was a need for all partners, including all levels of government and the development community to collaborate to make the changes work, but felt they should remain active considerations as planning proceeds.

## The Cooksville Community Hub Working Group







May 9, 2022

GSAI File: 1319-001

*(Via Email)*

Chairman and Members of Planning & Development Committee  
c/o Megan Piercey, Legislative Coordinator  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

**RE: Downtown Fairview, Cooksville and Hospital Policy Review  
Equity Three Holdings Inc.  
3085 Hurontario Street, City of Mississauga**

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Glen Schnarr & Associates Inc. ('GSAI') are the planning consultants to Equity Three Holdings Inc. (the 'Owner') of the lands municipally known as 3085 Hurontario Street, in Cooksville, in the City of Mississauga (the 'Subject Lands' or 'Site').

On behalf of the Owner and further to our previous correspondence dated January 24, 2022 we are pleased to provide this Comment Letter in relation to the ongoing City of Mississauga Downtown Fairview, Cooksville and Hospital Policy Review initiative ('Initiative').

GSAI has been participating in the Initiative, as well as the concurrent City of Mississauga Official Plan Review and the Region of Peel Municipal Comprehensive Review (referred to as 'Peel 2051+') initiatives. We understand that when complete, the City's Downtown Fairview, Cooksville and Hospital Policy Review Initiative will culminate in an Official Plan Amendment ('OPA') that will modify the policy framework permissions for lands within the Downtown Fairview, Cooksville and Hospital communities, including the Subject Lands. We have reviewed the draft Official Plan Amendment (hereinafter the 'draft Amendment') and Built Form Standards, released on April 29, 2022 and offer the following comments.

The draft Amendment identifies the Subject Lands as being located within the Dundas Major Transit Station Area ('MTSA'). We understand that in accordance with the Peel Regional Official Plan Amendment, adopted by Regional Council on April 28, 2022, the Dundas MTSA is one of ten (10) Protected Major Transit Station Areas ('PMTSAs') identified across the Downtown areas of the City. Furthermore, as a Site within a PMTSA, the Subject Lands and other lands surrounding the Cooksville GO Station have maximum building height permissions of 3 to 30 storeys.

Overall, the draft Amendment directs that the greatest heights within the Downtown Cooksville community are to be directed to lands in proximity to the Cooksville GO Station. As a Site within a comfortable walking distance of the Cooksville GO Station, the Subject Lands meet this criteria. As such, the Subject Lands are an appropriate and desirable location for development with the greatest heights to be concentrated. In our opinion, the permitted building height range should be modified to permit



heights up to 39 storeys on the Subject Lands given the Site is directly in front of and within walking distance of existing and planned transit services, it is situated in an area where intensification is to be directed and there is an absence of sensitive uses, parks or natural features that would be negatively impacted. Furthermore, building heights of up to 35 storeys would uphold the City Structure hierarchy, while facilitating contextually appropriate infill development to occur.

Similarly, the draft Amendment states that lands designated Residential High Density shall not have heights that exceed 25 storeys (Policy 12.1.2.2). This draft Policy is inconsistent with the proposed building height hierarchy and the City Structure. We request this draft Policy and mapping be modified to permit building heights that are consistent and appropriate in accordance with a building height schedule for the Downtown, which is to be supporting the minimum densities set out in Table 5-2 Protected Major Transit Areas.

The draft Amendment continues to propose refinements to the local road pattern. More specifically, Map 12-1.2: Downtown Fairview, Cooksville and Hospital Street Types and Map 12-4.2: Downtown Cooksville Character Area Future Roads direct that a new local street, identified as a 'C' Street is to be provided along the Subject Lands' southern property line to provide for a mid-block connection between Hurontario Street and Jaguar Valley Drive. Furthermore, this 'C' Street is intended to be a public road (Policy 12.4.6.3), support a pedestrian environment and also 'provide for vehicular access to on-site service, loading, parking and garbage storage areas' (Policy 12.1.8.2.6). Additionally, the segment of Kirwin Avenue directly in front of the Subject Lands continues to be identified as a 'B' Street. B' Streets are proposed to be residential in nature and are to provide generous setbacks from the street (Policy 12.1.8.2.8). The draft Amendment continues to not provide sufficient detail regarding the anticipated ultimate Right-of-Way widths, alignments or cross-sections of these proposed roads.

As currently contemplated, the positioning of Street 'C' and the proposed identification of Kirwin Avenue as a 'B' Street will significantly and adversely impact the development potential of the Subject Lands. It will also negatively impact the current development concept under consideration (City File OZ-OPA 21-11) by City, Regional and Agency Staff. The identification of a 'C' Street on the Subject Lands and the classification of Kirwin Avenue as a 'B' Street is concerning. We request further detail be provided.

As outlined in Figure 1 of the draft Amendment, a series of new pedestrian connections are to be provided across the Downtown Fairview, Cooksville and Hospital communities. We note that the draft Amendment continues to identify the Subject Lands as a desirable location for two (2) pedestrian connections. Collectively, these pedestrian connections are to facilitate mid-block connections and to provide safe, comfortable and convenient access across the Downtown Cooksville community. Provision of pedestrian connections in the desired locations would bisect the Subject Lands and significantly and adversely impact the development potential of the Subject Lands. Specifically, provision of the proposed pedestrian connections in the desired locations would result in significant reductions to the proposed built form and alter the proposed, optimal site design. Furthermore, the draft Built Form Standards contemplate pedestrian connections that have an ultimate corridor width of 12 metres, minimum 4 metre walkway widths and would require these lands to be unencumbered. The proposed connections would require additional land takings and the requirement for long-term public access easements to be



secured. We note that the proposed east-west connection is identified along a shared property line and would also require modifications to an existing rental complexes along Jaguar Valley Drive – which would be contrary to the City’s rental protection policy directions.

Based on the above, we request that the provision of pedestrian connections be removed and instead the provision of pedestrian pathways that support safe, comfortable and convenient movements across a Site be supported.

In Section 12.4.8 of the draft Amendment, a new area-specific policy (Site 6) is proposed. This proposed Site 6 policy states that a minimum of three (3) floors of non-residential uses will be required for buildings on lands designated Mixed Use or Residential High Density that have frontage on Hillcrest Avenue and Hurontario Street. We request that the Subject Lands be exempt from this policy. Exclusion of the Subject Lands from this policy requirement is appropriate given the proposed development has been planned and designed to facilitate a compact, higher density development with a refined, high-quality architectural design. Additionally, a range of non-residential uses are proposed and will be provided in a manner that accommodates a refined design, an optimal site design and is based on market demands.

Finally, we highlight that the draft Amendment seeks to implement Downtown Fairview, Cooksville and Hospital Built Form Standards (‘Standards’). We understand that these Standards, which serve as urban design guidelines, are to further implement the proposed built form and Character Area policies of the Mississauga Official Plan. We are concerned that the draft Standards which provide detailed design guidelines for developments occurring in the Downtown Fairview, Cooksville and Hospital Character Areas are restrictive and do not facilitate the best utilization of land, resources and infrastructure nor the creation of Cooksville as a vibrant, complete, 15-minute community.

In summary, we are concerned about the proposed Amendment and request that modifications be considered. Thank you for the opportunity to provide these comments. Our Client wishes to be included in the engagement for the Downtown Fairview, Cooksville and Hospital Policy Review initiative and wishes to be informed of updates and future meetings.

We look forward to being involved. Please feel free to contact the undersigned if there are any questions.

Yours very truly,

**GLEN SCHNARR & ASSOCIATES INC.**

  
Glen Broll, MCIP, RPP  
Partner

cc. Owner

Frank Doracin



May 9, 2022

GSAI File: 1278-001

*(Via Email)*

Chairman and Members of Planning & Development Committee  
c/o Megan Piercey, Legislative Coordinator  
City of Mississauga  
300 City Centre Drive  
Mississauga, ON L5B 3C1

RE: **Downtown Fairview, Cooksville and Hospital Policy Review**  
**33HC TAS LP; 33HC Corp.; 3168HS LP; 3168HS Corp.**  
**25 Hillcrest Avenue and 3154 Hurontario Street, City of Mississauga**

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Glen Schnarr & Associates Inc. ('GSAI') are the planning consultants to 33HC TAS LP, 33HC Corp., 3168HS LP and 3168HS Corp. (collectively, the 'Owner') of the lands municipally known as 25 Hillcrest Avenue and 3154 Hurontario Street, in Cooksville, in the City of Mississauga (the 'Subject Lands' or 'Site').

On behalf of the Owner and further to our previous correspondence dated January 24, 2022, we are pleased to provide this Comment Letter in relation to the ongoing City of Mississauga Downtown Fairview, Cooksville and Hospital Policy Review initiative ('Initiative').

GSAI has been participating in the Initiative, as well as the concurrent City of Mississauga Official Plan Review and the Region of Peel Municipal Comprehensive Review (referred to as 'Peel 2051+') initiatives. We understand that when complete, the City's Downtown Fairview, Cooksville and Hospital Policy Review Initiative will culminate in an Official Plan Amendment ('OPA') that will modify the policy framework permissions for lands within the Downtown Fairview, Cooksville and Hospital communities, including the Subject Lands. We have reviewed the draft Official Plan Amendment (hereinafter the 'draft Amendment') and Built Form Standards, released on April 29, 2022 and offer the following comments.

The draft Amendment identifies the Subject Lands as being located within the Cooksville GO Major Transit Station Area ('MTSA'). We understand that in accordance with the Peel Regional Official Plan Amendment, adopted by Regional Council on April 28, 2022, the Cooksville GO MTSA is one of ten (10) Protected Major Transit Station Areas ('PMTSAs') identified across the Downtown areas of the City. Furthermore, as a Site within a PMTSA, the Subject Lands and other lands surrounding the Cooksville GO Station have maximum building height permissions of 3 to 30 storeys. We understand that should the Province of Ontario approve the Peel Regional Official Plan Amendment, these building height permissions cannot be appealed in accordance with the *Planning Act*, as amended.

Overall, the draft Amendment directs that the greatest heights within the Downtown Cooksville community are to be directed to lands in proximity to the Cooksville GO Station. As a Site immediately adjacent to the Cooksville GO Station, the Subject Lands meets this criteria. As such, the Subject Lands are an appropriate and desirable location for development with the greatest heights to be concentrated.



In our opinion, the permitted building height range should be modified to permit heights up to 46 storeys on the Subject Lands given the Site is directly adjacent to the Cooksville GO Station, is in front of and within walking distance of existing and planned transit services, is situated in an area where intensification is to be directed and there is an absence of sensitive uses, parks or natural features that would be negatively impacted. Furthermore, building heights of up to 46 storeys in this location would uphold the City Structure hierarchy, while facilitating contextually appropriate infill development to occur. These building heights would also facilitate cost-effective delivery of housing units and sustainable development practices.

The draft Amendment continues to propose refinements to the local road pattern. More specifically, Figure 1 and Map 12-1.2: Downtown Fairview, Cooksville and Hospital Street Types direct that a new public park is to be provided on the Subject Lands, the Subject Lands are to be bisected by a future pedestrian pathway and the Subject Lands are to be surrounded by differing classifications of streets. For clarity, Map 12-1.2 continues to identify Hurontario Street and Hillcrest Avenue as an 'A1' Street, GO Access Road as a new 'B' Street and John Street as a 'C' Street. 'B' Streets are to be primarily residential in character (Policy 12.1.8.2.6), while 'C' Streets are to be tertiary streets intended to support a pedestrian environment, while also providing for vehicular access to on-site service, loading, parking and garbage storage areas. As currently contemplated, the provision of John Street as a 'C' Street and GO Access Road as a 'B' Street will impact the development potential of the Subject Lands. The provision of GO Access Road as a 'B' Street may also contribute to pedestrian-vehicular conflicts given the intended functions and users of this street. The draft Amendment continues to not provide sufficient detail regarding the anticipated ultimate Right-of-Way widths, alignments or cross-sections of these proposed roads.

As outlined in Figure 1 of the draft Amendment, new public parkland and a series of new pedestrian connections are to be provided across the Downtown Fairview, Cooksville and Hospital communities. We note that the draft Amendment continues to identify the Subject Lands as a desirable location for a new public park and a new pedestrian connection. Collectively, these features are to facilitate mid-block connections and to provide safe, comfortable and convenient access across the Downtown Cooksville community. Provision of a public park and pedestrian connection in the desired locations would bisect the Subject Lands and significantly and adversely impact the development potential of the Site. Furthermore, the draft Built Form Standards contemplate pedestrian connections that have an ultimate corridor width of 12 metres, minimum 4 metre walkway width and would require that these lands be unencumbered. The desired public parkland area is also required to be unencumbered. This would require additional land dedications, would impact an optimal site design and would adversely impact the ability to provide sufficient below-grade parking spaces. In our opinion, the appropriateness of a site for future parkland can and should be determined through the development review process. Additionally, provision of parkland in proximity to the Cooksville GO Station and within a proposed Major Transit Station Area ('MTSA') will impact the ability for any development proposal to achieve minimum density requirements as established in Provincial and Regional policies. Based on the above, we request that the provision of open space in lieu of parkland be supported and the provision of pedestrian connections that support safe, comfortable and convenient movements across the Site, but which do not require unencumbered land dedications, be supported.

In Section 12.8 of the draft Amendment, Special Site Policies are provided. We support the removal of





Special Site 2 policies which partially apply to the Subject Lands. We remain concerned about the proposed Special Site 6 policy. In particular, this proposed policy continues to state that a minimum of three (3) floors of non-residential uses will be required for buildings on lands designated Mixed Use and Residential High Density that have frontage on Hillcrest Avenue and Hurontario Street. Given both of these land use designations currently apply to the Subject Lands and given the Site's locational attributes and recognized development potential to support the creation of Cooksville as a healthy, vibrant, complete 15-minute community, we request that the Subject Lands be exempt from this policy. Exclusion of the Subject Lands from this policy requirement is appropriate given redevelopment of the Site will provide for an opportunity to incorporate a wide range of complimentary uses on the same lot while also facilitating a compact, higher density development with a refined, high-quality architectural design. Additionally, the provision of local employment opportunities through the provision of grade-related non-residential uses and new community uses and spaces on the Subject Lands will further support the direction of City Staff to facilitate this area of the City as a vibrant, comfortable, walkable main street area with a mixture of uses and high pedestrian activity. It will also support the creation of Cooksville as a complete 15-minute community, where residents and visitors are able to access facilities, services and amenities to meet daily needs within a comfortable walking distance. Additionally, the range of non-residential uses that can and should be provided should be planned to accommodate a refined design, an optimal site design and be based on market demands.

Finally, we highlight that the draft Amendment seeks to implement Downtown Fairview, Cooksville and Hospital Built Form Standards ('Standards'). We understand that these Standards, which serve as urban design guidelines, are to further implement the proposed built form and Character Area policies of the Mississauga Official Plan. We are concerned that the draft Standards which provide detailed design guidelines for developments occurring in the Downtown Fairview, Cooksville and Hospital Character Areas are restrictive and do not facilitate the best utilization of land, resources and infrastructure nor the creation of Cooksville as a vibrant, complete, 15-minute community.

In summary, we are concerned about the proposed Amendment and request that modifications be considered. Thank you for the opportunity to provide these comments. Our Client wishes to be included in the engagement for the Downtown Fairview, Cooksville and Hospital Policy Review initiative and wishes to be informed of updates and future meetings.

We look forward to being involved. Please feel free to contact the undersigned if there are any questions.

Yours very truly,

**GLEN SCHNARR & ASSOCIATES INC.**

  
Glen Broll, MCIP, RPP  
Partner

cc. Owner

## Response to Comments - Summary

Comment No.	Respondent	Topic or Character Area	Issue/Comment	Staff Comment	Recommendation for OPAs <sup>1</sup>
1	Andrew Gassman, Resident, President of Cooksville Munden Homeowners Organization at Planning and Development Committee (PDC) on January 24, 2022	General comment	(1) Supportive of the study, community engagement, heights and built form; noted that this was a good proposal.	(1) No further comment.	(1) No action required.
2	Glen Schnarr & Associates (GSAI), letters dated January 21, 2022 and May 5, 2022, at PDC on January 24, 2022 on behalf of Augend Investments Ltd., owner of lands at 189 Dundas Street West  <i>Staff have offered to meet with the consultant to discuss their concerns.</i>	Cooksville Neighbourhood Character Area	<p>(1) Request that the boundaries of the Cooksville node be reviewed to include client's lands; the lands should be reflected in the new Official Plan City Structure.</p> <p>(2) Supportive of the concept of a 15-minute City centred at the historic "Four Corners" of Hurontario/Dundas.</p> <p>(3) Client's lands are within 800 m of three forms of transit and have the locational attributes that contribute to the creation of a 15-minute city.</p> <p>(4) Dundas Connects Master Plan (DCMP) identified client's lands to be appropriate for possible</p>	<p>(1) Staff have reviewed the request and do not find it appropriate to include the subject lands within the Downtown Cooksville Character Area.</p> <p>The lands have been comprehensively assessed by the Region of Peel (Region). The Region has delineated boundaries of Major Transit Stations (MTSAs) and included the subject lands in a MTSA now known as the "Confederation Parkway MTSA" in the new Region of Peel Official Plan (RPOP, adopted April 2022).</p> <p>(2) No further comments.</p>	(1) to (5) No action required.

Comment No.	Respondent	Topic or Character Area	Issue/Comment	Staff Comment	Recommendation for OPAs <sup>1</sup>
			<p>future inclusion of the Downtown Cooksville area.</p> <p>(5) Concerned that the Downtown Fairview, Cooksville and Hospital Official Plan Amendment (OPA) will restrict intensification and development of this site.</p>	<p>(3) The Region has developed a strategy and policies that guide how growth is accommodated within Major Transit Station Areas (MTSAs).</p> <p>(4) Focus Area boundaries in the DCMP represent generalized areas for the purposes of making recommendations and do not represent Character Area boundaries. The property at 189 Dundas Street West was analyzed in the DCMP as part of the Cooksville Focus Area.</p> <p>The DCMP did not recommend expanding the Downtown Cooksville boundary to include the subject property.</p> <p>(5) The subject lands are not part of the Downtown Fairview, Cooksville and Hospital Policy review and therefore are out of the scope. However, they are being studied through the Dundas Corridor Policy Implementation OPA.</p> <p>The subject lands are located within the Confederation Parkway MTSA (RPOP) which is an area identified to accommodate intensification and achieve higher densities</p>	

Comment No.	Respondent	Topic or Character Area	Issue/Comment	Staff Comment	Recommendation for OPAs <sup>1</sup>
				<p>and compact mixed-use, transit-supportive development.</p> <p>The draft OPA does not propose to amend the Cooksville Neighbourhood Character Area policies and does not propose restrictions on intensification or development of the lands at 189 Dundas Street West.</p>	
3	<p>Shawn Kerr, Trillium Health Partners (THP) at PDC on January 24, 2022</p> <p><i>Staff met with THP and Dialog on February 11, 2022 to discuss their concerns</i></p>	Downtown Hospital	(1) Noted support for the plan and advised that THP would work with staff and provide feedback.	<p>(1) Staff has met with Dialog and THP to discuss the concerns raised in the letter submitted by Dialog, on behalf of THP (see Comment 10).</p> <p>Staff proposes modifications to the draft OPA to reflect the uses in the Zoning Order issued by the Province on April 1, 2022.</p>	(1) See Comment 10
4	<p>Trolleybus Urban Development, letter dated January 19, 2022, owner of 3161 Hurontario Street</p> <p><i>Staff met with Trolleybus Development on March 22, 2022 to discuss their concerns</i></p>	Downtown Cooksville	<p>(1) Concerned that the 25-storey height does not promote minimum transit-supportive densities and heights within the Cooksville MTSA.</p> <p>The site is located across from the Cooksville GO Station and LRT Station and within a short walk to the Dundas/Hurontario BRT Station.</p>	(1) to (3) The RPOP provides policies on Major Transit Station Areas and directs local municipalities to address: land uses that support complete communities; minimum densities; minimum and maximum heights where appropriate; and improved multi-modal access and connectivity, among other matters.	<p>(1) to (3) That the following new policy be added:</p> <p>12.4.5.2 On lands designated Residential High Density and Mixed Use and located outside of Special Site 1 in Downtown Cooksville, the maximum permitted building height as shown on Schedule 11-L may be exceeded by up to three storeys without an amendment to this Plan,</p>

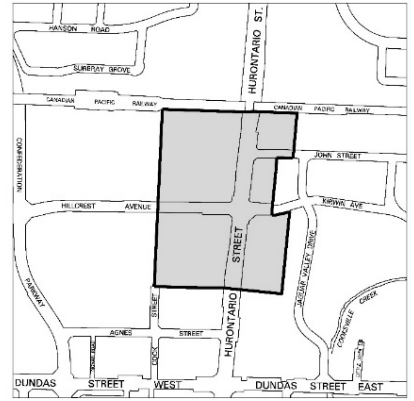
Comment No.	Respondent	Topic or Character Area	Issue/Comment	Staff Comment	Recommendation for OPAs <sup>1</sup>
			<p>(2) A number of sites along Hurontario and within Downtown Fairview exceed the 25-storey height limit approved through Council or the Ontario Land Tribunal. A height of 25-storeys is outdated in the Official Plan and will create a burden for staff and Council to defend.</p> <p>(3) Properties along Hurontario north of Hillcrest and south of the Canadian Pacific Railway should be reconsidered as the height peak of the Cooksville GO MTSA with height limits between 35-40 storeys.</p>	<p>The City's MTSA Study will be introducing official plan policies on minimum densities and minimum and maximum heights, among other matters, that conform with the RPOP.</p> <p>Information on the City-Wide MTSA Study is available at: <a href="https://yoursay.mississauga.ca/major-transit-station-areas">https://yoursay.mississauga.ca/major-transit-station-areas</a></p> <p>In Downtown Cooksville, the greatest amount of growth for population and employment is planned near the Cooksville GO Station. It is recommended that the City-wide MTSA OPA - Building Heights Schedule (Schedule L) be updated to reflect a maximum of 30 storeys for the lands identified as Special Site 2 in this OPA.</p> <p>Furthermore, incentives to provide more non-residential uses can be considered by allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p> <p>Staff are of the opinion that the proposed building heights will meet the required minimum densities of 300 residents and</p>	<p>subject to meeting the building transition policies of this Plan, where a development provides additional non-residential uses, including community infrastructure. One additional storey in building height may be permitted for every 900 square metres of non-residential gross floor area (GFA) provided above the first storey. This does not include amenity space, above grade parking or ground floor non-residential uses, where required by the policies of this Plan.</p> <p>Note: For the City-wide MTSA OPA, it is recommended that the height map show a maximum of 30 storeys on the subject lands identified as a new Special Site (Site 2) in the draft OPA.</p>



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				jobs per hectare and the minimum FSI of 1.4 for transit-supportive development in the proposed Dundas MTSA.	
5	GSAI, letter dated January 21, 2022 on behalf of Morguard Corporation, owner of lands at 1477 & 1547 Mississauga Valley Boulevard	Downtown Fairview	<p>(1) Require clarification on Map 12.3, FSI density ranges. Will FSI be replaced with updated FSI density ranges?</p> <p>(2) Concerned that the Built Form Standards (BFS) are very prescriptive on matters related to setbacks, podium setbacks and floor plate sizes. Greater flexibility should be provided to avoid replicating slab massed buildings.</p>	<p>(1) Proposed modifications to Map 12-3 include the deletion of the FSI ranges. A policy on minimum FSIs is being proposed through the City-wide MTSA OPA.</p> <p>(2) The proposed Built Form Standards (BFS) is not intended to restrict the density or development potential of new developments. The provisions in the BFS for angular planes and setbacks will allow for the redistribution of density on a site while achieving a more desirable urban form. The BFS is a document that sets out the expectations for a high quality built form. It is intended to guide development while recognizing that each site may have unique circumstances and need to be treated differently.</p>	(1) and (2) No action required.
6	Stikeman Elliott LLP, letter dated January 21, 2022 counsel to owner of lands at 3420 & 3442 Hurontario Street	Downtown Fairview	(1) Client's Official Plan and Zoning by-law Amendments (File: OZ 20/022) are proceeding before the Ontario Land Tribunal (Case No. OLT 21-001693). Stikeman Elliott is writing to comment on the draft OPA and proposed BFS.	<p>(1) Acknowledged.</p> <p>(2) The RPOP provides policies on major transit station areas and directs local municipalities to address: land uses that support complete communities; minimum densities; minimum and maximum heights where</p>	<p>(1) and (2) No action required.</p> <p>(3) See Comment 4, recommendation for Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p>

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			<p>(2) Concerned that several policies and guidelines will prevent certain lands, including the subject lands, from being developed in an optimal manner in line with the Provincial policy direction.</p> <p>(3) Concerned that the maximum height of 25 storeys for the subject lands represents an underutilization of the site. Other lands in the surrounding area were approved with greater heights, ranging from 35 to 50 storeys.</p> <p>(4) Require clarification on the removal of FSI ranges and if they will be eliminated or replaced with existing FSI ranges/figure that better reflects the density planned for the Hurontario corridor?</p> <p>(5) Concerned that the BFS is prescriptive, not a Planning Act instrument and not subject to an independent review by the OLT.</p>	<p>appropriate; and improved multi-modal access and connectivity, among other matters.</p> <p>The City-wide MTSA Study will be introducing official plan policies on minimum densities and minimum and maximum heights, among other matters, that conform with the RPOP.</p> <p>Information on the City-Wide MTSA Study is available at: <a href="https://yoursay.mississauga.ca/major-transit-station-areas">https://yoursay.mississauga.ca/major-transit-station-areas</a></p> <p>(3) Heights approved for other applications in the surrounding area reflect previous development decisions at the time. The Downtown Fairview, Cooksville and Hospital Policy Review establishes a new vision and policy framework that takes into consideration how growth is planned and managed in protected Major Transit Station Areas, which conforms with and is consistent with provincial and regional directives.</p> <p>Also see Comment 4, response to Issues 1-3 on allowing additional heights of up to 3 storeys where new</p>	<p>(4) to (5) No action required.</p>

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				<p>developments provide non-residential uses, among other requirements.</p> <p>(4) See Comment 5, response to Issue (1).</p> <p>(5) See Comment 5, response to Issue (2).</p>	
7	<p>GSAI, letters dated January 21, 2022, and May 9, 2022 on behalf of 33HC TAS LP, 33HC Corp, 3168HS LP and 3168HS Corp.</p> <p><i>Staff met with GSAI and TAS on March 16, 2022 and additional meetings with TAS as requested to discuss their concerns</i></p>	Downtown Cooksville	<p>(1) Future parkland should be determined through the development application review process. Parkland provision will impact the ability to achieve minimum density requirements established in Provincial and Regional policies (letter dated January 21, 2022).</p> <p>Request that the provision of open space in lieu of parkland be supported (letter dated May 9, 2022).</p> <p>(2) Building heights of 3 to 25 storeys should be modified (letter dated January 21, 2022). Lands to the north in Downtown Fairview have less transit connectivity than the subject lands and include permitted heights of up to 50 storeys.</p> <p>Building height range should be modified to permit heights of up to 46 storeys (letter dated May 9, 2022). Site is adjacent to the</p>	<p>(1) Parkland requirements will be determined through the development application review process. Parkland can help create more liveable and healthier communities. Opportunities to expand the parkland system will help foster social interaction and a sense of place.</p> <p>(2) See Comment 6, response to Issue 3 and Comment 4, response to Issues 1-3.</p> <p>(3) After further consideration, staff proposes that this should be a general policy direction. References to the specific building height variations of “3 to 5” storeys in the policy (formerly draft policy 12.4.6.1.2) will be deleted.</p> <p>(4) The proposed BFS allows up to 25% of the building frontage to be further setback to</p>	<p>(1) and (2) No action required.</p> <p>(3) That draft policy 12.1.8.2.2 be revised as follows:</p> <p>12.1.8.2.2 For larger developments where more than one tower is proposed, a variation in building heights will be achieved.</p> <p>(4) to (7) No action required.</p> <p>(8) That the Mixed Use policy requirements for non-residential uses on the ground floor be revised as follows:</p> <p>12.4.7.2.1 Notwithstanding the policies of this Plan, development along Hurontario Street, Dundas Street, Hillcrest Avenue (Cook Street to Hurontario Street) and King Street (new Cook Street to Shepard Avenue):</p>

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			<p>Cooksville GO Station and within walking distance of existing and planned transit.</p> <p>(3) Request policy 12.4.6.1.2 (variations of building heights) be modified. The development proponent should identify whether height variation between multiple structures are desirable and determine the height variation on a site-by-site basis.</p> <p>The proposed policy is prescriptive and can limit the development potential and ability to provide for compact, higher density, pedestrian-oriented transit-supportive forms.</p> <p>(4) Request clarification on the BFS in regards to Hillcrest Avenue as a Street 'A' with a continuous streetwall condition along 90% of the frontage lands - if this will allow for deviations for pedestrian connections or parkland?</p> <p>(5) John Street is identified as a 'C' Street and GO Access is identified as a 'B' Street which will impact development potential of the subject lands. Furthermore, this will also</p>	<p>accommodate other streetscape elements.</p> <p>(5) Site circulation and access will be confirmed through the development application review process. Road widenings are not proposed in the draft OPA. However, policy 8.2.2.1.d in Mississauga Official Plan allows for minor adjustments to the basic right-of-way widths and alignments for roads without further amendment to the Plan.</p> <p>(6) The pedestrian connections will enhance the transportation system, provide linkages between areas and to transit stations area and are appropriate uses that support active transportation. The pedestrian connections shown on Figure 1 are not the exact locations but provide the general locations for future pedestrian connections.</p> <p>Pedestrian connections may be secured in the form of public easements, sidewalks or walkways through development applications.</p> <p>(7) The policy concerning replacement of non-residential</p>	<p>a. will provide non-residential uses on the ground floor where the building is immediately adjacent to a street listed above; and</p> <p>b. may consist of primarily residential uses above the ground floor if not subject to a special site policy.</p> <p>(9) No action required.</p> <p>(10) That the new Special Site near the Cooksville GO Station be revised as follows:</p> <p>12.4.8.2 Site 2</p>  <p>12.4.8.2.1 The lands identified as Special Site 2 are located on the east and west sides of Hurontario Street between</p>

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			<p>impact site design given that vehicular access is encouraged off 'C' Streets. These new public roads may require additional road widening and would influence the massing, scale and site design.</p> <p>(6) New pedestrian connections as identified on Figure [1] (pedestrian connections) will bisect the site and adversely impact the development potential of the subject lands. Request that modifications be made to preserve the development potential and ability to provide optimal design that incorporates residential and non-residential uses, pedestrian pathways, landscaped open spaces, and safe and convenient access.</p> <p>Pedestrian connections in the draft BFS have a corridor width of 12 metres, including a minimum 4 metre walkway width and would require that these lands be encumbered (letter dated May 9, 2022).</p> <p>(7) Concern that policy 12.4.8.2.1 (Mixed Use designation - replacement of existing office and retail and service commercial floor space) will</p>	<p>uses will ensure that redevelopment of these lands for more housing units will be balanced with non-residential uses to achieve a complete community. The policy is now being proposed through the City-wide MTSA OPA and as such, has been removed from this draft OPA.</p> <p>(8) At grade uses with retail and service commercial businesses along key corridors are needed to achieve the vision of a vibrant, 15 minute city. After further consideration, staff proposes that at grade requirements be expanded to allow for a broader mix of uses. References to “retail and service commercial uses” should be broadened to include “non-residential uses”.</p> <p>(9) Acknowledged.</p> <p>(10) Given the subject site’s location within walking distance to the Cooksville GO Station and light rail transit, the requirements for 3 floors of non-residential uses is appropriate. This requirement provides opportunity for more employment in MTSA’s and a greater mix of uses such as</p>	<p>the Canadian Pacific Railway and north of Agnes Street.</p> <p>12.4.8.2.2 Notwithstanding the policies of this Plan, a minimum of three floors of non-residential uses will be required for buildings on lands designated Mixed Use or Residential High Density that are immediately adjacent to Hillcrest Avenue and Hurontario Street.</p> <p>Also see Comment 4, recommendation for Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p> <p>(11) No action required.</p>



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			<p>adversely impact the ability to provide a mixture of residential and non-residential and community uses on the subject site.</p> <p>(8) Policy 12.4.8.1 requiring at grade retail and service commercial uses while not permitting at grade residential dwellings is restrictive and hinders the ability to provide a wide range of uses. Request that consideration be given to modify or grant a site-specific exception.</p> <p>(9) Support the removal of Special Site 2 policies.</p> <p>(10) Request to be exempt from the new Special Site policy [Site 2] requiring a minimum of 3 floors of non-residential uses on lands designated Mixed Use and Residential High Density. The subject site's location attributes and development potential can support the creation of Cooksville as a healthy, vibrant complete 15-minute community. Redevelopment of the site will incorporate a range of complimentary uses while facilitating high density development.</p>	<p>retail, services and amenities that are accessible by walking or cycling. The area should be focused near the Cooksville GO Station at Hillcrest Avenue and Hurontario Street (see Special Site 2 map)</p> <p>Also see Comment 4, response to Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p> <p>(11) See Comment 5, response to Issue 2.</p>	

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			(11)The BFS are restrictive and do not facilitate the best utilization of land, resources, infrastructure and creation of a vibrant, 15-minute community.		
8	Bousfields Inc., letter dated January 21, 2022, on behalf of Almega Asset Management, owner of lands at 60 Dundas Street East	Downtown Cooksville	<p>(1) Client owns the subject site which is currently a commercial plaza and will be proposing to redevelop the site for a mix of uses with new housing and a public park.</p> <p>(2) Proposed building heights (Map 12-4.2) for a maximum of 3 to 16 storeys on the subject site does not recognize that the Dundas Street East will see higher order transit (bus rapid transit). Increased heights should be considered.</p> <p>(3) Request clarification on policy 12.4.8.2.1 regarding the planned function of the non-residential components to be maintained or replaced in new developments.</p> <p>(4) Request clarification on policy 12.4.7.4 that would restrict vehicular access to Dundas Street.</p> <p>(5) General comment that the area can benefit from a new signalized intersection to allow safe pedestrian crossing at</p>	<p>(1) Acknowledged.</p> <p>(2) The Downtown Fairview, Cooksville and Hospital Policy Review takes into consideration the Hurontario/Main Street Corridor Master Plan and Dundas Connects Master Plan which were endorsed by City Council in 2010 and 2018, respectively. They were both conducted as a Municipal Class Environmental Assessment and provide a number of recommendations on transportation, urban design, heights and changes to land uses to support sustainable transit-supportive development and intensification. The maximum heights in the proposed City-wide MTSA OPA will allow for appropriate transit-supportive development in keeping with the vision for the areas.</p> <p>See Comment 4, response to Issues 1-3 on allowing additional heights of up to 3 storeys where new</p>	<p>(1) No action required.</p> <p>(2) See Comment 4, recommendation for Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p> <p>(3) to (5) No action required.</p>

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			Dundas Street and Cooksville Creek.	<p>developments provide non-residential uses, among other requirements.</p> <p>(3) The policy is intended to ensure that essential elements of a complete community are retained, enhanced and integrated in new developments. This policy is now being proposed through the City-wide MTSA OPA and will not be contained in Chapter 12: Downtown of the Mississauga Official Plan.</p> <p>(4) The policy will require development applicants to evaluate how the number of driveways can be reduced or consolidated to minimize conflict with pedestrians, cyclists and transit.</p> <p>(5) The City's Pedestrian Master Plan (2021) is a long-term plan with the goal to improve the pedestrian network and infrastructure, among other matters. It identified a multi-use crossing gap at Dundas Street and Cooksville Creek. The City's Transportation and Works Department is developing a plan to install crossings where multi-use trails and pedestrian trails intersect</p>	

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				with streets and mid-block, where appropriate and feasible.	
9	GSAI, letter dated January 21, 2022, on behalf of RASDA Holdings Ltd and Baro Construction Ltd, owner of lands at 55 & 93 Dundas Street East	Downtown Cooksville	<p>(1) The building height range on the subject lands should be modified. The subject lands are within walking distance of existing and planned transit services. There is inconsistent application of building heights. Building heights of up to 29 storeys are permitted on lands to the north, along the Cooksville Creek within Downtown Cooksville.</p> <p>(2) Concerned about several urban form policy requirements: height variations of 3 to 5 storeys, 45 degree angular plane transition and Jaguar Valley Drive as a 'B' Street. These requirements will impact the development potential of the subject lands to support a pedestrian-oriented, active, complete main street.</p> <p>(3) The site is currently a retail plaza containing a variety of uses. Concerned that policy 12.4.8.2.1 will require the replacement of existing office and retail and service commercial floor space and policy 12.4.8.2.3a will require new development to provide convenient and easily</p>	<p>(1) The property located at 55 Dundas Street East is identified in the Official Plan as Special Site 1 in Downtown Cooksville. The maximum building height proposed in the City-wide MTSA OPA of 18 storeys is equivalent to the maximum height permitted in the existing Official Plan. This area is envisioned to be developed with a main street focus and a human-scaled built form.</p> <p>The 29-storey apartment building at Cooksville Creek and Dundas Street East (3100 Kirwin Avenue) was constructed in the 1970s and the maximum building height recognizes the existing height.</p> <p>Staff has further considered the recommendations of the Dundas Connects Master Plan and proposes to increase the maximum height of buildings, in areas outside Special Site 1, from 8-12 storeys to 16 storeys.</p> <p>A new policy is also proposed to provide opportunity and flexibility to increase the</p>	<p>(1) See Comment 4, recommendation on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p> <p>(2) See Comment 7, Issue 3</p> <p>(3) to (5) No further action required.</p>

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			<p>accessible office, retail and commercial uses.</p> <p>Requirement to replace existing floor space and to provide office space will adversely impact the ability to provide a mix of residential and non-residential uses and may not represent current or market trends. Request that policy requirement be modified or be granted a site-specific exception.</p> <p>(4) Encouraged by Special Site policy 12.4.9.1 which proposes modified heights up to 18 storeys (55 Dundas St. E). Concerned with the requirement of 3 floors of non-residential uses. This is restrictive and may adversely impact development potential given that the site is able to provide a wide range of non-residential uses. Request that Special Site policy 1 be modified or a site-specific exclusion of the policy requirement for 3 floors of non-residential uses.</p> <p>(5) The BFS are restrictive and do not facilitate the best utilization of land, resources, infrastructure and creation of a vibrant, 15-minute community.</p>	<p>maximum building height permission by up to 3 storeys to accommodate employment uses outside Special Site 1.</p> <p>(2) For comments on the height variation, see Comment 7, response to Issue 3.</p> <p>For comments on angular plane and built form policies/standards, see Comment 5, response to Issue 2.</p> <p>(3) See Comment 7, response to Issue 7.</p> <p>(4) See Comment 7, response to Issue 10.</p> <p>(5) See Comment 5, response to Issue 2.</p>	



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10	<p>Dialog, letter dated January 21, 2022, planning consultant for Trillium Health Partners, owner of lands at 100 Queenway West (Mississauga Hospital)</p> <p><i>Staff met with THP and Dialog on February 11, 2022 to discuss their concerns</i></p>	Downtown Hospital	<p>(1) Development application for the new Mississauga Hospital expansion project includes the lands at 100 Queensway West and 2250 Hurontario Street.</p> <p>(2) Introduction and Vision – provide additional language under the Guiding Principle “Support health care services” to add “that create a healthier community” following the words “promote a range of health care services and supportive uses...”</p> <p>(3) Noted that the FSI information is not labelled and will be implemented through the City-wide MTSA and requested engagement and notification of the project.</p> <p>(4) General section, policy 12.5.3, Figure 1 – request removal of conceptual future public open space on the western half of 100 Queensway West as THP is not considering a parkland parcel.</p> <p>(5) Building Height, policy 12.5.4 and Map 12-5.2 – request that 100 Queensway West and 2250 Hurontario Street be identified as “Development application in process”.</p>	<p>(1) Acknowledged.</p> <p>(2) The suggested wording is appropriate and builds on the vision of Downtown Hospital.</p> <p>(3) Staff provided THP with online links to contact information regarding the Regional and the City-wide (Mississauga) MTSA projects.</p> <p>(4) After further consultation with Community Services, staff finds it appropriate to remove the conceptual parkland symbol on the subject lands.</p> <p>(5) The hospital will be expanded to include the lands at 2250 Hurontario Street and as such, staff finds it appropriate to identify lands that are designated Institutional to be shown as ‘height not applicable’ on the map for the City-wide MTSA OPA.</p> <p>(6) The intent of the policy for active building frontages is to promote at-grade non-residential uses, inclusive of institutional uses.</p> <p>(7) See Comment 5, response to Issue 2.</p>	<p>(1) No action required.</p> <p>(2) That the draft OPA be modified to add the words “that create a healthier community” following the “promote a range of health care services and supportive uses...” under Guiding Principle “Support health care services”</p> <p>(3) No action required.</p> <p>(4) That the draft OPA be modified to remove the symbol showing a future public park on the subject lands.</p> <p>(5) No action required for this OPA.</p> <p>It is recommended that lands that are owned by THP and designated Institutional be shown on the height map as “height not applicable” in the City-wide MTSA OPA.</p> <p>(6) to (8) No action required.</p> <p>(9) and (10) That the Special Site policy for the THP lands (renumbered to 12.5.6.2 Site 2, policy 12.5.6.2.2.c) be written as follows:</p>

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			<p>(6) Urban Form, policy 12.5.5 [renumbered to 12.1.8.2.7] and BFS – request to include institutional use as part of an active building frontage.</p> <p>(7) Request that the BFS include flexible language and/or exemptions for institutional uses on standards such as the maximum separation distance, podium setbacks and a maximum tower floor plate size.</p> <p>(8) Transportation – request clarification on the definition of a pedestrian connection and whether the connection will require an easement registered on title.</p> <p>(9) Request that the intersection of Queensway and Hurontario be identified as a gateway feature or an urban plaza in the draft policies.</p> <p>(10) Special Sites, policy 12.5.9.7 (Site 7) – request that the uses include Residential High Density to allow the site to achieve its full potential within the vicinity of a higher-order transit node and a complete community supportive of a connected health care system.</p>	<p>Staff further note that the Zoning Order issued by the Province supersedes municipal planning authority.</p> <p>(8) Pedestrian connections are not being proposed on the THP lands through this OPA.</p> <p>Where pedestrian connections are proposed on private property, public easements will be required.</p> <p>(9) Staff finds that it is appropriate to add an urban plaza to the Special Site policy.</p> <p>(10) The OPA should be updated to reflect the uses as issued by the Province's Zoning Order on April 1, 2022 and include residential buildings up to 25 storeys for e.g., retirement buildings or special needs housing.</p> <p>(11) Further review by THP and staff, find the request appropriate and for clarity, a new policy should be added requiring development applications to demonstrate that proposals do not impede with the hospital heliport.</p>	<p>c. lands identified as Area C provide a potential location for an urban plaza and will also permit the following uses:</p> <ul style="list-style-type: none"> <li>○ conference centre;</li> <li>○ education and training facility;</li> <li>○ financial institution;</li> <li>○ major and secondary office;</li> <li>○ overnight accommodation;</li> <li>○ personal service establishment;</li> <li>○ research and development;</li> <li>○ restaurant;</li> <li>○ retail store;</li> <li>○ retirement building to a maximum building height of 25 storeys; and</li> <li>○ special needs housing, including a long-term care facility and a hospice to a maximum building height of 25 storeys.</li> </ul> <p>(11) That the following policy be added:</p> <p>12.5.5.1 Development proposals in the vicinity of the Mississauga Hospital must demonstrate that new</p>

Comment No.	Respondent	Topic or Character Area	Issue/Comment	Staff Comment	Recommendation for OPAs <sup>1</sup>
			(11) At the meeting with THP and City staff on February 11, 2022, a new policy on heliports was considered to help ensure that any new building or structure does not impact the flight path to the Mississauga Hospital heliport.		buildings and structures do not interfere with the flight path to the hospital heliport.
11	<p>GSAI, letters dated January 24, 2022 and May 9, 2022, and at Public Meeting, PDC on May 9, 2022 on behalf of Equity Three Holdings Inc., owner of lands at 3085 Hurontario Street</p> <p><i>Staff met with GSAI and the landowner on March 15, 2022 to discuss their concerns</i></p>	Downtown Cooksville	<p>(1) Building height range should be modified to permit up to 35 storeys on the subject lands (letter dated January 24, 2022). The subject lands are within walking distance of existing and planned transit services. There is inconsistent application of building heights. Building heights of up to 29 storeys are permitted on lands to the north, along the Cooksville Creek within Downtown Cooksville. Lands to the north of Downtown Cooksville have permitted heights of up to 50 storeys.</p> <p>Building height range should be modified to permit up to 39 storeys given that the site is within walking distance of existing and planned transit services. It is situated in an area where intensification is to be directed (letter dated May 9, 2022).</p>	<p>(1) See Comment 4, response to Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p> <p>See Comment 6, response to Issue 3 on previously approved heights.</p> <p>(2) For comments on the height variation policy, see Comment 7, response to Issue 3.</p> <p>(3) and (4) See Comment 7, response to Issue 5</p> <p>(5) Pedestrian connections may be secured in the form of public easements, sidewalks or walkways through development applications. Modifications to existing rental apartment sites are not proposed in the draft OPA. The OPA identifies future</p>	<p>(1) See Comment 4, recommendation to Issues 1-3 on allowing additional heights of up to 3 storeys</p> <p>(2) See Comment 7, recommendation to Issue 3</p> <p>(3) to (7) No action required.</p> <p>(8) See Comment 7, recommendation to Issue 10</p> <p>(9) and (10) No action required.</p>

Comment No.	Respondent	Topic or Character Area	Issue/Comment	Staff Comment	Recommendation for OPAs <sup>1</sup>
			<p>(2) Concerned about the urban form policy requirement: height variations of 3 to 5 storeys, podium heights of 3 to 6 storeys. Height variations should be determined through development application process.</p> <p>(3) There is insufficient details on right-of-way widths, alignments and cross-sections of the proposed roads identified on Map [12-4.2] and the figure.</p> <p>(4) The 'C' Street and identification of Kirwin Avenue as a 'B' Street will negatively impact the current development concept/potential.</p> <p>'C' Street will result in a land taking and removal of proposed pedestrian networks, landscaped open spaces and mix-use built form.</p> <p>'B' Street may require additional road widening. Request that further details be provided and that new public roads be negotiated through the development application process.</p> <p>(5) Request that the pedestrian connections be removed and</p>	<p>pedestrian connections should redevelopment occur.</p> <p>(6) An angular plane would not apply to the subject lands, in this instance.</p> <p>(7) See Comment 7, response to Issue 7.</p> <p>(8) See Comment 7, response to Issue 10.</p> <p>(9) See Comment 5, response to Issue 2.</p> <p>(10) In the interim, development applications in process will continue to be reviewed on their own planning merit with consideration for the proposed MTSA policies. If applications are approved prior to the City-wide MTSA OPA being in force and effect, the approved heights will be reflected in the updated MTSA amendment.</p>	

Comment No.	Respondent	Topic or Character Area	Issue/Comment	Staff Comment	Recommendation for OPAs <sup>1</sup>
			<p>instead, support the provision of pedestrian pathways that support safe, comfortable and convenient movements across a site. Pedestrian connections would bisect the subject lands and significantly and adversely impact the development potential.</p> <p>Pedestrian connections would require land takings and requirements for public access easements.</p> <p>The east-west pedestrian connection will require modifications to the existing rental complexes and would be contrary to the City's rental protection policy directions.</p> <p>Pedestrian connections in the draft BFS have a corridor width of 12 metres, including a minimum 4 metre walkway width and would require that these lands be encumbered (letter dated May 9, 2022)</p> <p>(6) Applying an angular plane on the subject property is inappropriate and will adversely impact the development potential. Request that this policy requirement exclude the</p>		



Comment No.	Respondent	Topic or Character Area	Issue/Comment	Staff Comment	Recommendation for OPAs <sup>1</sup>
			<p>subject lands.</p> <p>(7) Concern that policy 12.4.8.2.1 (Mixed Use designation - replacement of existing office and retail and service commercial floor space) will adversely impact the ability to provide a mixture of residential and non-residential and community uses on the subject site. Request that this policy requirement be removed. This should be determined by market trends and a Market Impact Study.</p> <p>(8) Request to be exempt from the new Special Site policy (Site 9) requiring a minimum of 3 floors of non-residential uses on lands designated Mixed Use and Residential High Density.</p> <p>The proposed development is planned for compact, higher density development and high-quality architectural design. A range of non-residential uses will be provided based on market demands.</p> <p>(9) BFS are restrictive and do not facilitate the best utilization of land, resources, infrastructure and creation of 15-min community.</p>		

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			(10) Concerned that new Major Transit Station Area policies will impact the processing of development applications under review.		
12	GSAI, letter dated January 24, 2022, on behalf Pro-M Capital Partners Inc., owner of lands at 2434-2490 Shepard Avenue	Downtown Hospital	<p>(1) New pedestrian connections would bisect the site and negatively impact the development potential of the subject lands. Request that modifications be made to facilitate appropriate pedestrian connections, evaluated on a site-by-site basis.</p> <p>(2) Building heights of 3 to 8 storeys should be modified. Lands along Hurontario Street have increased height permissions. The subject site is within a proposed MTSA and Downtown Mississauga Urban Growth Centre. The proposed building heights are not consistent with Provincial, Regional and local policy directions to accommodate compact, higher density, mixed-use, transit-supportive development.</p> <p>(3) Concerned about several urban form policy requirements: height variations of 3 to 5 storeys, a 45 degree angular plane and maximum floor plate allowances. Shepard Avenue is</p>	<p>(1) See Comment 7, response to Issue 6.</p> <p>(2) The RPOP provides policies on major transit station areas and directs local municipalities to address: land uses that support complete communities; minimum densities; minimum and maximum heights where appropriate; and improved multi-modal access and connectivity, among other matters.</p> <p>The City's MTSA Study will introduce official plan policies on minimum densities and minimum and maximum heights, among other matters, that conforms with the RPOP. The lands do not have direct frontage onto Hurontario Street, has a narrow configuration and are adjacent to lower density residential land uses. As such, a mid-rise form is appropriate for the lands.</p>	<p>(1) No action required.</p> <p>(2) See Comment 4, recommendation for Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p> <p>(3) See Comment 7, Issue 3</p> <p>(4) That the following policy be deleted from Downtown Hospital, Special Site 1, in the Mississauga Official Plan:</p> <p>12.5.4.1.2 (renumbered to 12.5.6.1.2)</p> <p>d. housing forms that provide for the stepping of densities and building heights from higher at the north and west ends of the site to lower at the south and east ends of the site.</p> <p>(5) and (6) No action required.</p>

Comment No.	Respondent	Topic or Character Area	Issue/Comment	Staff Comment	Recommendation for OPAs <sup>1</sup>
			<p>identified as a 'B' Street which may require additional land dedication These requirements will impact the development potential of the subject lands and to support a pedestrian-oriented, active complete main street.</p> <p>Urban form policies 12.5.5 re: required 45 degree angular plane, height variations of 3 to 5 storeys where multiple buildings are proposed and maximum floor plates will adversely impact development potential of the site. Also concerned that 'B' Street may require additional land dedication.</p> <p>(4) Special Site 1 [Policy 12.5.4.1.2]: removal of references to land use designations is supported. Request that part 'd' which identifies where highest heights are to be situated should be modified.</p> <p>(5) Support re-designation from Residential Medium Density to Residential High Density.</p> <p>(6) BFS are restrictive and do not facilitate best utilization of land, resources and infrastructure nor creation of vibrant, complete, 15-minute community</p>	<p>Information on the City-Wide MTSA is available at:  <a href="https://yoursay.mississauga.ca/major-transit-station-areas">https://yoursay.mississauga.ca/major-transit-station-areas</a></p> <p>Furthermore, incentives to provide more non-residential uses can be considered by allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements. See response to Comment 4.</p> <p>(3) See Comment 7, response to Issue 3 specific to height variations.</p> <p>See Comment 5, response to Issue 2 on built form standards.</p> <p>(4) Policy 12.5.4.1.2.d (Special Site 1, Downtown Hospital), refers to the transition of densities and building heights. Transition policies and new built form standards are proposed in the OPA. In addition, building height schedules will be implemented through the City-wide MTSA OPA. Therefore, it is appropriate to delete part 'd' of this policy.</p> <p>(5) Acknowledged.</p>	

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				(6) See Comment 5, response to Issue 2.	
13	Online comments from participants using the Konveio platform, February 5, 2022 to March 15, 2022	Various comments on vision, parking, views, height, transition, traffic and street type	<p>(1) Downtown Cooksville – Vision: Agree and support more mixed use development, with more walkable shops, etc.</p> <p>(2) Downtown Fairview – Guiding Principle – Transition in Building Height: One of the most beautiful aspects of Mississauga is its horizon while driving down Hurontario Street. The sunrise and sunsets in this area are breathtaking. Efforts should be made to support this continued unobstructed view.</p> <p>(3) Downtown Fairview – Building Height: The southeast corner of Hurontario Street and Fairview Road has an extremely sharp transition from a high density building to an established community. This area should be re-classified to the existing buildings in the area for a minimum/maximum of 3 storeys.</p> <p>(4) Parking: The City should eliminate parking minimums to help reduce housing prices and encourage pedestrian and transit oriented communities.</p>	<p>(1) Acknowledged.</p> <p>(2) Mississauga Official Plan reinforces policies on spacing tall buildings to also permit skyviews. The draft OPA and BFS also provides further direction on setbacks and stepbacks along Hurontario Street that can also protect view corridors.</p> <p>(3) See response to Comment 20, response to Issue 1.</p> <p>(4) The City has completed a parking regulations review and provided recommendations on reducing parking in the Mississauga Zoning By-law.</p> <p>More information is available at <a href="https://yoursay.mississauga.ca/parking-regulations-study">https://yoursay.mississauga.ca/parking-regulations-study</a>.</p> <p>(5) The completion of the Cook Street extension will provide a public road connection from King Street northerly to the Cooksville GO Station. The road will be well-designed as a complete street to support</p>	<p>(1) to (6) No action required.</p> <p>(7) That Map 12-1.2: Downtown Fairview, Cooksville and Hospital Street Types be updated to identify a segment of Dundas Street West, west of Confederation Parkway as an 'A2' Street as follows:</p>





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14	Participants at Virtual Community Meeting, February 16, 2022	Various comments	(1) Comments and questions were asked including more cycling connections, more places of work, new housing, maximum heights and densities, reducing parking rates, and transition to existing residential areas	(1) Responses to the questions are posted online at <a href="https://yoursay.mississauga.ca/downtown-fairview-cooksville-and-hospital-policy-review/widgets/116078/faqs">https://yoursay.mississauga.ca/downtown-fairview-cooksville-and-hospital-policy-review/widgets/116078/faqs</a>	(1) No further action required.
15	Bousfields Inc., letter dated March 9, 2022, on behalf of owner of lands at 65-71 Agnes Street	Downtown Cooksville	<p>(1) Concern that the 25 storeys height limit would not optimize the land. The site is within three major transit services. Greater height and density should be encouraged on the subject site to facilitate growth and support planned structure and higher order transit services.</p> <p>No planning rationale why 29 storeys is supported on lands to the east while this site is limited to 25 storeys</p> <p>(2) Request that policy 12.4.6.1.1(a) be reworded to provide clarity if the angular plane is taken from properties that are designated Residential Low or Medium Density as shown on Map E-1.</p> <p>(3) Suggest rewording policy 12.4.6.1.3 so it does not have prescribed limits of 3 to 6 storeys for podiums e.g. “encourage an appropriate and comfortable street wall condition, which would allow for site-specific considerations in</p>	<p>(1) See Comment 9, response to Issue 1 on the 29-storey building at Cooksville Creek and Dundas Street East.</p> <p>Also see Comment 4, response to Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p> <p>(2) The angular plane should be measured from Residential Low and Medium Density land use designations. The draft policy (renumbered to 12.1.8.2.1a) should be revised to provide clarity.</p> <p>(3) See Comment 5, response to Issue 2.</p>	<p>(1) See Comment 4, recommendation for Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p> <p>(2) That policy 12.1.8.2.1a specify the measurement of the angular plane from residential low and medium density land use designations as follows:</p> <p>12.1.8.2.1 New buildings will achieve a high quality urban design and built form, and will be designed and located to:</p> <p>a. create a transition in height generally consistent with a 45 degree angular plane that is measured from the property line adjacent to residential low and medium density land use designations;</p> <p>(3) No action required.</p>

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			determining the appropriate height of podiums”		
16	Credit Valley Conservation (CVC), March 24, 2022  <i>Staff met with CVC on February 10, 2022 to discuss their concerns</i>	Cooksville Creek	(1) Suggest that the draft policies recognize Cooksville Creek as a significant natural heritage feature for all three communities.  (2) Propose language to reinforce the importance of assessing impacts resulting from development in the floodplain.	(1) Agree - provide additional language in the introduction on the importance of Cooksville Creek as a significant natural heritage feature.  (2) Agree - additional policy on assessing flood impacts are appropriate to include in the OPA.	(1) That the following text be added to the introduction section for each Character Area:  Downtown Fairview Character Area 12.3.1 Introduction ... The Cooksville Creek is a significant <b><i>natural heritage feature</i></b> that runs north-south in Downtown Fairview and drains into Lake Ontario. Opportunities to enhance the ecological features of existing natural habitats and the urban forest will be encouraged.  Downtown Cooksville Character Area 12.4.1 Introduction ... The Cooksville Creek is a significant <b><i>natural heritage feature</i></b> in Downtown Cooksville. Cooksville Creek serves as a wildlife corridor and linkage to other features within and beyond Downtown Cooksville. This corridor is also used for recreation with opportunities for restoration, rehabilitation and hazard

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					<p>mitigation along the <b><i>watercourse</i></b>.</p> <p>Downtown Hospital 12.5.1 Introduction ... Cooksville Creek is a significant <b><i>natural heritage feature</i></b> and portions of the creek have been impacted by channelization associated with flood and erosion control and storm water management. Enhancements to the ecological features of existing natural habitats and the urban forest will be encouraged.</p> <p>(2) That the following policy be added:</p> <p>12.1.1.7 Prior to development within flood plains, the development proponent will assess impacts, identify flood management measures and mitigate flood hazards to the satisfaction of the City and the appropriate conservation authority.</p>
17	Kaneff Group, letter dated March 23, 2022, owner of lands at 3575 Kaneff Crescent, 3355	Downtown Fairview, Downtown Hospital	(1) An application for a 33-storey purpose built rental apartment building is under review for the lands located at the northwest corner of Mississauga Valley Boulevard and Elm Drive East.	<p>(1) Acknowledged.</p> <p>(2) Some rental apartment buildings have been approved for less than 25 storeys. For example, in 2021, a new 6-</p>	<p>(1) and (2) No action required.</p> <p>(3) See Comment 4, recommendation to Issues 1-3.</p>

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	<p>Hurontario Street, 2300 Confederation Parkway, 2170, 2177 and 2211 Sherobee Road</p> <p><i>Staff have met with Kaneff on May 12, 2022 to discuss their concerns</i></p>		<p>Two properties at Sherobee Road and Hurontario Street are in the early stages of planning.</p> <p>(2) Height restrictions will cause viability concerns for purpose built rental projects.</p> <p>Recently approved development applications permit building heights that exceed 25 storeys.</p> <p>Maximum building heights along the Hurontario Corridor should be increased to 35 - 40 storeys, which has been demonstrated in recent decisions.</p> <p>(3) Provide flexibility that buildings may exceed the maximum height without an OPA provided appropriate justification is included with an application for a Zoning By-law Amendment.</p> <p>(4) Increase the maximum height at 2300 Confederation Parkway from 18 storeys to 35 storeys. An increased maximum building height would support additional purpose-built rental units and create an opportunity to incorporate much needed office/retail space to support the Trillium Hospital redevelopment.</p>	<p>storey rental apartment was built along Hurontario Street south of King Street West (2475 Hurontario Street).</p> <p>Also see Comment 9, response to Issue 1 and Comment 6, response to Issue 3.</p> <p>(3) See Comment 4, response to Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p> <p>(4) Staff have taken into consideration the respondent's concern regarding a maximum height of 18 storeys for the property located on the northwest corner of Queensway West and Confederation Parkway. It is recommended that the City-wide MTSA OPA be updated to reflect a maximum height of 25 storeys for that property which is consistent with the heights planned along the Queensway within Downtown Hospital.</p> <p>When a new building abuts an established neighbourhood, a transition in height that is generally consistent with a 45</p>	<p>(4) No further action required.</p> <p>Note: For the City-wide MTSA OPA, it is recommended that the height map show a maximum of 25 storeys on the property located on the northwest corner of Queensway West and Confederation Parkway.</p>

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			The Angular Plane Study prepared for Kaneff by Richmond Architects Ltd. illustrates that a building height of 35 storeys is achievable by using a 70 degree angular plane. This measurement replicates the existing angular plane that currently separates the low-density neighbourhood from the existing 17 storey apartment building.	degree angular plane is applied. Assessment of the development proposal will also require studies such as Sun-Shadow and Wind studies to ensure shadow and wind impacts on adjacent neighbourhoods are addressed.	
18	Dentons, letter dated May 4, 2022, solicitors on behalf of owner of lands at 88 Bronte College Court and at Public Meeting, PDC on May 9, 2022  <i>Staff met with Bronte College on April 26, 2022 to discuss their concerns</i>	Downtown Hospital	(1) Concerned that the redevelopment of the Mississauga Hospital may result in Bronte College needing to consider other use options and ensure that the subject property is considered for its highest and best use.  (2) Concerned that the subject property is proposed for a maximum 12 storey height limit. The maximum height should be increased to between the approved building height of 29 storeys (Gordon Woods condominium site) and the maximum height for the Mississauga Hospital.	(1) Acknowledged.  (2) The 29-storey apartment building located on Hurontario south of Bronte College Court was approved in 2017 through the development application process. The approved maximum building height is recognized in the proposed City-wide MTSA OPA Building Heights Schedule (Schedule 11-L).  For the hospital lands, the Minister issued a Zoning Order that recognizes the maximum height for the hospital of 120 metres and 22 storeys.  Staff has considered the requests and recommends that the City-wide MTSA OPA be updated to reflect a maximum height of 25 storeys for the	(1) No action required.  (2) See Comment 4, recommendation for Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.  Note: For the City-wide MTSA OPA, it is recommended that the height map show a maximum of 25 storeys on the subject lands.



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				<p>property, consistent with the heights planned immediately adjacent to Mississauga Hospital.</p> <p>Also see Comment 4, response to Issues 1-3 on allowing additional heights of up to 3 storeys where new developments provide non-residential uses, among other requirements.</p>	
19	Robin Vanderfleet, on behalf of ACORN Canada, at PDC on May 9, 2022	Affordable Housing	(1) Emphasized the importance of ensuring affordable housing and inclusionary zoning.	<p>(1) The City is advancing the Inclusionary Zoning for Affordable Housing Study and preparing the final recommendations for the inclusionary zoning official plan amendment and zoning by-law.</p> <p>Information on this study is available at:  <a href="https://yoursay.mississauga.ca/inclusionary-zoning-policy-for-affordable-housing">https://yoursay.mississauga.ca/inclusionary-zoning-policy-for-affordable-housing</a>.</p>	(1) No action required.
20	<p>Residents at PDC on May 9, 2022</p> <p><i>Staff were also invited to attend the Local Ward Councillor's Town Hall Meeting April 13, 2022 on the Downtown Fairview Policy Review.</i></p>	Downtown Fairview	(1) Concerned about the built form standards, transitional plane, height, privacy, security, shadowing, flooding and the impact of development on the existing character of Burdock Place.	<p>(1) The City has not received a development application for the lands located on the southeast corner of Hurontario Street and Fairview Road East.</p> <p>The proposed BFS is a document that sets out the expectations for a high quality built form. It is intended to guide development while recognizing that each site may</p>	(1) No action required.

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				<p>have unique circumstances and need to be treated differently.</p> <p>The proposed BFS will provide standards that can help ensure new developments provide adequate transition from established residential neighbourhoods.</p> <p>When a new building abuts an established neighbourhood, a transition in height that is generally consistent a 45 degree angular plane is applied. Assessment of the development proposal will also require studies such as Sun-Shadow and Wind studies to ensure shadow and wind impacts on adjacent neighbourhoods are addressed.</p> <p>Burdock Place is unique due to a 3 metre grade difference between Burdock Place and Fairview Road East. The angular plane would be measured from the lowest grade level and thus limiting the overall maximum height for new developments.</p> <p>Also, appropriate setbacks and stepbacks will be required to mitigate the impacts of new development to existing residential dwellings.</p>	

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21	<p>Sean Meagher, comments received May 9, 2022, Cooksville Hub Working Group</p> <p><i>Staff met with the Cooksville Community Hub Working Group and/or the respondent on March 17, April 6 and 28, 2022 to discuss their concerns. Staff were invited to attend a meeting hosted by the Cooksville Hub Community Meeting on May 4, 2022.</i></p>	Downtown Cooksville	<p>(1) Concern that Cooksville will become less affordable to own or rent units; there is a need to address homelessness.</p> <p>(2) Support policies to retain, expand retail space and increase local employment. Concerned that mechanisms are needed to help preserve existing retailers and support existing local businesses.</p> <p>(3) Concern that the Cooksville community lacks community space. A new community centre/community hub including a sports facility is important and necessary with the anticipated population growth.</p> <p>(4) Excited about the prospect of more parks and greenspaces. New spaces should provide active use and be safe and inviting spaces. They should be developed with extensive public input.</p> <p>(5) Additional school space and health care facilities should be provided for a growing population.</p> <p>(6) Support the idea of walking trails, pedestrian pathways and</p>	<p>(1) See Comment 19, response to Issue 1.</p> <p>(2) It is recommended that the City explore ways to help support and retain local businesses. A policy that provides this direction is suggested for the OPA.</p> <p>(3) Community Services is working on a feasibility review with the Peel District School Board to consider options for integrated community uses. More information will be provided on the City's web site when it becomes available.</p> <p>(4) The City has engaged with the community on various park redevelopment plans in Mississauga. Seeking feedback from residents is important to understanding the type of park and amenities that could best serve the community.</p> <p>(5) School boards review and comment on development applications to determine the school capacity to meet accommodation needs.</p> <p>(6) Acknowledged.</p>	<p>(1) No action required.</p> <p>(2) That a new policy be added to the draft OPA as follows:</p> <p>12.1.8.1.5 Strategies to support the retention of local businesses and increase employment uses may be pursued including Community Improvement Plans and other incentives.</p> <p>(3) to (11) No further action required.</p>

Comment No.	Respondent	Topic or Character Area	Issue/Comment	Staff Comment	Recommendation for OPAs <sup>1</sup>
			<p>new roads that provide more connections for walkability through the neighbourhood.</p> <p>(7) Concern about traffic planning and that adding people would worsen the situation if there are no plans to manage traffic better.</p> <p>(8) Hurontario is already very windy and tall buildings must be designed to minimize wind. The streetscape on Hurontario Street is poorly designed and wider sidewalks, plantings and space for patios are desirable.</p> <p>(9) Concern about the ongoing LRT construction and eager for initiatives that support local businesses and protect them from too much disruption.</p> <p>(10) Concern there was little to address homelessness and more comprehensive planning is needed. There are few places for people who are homeless; many are in need of shelters. More affordable housing and a local service hub can help.</p> <p>(11) Overall principles in the draft policies are good but should be developed and refined with community participation and</p>	<p>(7) The City is looking at more transportation options to decrease the pressure on our existing streets to accommodate future growth. In addition to light rail transit along Hurontario Street, rapid transit is being planned along Dundas Street. Master plans completed for the Hurontario/Main Street corridor and the Dundas corridor provide recommendations for improved connections for pedestrians, cyclists and transit users.</p> <p>(8) The City may request Pedestrian Wind Comfort and Safety Study be submitted as a requirement for a complete development application.</p> <p>Construction of transit improvements (LRT, BRT) will look at streetscape design including sidewalk widths, street and pedestrian lighting and space to accommodate street trees, benches and bike rings.</p> <p>(9) The City is working with Metrolinx, the owner of the project, to mitigate impacts to businesses as much as possible.</p>	

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			engagement. The plan does not seem to be unique for this community. The plan should reflect the challenges of immigration including settlement issues and lower incomes. Preservation of small local retailers should also be part of the plan.	<p>Information is available at:  <a href="http://www.metrolinx.com/en/greaterregion/projects/hurontario-lrt.aspx#projectupdate">http://www.metrolinx.com/en/greaterregion/projects/hurontario-lrt.aspx#projectupdate</a></p> <p>Small business supports:  <a href="https://www.mississauga.ca/services-and-programs/business/small-business/">https://www.mississauga.ca/services-and-programs/business/small-business/</a></p> <p>(10)The Region of Peel founded Homeless Health Peel in 2020 and created an Isolation and Recovery program to support residents experiencing homelessness. More information is available at:  <a href="https://homeleshealthpeel.ca/">https://homeleshealthpeel.ca/</a></p> <p>(11)Staff have held a number of meetings on the draft OPA and BFS. A virtual community meeting was held on February 16, 2022. Staff also met with various participant groups throughout the year. Furthermore, a public meeting was held on May 9<sup>th</sup>, 2022 and a second public meeting is planned for July 5, 2022.</p> <p>Information for newcomers on services, programs and events is available at Immigration Peel:</p>	



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				<a href="https://www.immigrationpeel.ca/">https://www.immigrationpeel.ca/</a>  See Comment 21, response to Issue 2 regarding local businesses.	

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<sup>1</sup> Recommendation for OPAs will implement policies on vision, guiding principles, community infrastructure, urban design, roads and pedestrian connections (OPA No. 145) and Protected MTSA policies on heights and land uses that are specific to Downtown Fairview, Cooksville and Hospital (OPA No. 146).

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**Planning and Development Committee**

**Date:** May 9, 2022  
**Time:** 6:01 PM  
**Location:** Council Chambers, Civic Centre, 2nd Floor  
 300 City Centre Drive, Mississauga, Ontario, L5B 3C1  
 And Online Video Conference

Members Present	Councillor Stephen Dasko	Ward 1
	Councillor Chris Fonseca	Ward 3
	Councillor John Kovac	Ward 4
	Councillor Ron Starr	Ward 6 (Vice-Chair)
	Councillor Matt Mahoney	Ward 8
	Councillor Sue McFadden	Ward 10
	Councillor George Carlson	Ward 11 (Chair)
	Councillor Carolyn Parrish	Ward 5 (ex-officio)
	Councillor Pat Mullin	Ward 2 (ex-officio)
Members Absent	Mayor Bonnie Crombie	
	Councillor Dipika Damerla	Ward 7 (Leave of Absence)
	Councillor Pat Saito	Ward 9 (ex-officio)

---

**Staff Present**

Andrew Whittemore, Commissioner, Planning & Building  
 Shari Lichterman, Commissioner, Corporate Services & CFO  
 Chris Rouse, Director, Development & Design  
 Jason Bevan, Director, City Planning Strategies  
 Emma Calvert, Manager, Engineering and Construction  
 Graham Walsh, Deputy City Solicitor  
 Ryan Au, Traffic Planning Coordinator  
 Sangita Manandhar, Team Leader, Park Assets  
 Jeremy Blair, Manager, Transportation Infrastructure Management  
 Bashar Al-Hussaini, Planner  
 Christian Binette, Planner  
 Romas Juknevičius, Project Leader, Planning Programs  
 Luisa Galli, Manager, Planning  
 Sacha Smith, Manager, Legislative Services & Deputy Clerk  
 Megan Piercey, Legislative Coordinator

1. CALL TO ORDER - 6:01 PM

2. INDIGENOUS LAND STATEMENT

Councillor G. Carlson recited the Indigenous Land Statement.

3. DECLARATION OF CONFLICT OF INTEREST - Nil

4. MINUTES OF PREVIOUS MEETING - Nil

5. MATTERS CONSIDERED (AFTERNOON SESSION) - Nil

6. MATTERS CONSIDERED (EVENING SESSION)

6.1 PUBLIC MEETING INFORMATION REPORT (ALL WARDS)

City-Wide Major Transit Station Area Study - Draft Official Plan Amendment  
File: LA.07-CIT

Bashar Al-Hussaini, Planner provided an overview of the Draft Official Plan Amendment. Committee Members engaged in discussion regarding height requirements, the Ministry of Environments role, density targets, shadow impacts, and the floodplain in the Dixie and Dundas area at the Etobicoke boarder. Mr. Al-Hussaini and Jason Bevan, Director, City Planning Strategies responded to questions.

The following person spoke:

1. Peter Gross, Gowling WLG LLP requested a pre-consultation meeting with Ahmed Developments Inc. regarding the proposed 9 storey limit in this area before the Official Plan Amendment is approved.

Andrew, Whittemore, Commissioner, Planning and Building responded to questions. Councillor S. Dasko directed planning staff to have an informal discussion with Ahmed Developments Inc.

RECOMMENDATION PDC-0040-2022

Moved By Councillor S. Dasko

1. That the report titled “City-Wide Major Transit Station Area Study - Draft Official Plan Amendment” dated, April 14, 2022, from the Commissioner of Planning and Building, and any submissions received at the public meeting held on May 9, 2022, be received for information.
2. That 1 oral submission be received.

YES (9): Councillor S. Dasko, Councillor C. Fonseca, Councillor J. Kovac, Councillor R. Starr, Councillor M. Mahoney, Councillor S. McFadden, Councillor G. Carlson , Councillor C. Parrish, and Councillor P. Mullin

ABSENT (3): Mayor Crombie, Councillor D. Damerla, and Councillor P. Saito

Carried (9 to 0)

## 6.2 PUBLIC MEETING INFORMATION REPORT (WARDS 4 and 7)

Planning 15-Minute Cities: Downtown Fairview, Cooksville and Hospital Policy Review – Draft Official Plan Amendment and Built Form Standards

File: CD.03-DOW

Christian Binette, Planner provided an overview of the Draft Official Plan Amendment and Built Form Standards.

The following persons spoke:

1. Mary Ellen Bench, Dentons Canada LLP spoke on behalf of Bronte College with respect to the implications of the proposed draft official plan amendment. Ms. Bench noted her client would like to remain in this location, is supportive of the Residential High Density designation, and requested that the subject property have far greater height potential than the proposed maximum 12 storey height limit.
2. Larry Lindsay, Resident expressed concerns regarding the built form standards, transitional plane, height, shadowing, flooding and the impact to the existing character of Burdock Place.
3. Glen Broll, GSAI spoke on behalf of Equity Three Holdings Inc. and spoke to the built form standards and the 25 storey height restriction. Mr. Broll advised that his client currently has an application in for 35 storeys and noted concerns for the maximum height restriction, minimum 3 storeys of non-residential uses and whether new protected Major Transit Station Area policies will impact the processing of development applications currently under review.
4. Robin Vanderfleet, Resident spoke to the importance of ensuring affordable housing and inclusionary zoning.
5. Richard Paciorek, Resident expressed concerns regarding height, privacy, security, shadowing and flooding.

Councillor J. Kovac requested clarity on the City's plan for growth with the proposed policies. Mr. Binette, Andrew Whittemore, Commissioner, Planning & Building and Jason Bevan, Director, City Planning Strategies responded to questions.

### RECOMMENDATION PDC-0041-2022

Moved By Councillor C. Parrish

1. That the corporate report titled “Planning 15-Minute Cities: Downtown Fairview, Cooksville and Hospital Policy Review – Draft Official Plan Amendment and Built Form Standards” dated April 14, 2022, from the Commissioner of Planning and Building and any submissions made at the public meeting held on May 9, 2022, be received for information.
2. That 5 oral submissions be received.

YES (8): Councillor S. Dasko, Councillor C. Fonseca, Councillor J. Kovac, Councillor M. Mahoney, Councillor S. McFadden, Councillor G. Carlson , Councillor C. Parrish, and Councillor P. Mullin

ABSENT (4): Mayor Crombie, Councillor R. Starr, Councillor D. Damerla, and Councillor P. Saito

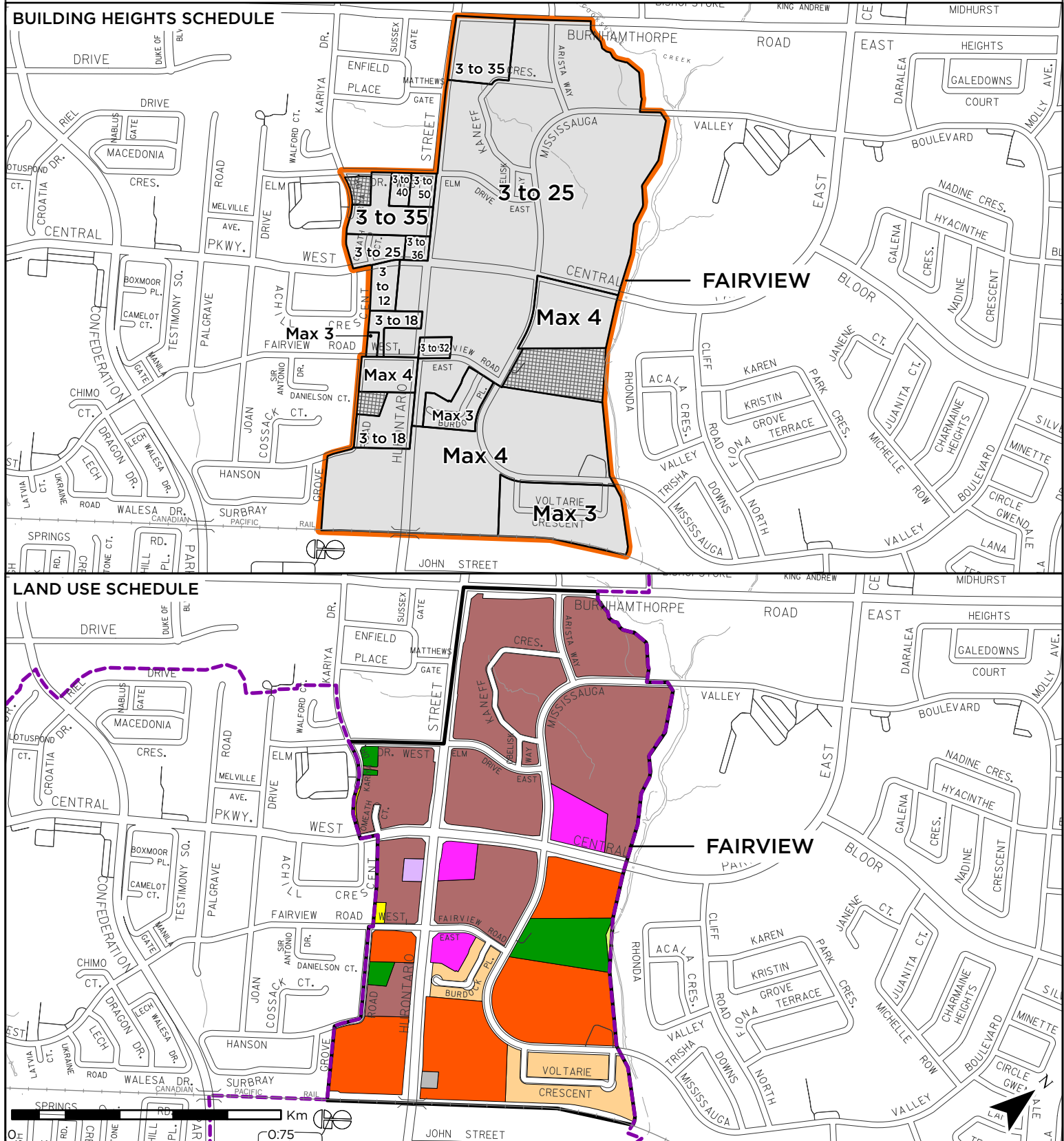
Carried (8 to 0)

7. ADJOURNMENT - 7:45 PM (Councillor C. Fonseca)



# PROTECTED MAJOR TRANSIT STATION AREA (PMTSA) 5.6

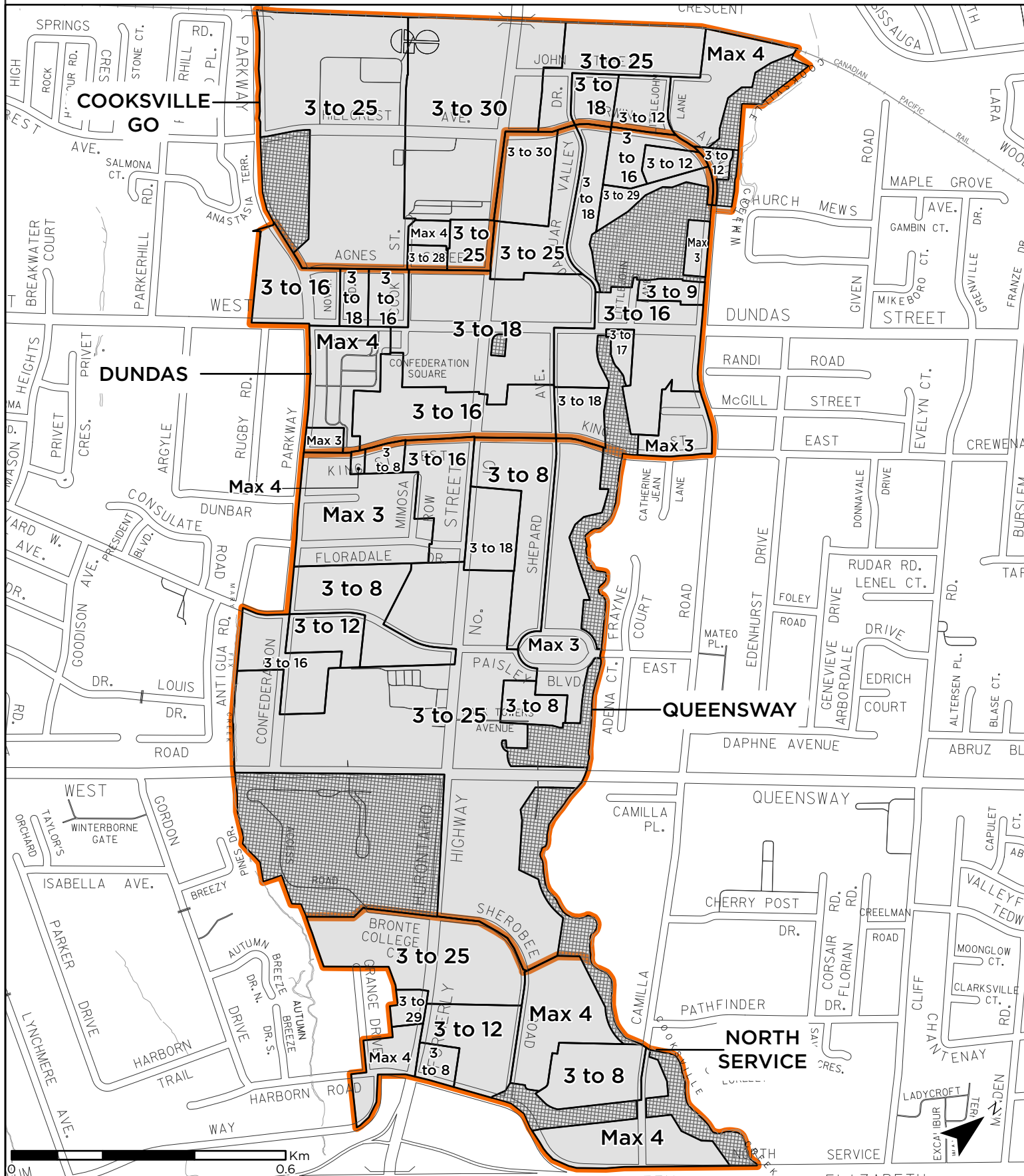
## (HURONTARIO LRT - FAIRVIEW)



## PROTECTED MAJOR TRANSIT STATION AREA (PMTSA) <sup>5.6</sup>

(HURONTARIO LRT - COOKSVILLE GO , DUNDAS, QUEENSWAY, NORTH SERVICE)

## BUILDING HEIGHTS SCHEDULE

 PMTSA Boundary (Height Map)

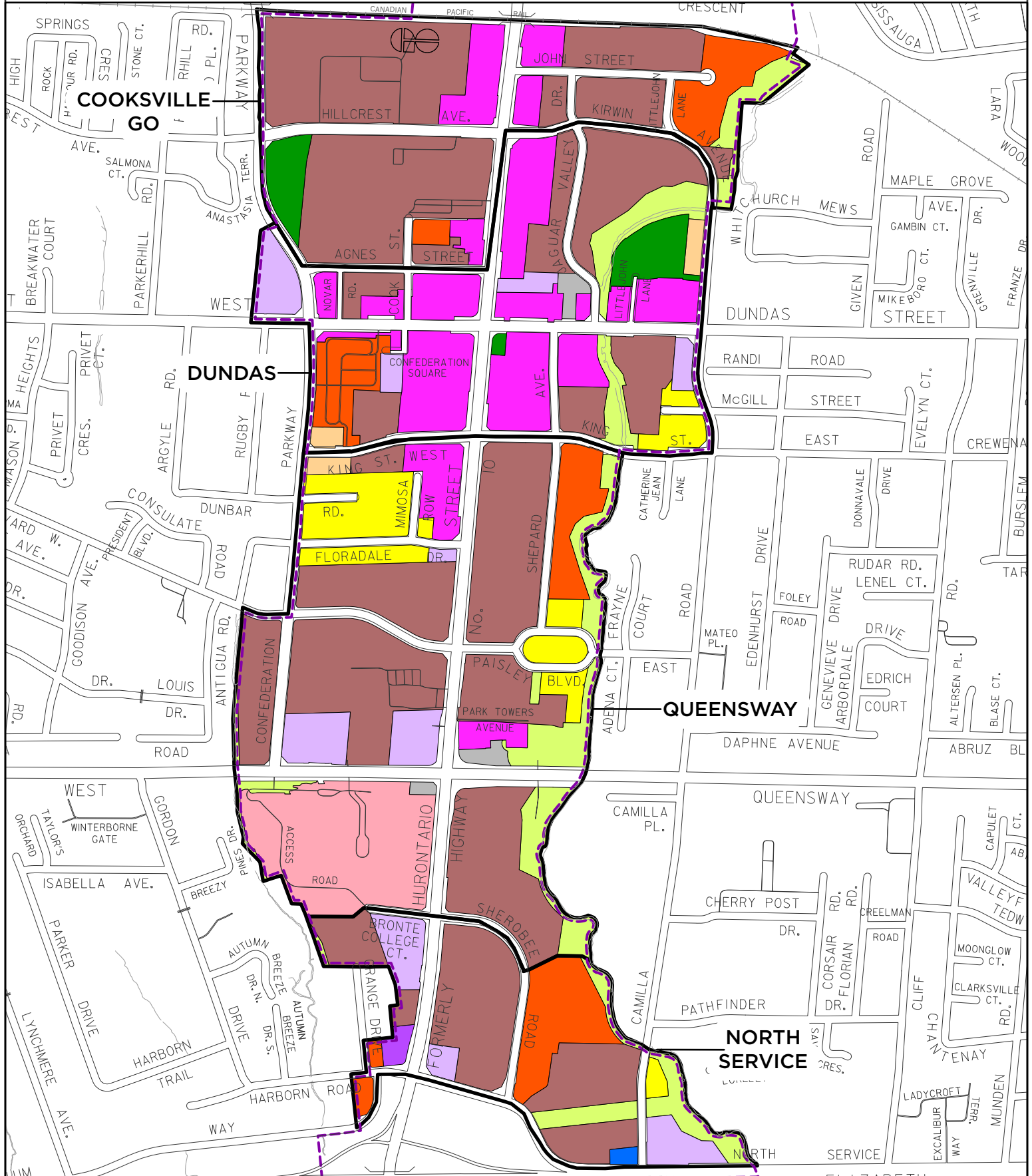
Min # to Max # (In Storeys)

 Height Not Applicable

## Schedule 11-L

# PROTECTED MAJOR TRANSIT STATION AREA (PMTSA) 5.6 (HURONTARIO LRT - COOKSVILLE GO , DUNDAS, QUEENSWAY, NORTH SERVICE)

## LAND USE SCHEDULE



**PMTSA Boundary (Land Use Map)**

- Convenience Commercial
- Greenlands
- Residential High Density
- Institutional

- Residential Low Density I
- Residential Low Density II
- Residential Medium Density
- Mixed Use
- Motor Vehicle Commercial

- Office
- Public Open Space
- Utility
- City Structure - Downtown

**Schedule 11-M**



Produced by Geospatial Solutions



# 12 Downtown

## 12.1 Introduction

### Notes:

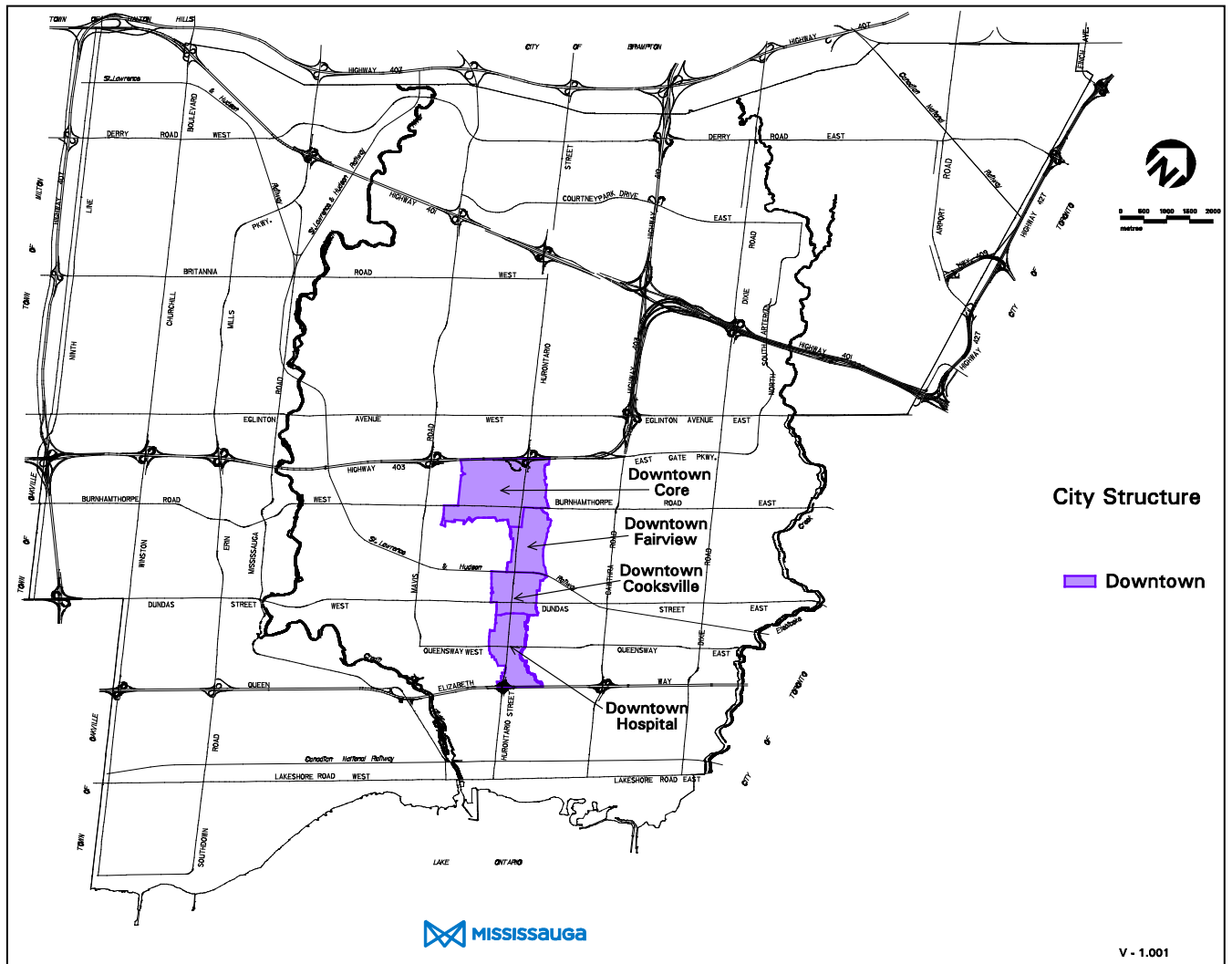
Existing policies are shown in the black text. Proposed new changes are shown in the red text. Proposed deletions to existing policies are shown as strike outs. Text and maps shown in a text box support development in Protected Major Transit Station Areas. Text highlighted in yellow indicate a change from the draft official plan amendment to the Planning and Development (PDC) on May 9, 2022.

The Downtown is comprised of four Character Areas:

- Downtown Core;
- Downtown Fairview;
- Downtown Cooksville; and
- Downtown Hospital.

This section identifies the modifications to the General Land Use designations in Chapter 11 that apply to the Downtown.

~~This section also includes policies that support development in Protected Major Transit Station Areas as identified in the Planning Act and identified in a blue box. Policies shown in a text box are Protected Major Transit Station Area policies in accordance with the Planning Act. The Protected~~



Map 12-1: City Structure – Downtown

**Major Transit Station Area** policies of this section will not come into force and effect until the Major Transit Station Area policies of the Region of Peel Official Plan adopted by Region of Peel By-law 20-2022 is approved by the Minister of Municipal Affairs and Housing, and Mississauga Official Plan Amendment No. 144 is in force and effect.

In the event of a conflict between the policies of the Region of Peel Official Plan as adopted by Region of Peel By-law 20-2022 and the policies of Mississauga Official Plan Amendment No. 146, the Region of Peel Official Plan policies will prevail.

The Downtown **comprises** **includes** 10 Protected **Major Transit Station Areas** as shown on Schedule 2: Intensification Areas and identified on Schedule 11: Protected Major Transit Station Areas.

- Burnhamthorpe;
- City Centre;
- Cooksville GO;
- Duke of York;
- Dundas;
- Fairview;
- Robert Speck;
- Main;
- North Service; and
- Queensway.

### 12.1.1 General

~~12.1.1.1 Proponents of development applications within the Downtown may be required to demonstrate how the new development contributes to the achievement of the residents and jobs density target and the population to employment ratio.~~

12.1.1.1 Proponents of development applications within the Downtown may be required to demonstrate how new development contributes to a concentration and mix of jobs as a key component of a mixed use transit-supportive development.

~~12.1.1.2 A minimum building height of three storeys is required on lands within the Downtown. Character Area policies may specify maximum building height requirements. Alternative minimum building heights may be established for existing areas with low density residential development.~~

~~12.1.1.3 Proposals for heights less than three storeys or different than established in the Character Area policies, will only be considered where it can be demonstrated to the City's satisfaction, that:~~

- ~~a. an appropriate transition in heights that respects the surrounding context will be achieved;~~
- ~~b. the development proposal enhances the existing or planned development;~~
- ~~c. the City Structure hierarchy is maintained; and~~
- ~~d. the development proposal is consistent with the policies of this Plan.~~

12.1.1.4 Lands immediately adjacent to, or within the Downtown, should provide both a transition between the higher density and height of development within the Downtown and lower density and height of development in the surrounding area.

~~12.1.1.5 Down zoning of lands within the Downtown and in the transition area adjacent to the Downtown should not be permitted.~~

12.1.1.6 Within the Downtown, on lands designated Mixed Use, Downtown Core Commercial, Convenience Commercial or Office, ground floor retail or office uses will be provided.

12.1.1.7 Prior to development within flood plains, the development proponent will assess impacts, identify flood management measures and mitigate flood hazards to the satisfaction of the City and the appropriate conservation authority.

## 12.1.2 Residential

12.1.2.1 The Residential Low Density I and Residential Low Density II designations will not be permitted, except for lands designated Low Density I and Residential Low Density II at the time this Plan comes into effect.

~~12.1.2.2 Notwithstanding the Residential High Density policies of this Plan, the maximum building height for lands designated Residential High Density will not exceed 25 storeys.~~

## 12.1.3 Mixed Use

12.1.3.1 Notwithstanding the Mixed Use policies of this Plan, the following additional use will be permitted:

- a. Major office.

12.1.3.2 Mixed Use development will be pedestrian oriented and street related.

12.1.3.3 Compatible development is encouraged that recognizes the scale and enhances the form and character of Mixed Use areas.

## 12.1.4 Office

12.1.4.1 Notwithstanding the Office policies of this Plan, the following additional use will be permitted:

- a. Post-secondary educational facility.

## 12.1.5 Motor Vehicle Commercial

12.1.5.1 The Motor Vehicle Commercial designation will not be permitted, except for lands designated Motor Vehicle Commercial at the time this Plan comes into effect.

## 12.1.6 Business Employment

12.1.6.1 The Business Employment designation will not be permitted.

## 12.1.7 Industrial

12.1.7.1 The Industrial designation will not be permitted.

## 12.1.8 Downtown Fairview, Cooksville and Hospital

The following additional policies apply to the Downtown Fairview, Downtown Cooksville and Downtown Hospital Character Areas.

### 12.1.8.1 General

12.1.8.1.1 Partnerships and collaborations will be encouraged to identify needs and develop community infrastructure such as integrating a school, a daycare facility or a community facility within a building with other uses.

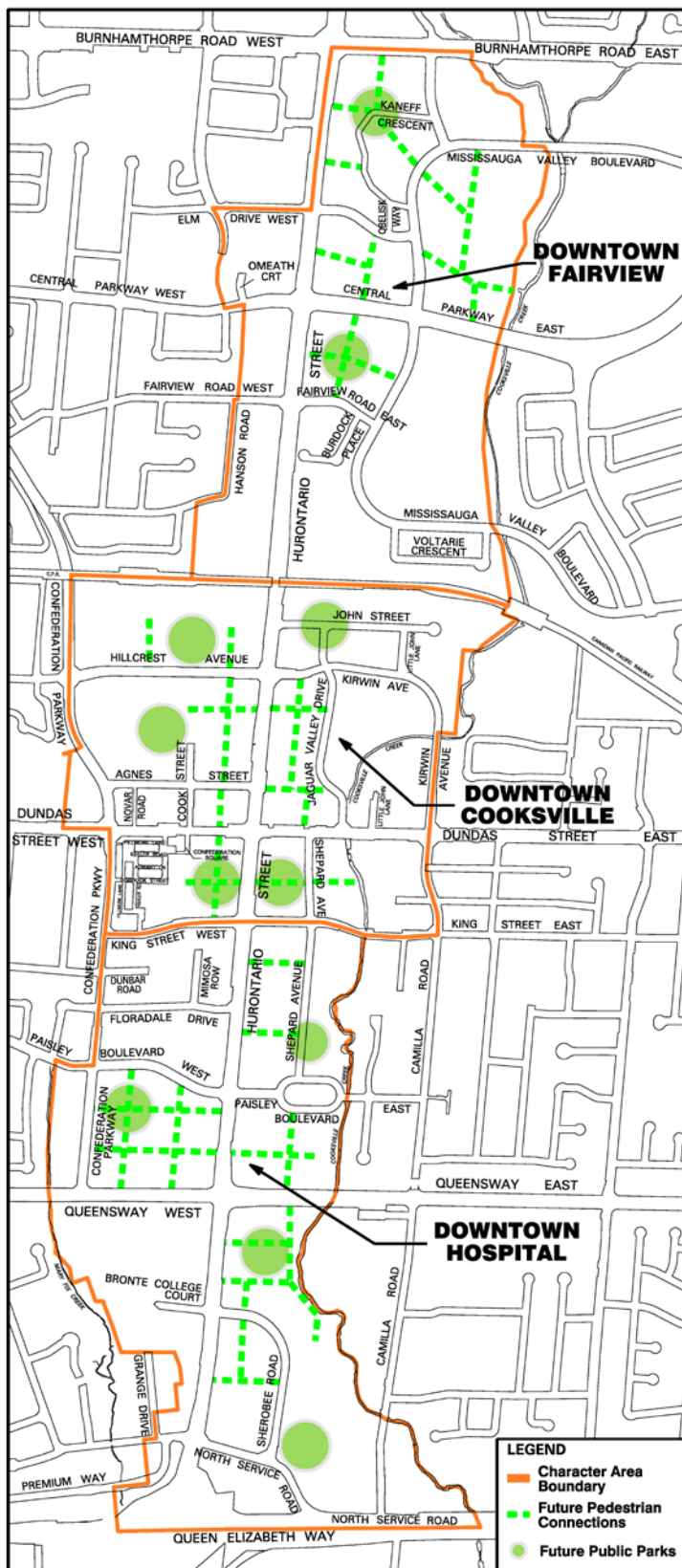
~~12.1.8.1.2 The City will work in collaboration with the school boards to determine the need for educational facilities. The location and size of a school facility will be confirmed through the review of development applications.~~

12.1.8.1.32 Downtown Fairview, Cooksville and Hospital will develop with a high quality, well-designed and expanded network of public parkland and open space that is green, safe, attractive, and supports a range of social and recreation activities. Parkland may also provide gathering spaces and connections, to existing and future parks and open spaces, commercial developments, community infrastructure and to surrounding neighbourhoods.

12.1.8.1.43 As part of the development application review process, the City will determine the location, configuration and size of new and/or expanded parkland blocks, and the requirement for land dedication. Opportunities to obtain parkland will also be explored through purchase by the City.

12.1.8.1.54 Through development applications, opportunities to increase the provision of open space will be explored. These open spaces will be complementary to the public parkland system.





**Figure 1:** Future pedestrian connections and public parkland network. These symbols represent the general location of new pedestrian connections, as well as public parkland in the vicinity.

12.1.8.1.65 Strategies to support the retention of local businesses and increase employment uses may be pursued including Community Improvement Plans and other incentives.

#### 12.1.8.2 Urban Form and Building Transition

12.1.8.2.1 New buildings will achieve a high quality urban design and built form, and will be designed and located to:

- create a transition in height generally consistent with a 45 degree angular plane that is measured from the property line adjacent to existing residential low and medium density residential areas housing forms land use designations;
- generally maintain a minimum separation distance, generally of 30 metres between portions of buildings that are greater than six storeys;
- add visual interest by varying the massing of buildings; and
- promote visibility and interest from the street through the use of high quality materials and architectural detailing in the design of the podium.

12.1.8.2.2 For larger developments where more than one tower is proposed, a variation in building heights of three to five storeys will be achieved.

12.1.8.2.3 **Tall buildings** will incorporate podiums that are a minimum of three storeys and a maximum of six storeys.

12.1.8.2.4 The floor plate of buildings above the podium will vary in size depending on the height of the building. For **tall buildings**, the tower above the podium will have a smaller floor plate size.

12.1.8.2.5 Infill development on existing sites will orient new buildings to face Hurontario Street, where possible.

Infill development on lands immediately adjacent to the Hurontario Street right-of-way will have buildings located along the street edge, where possible.

#### 12.1.8.2.6 Street Frontage Provisions

The majority of existing and new roads in Downtown Fairview, Downtown Cooksville and Downtown Hospital are categorized as 'A', 'B' or 'C' Streets and are shown on Map 12-1.2: Downtown Fairview, Cooksville and Hospital Street Types.

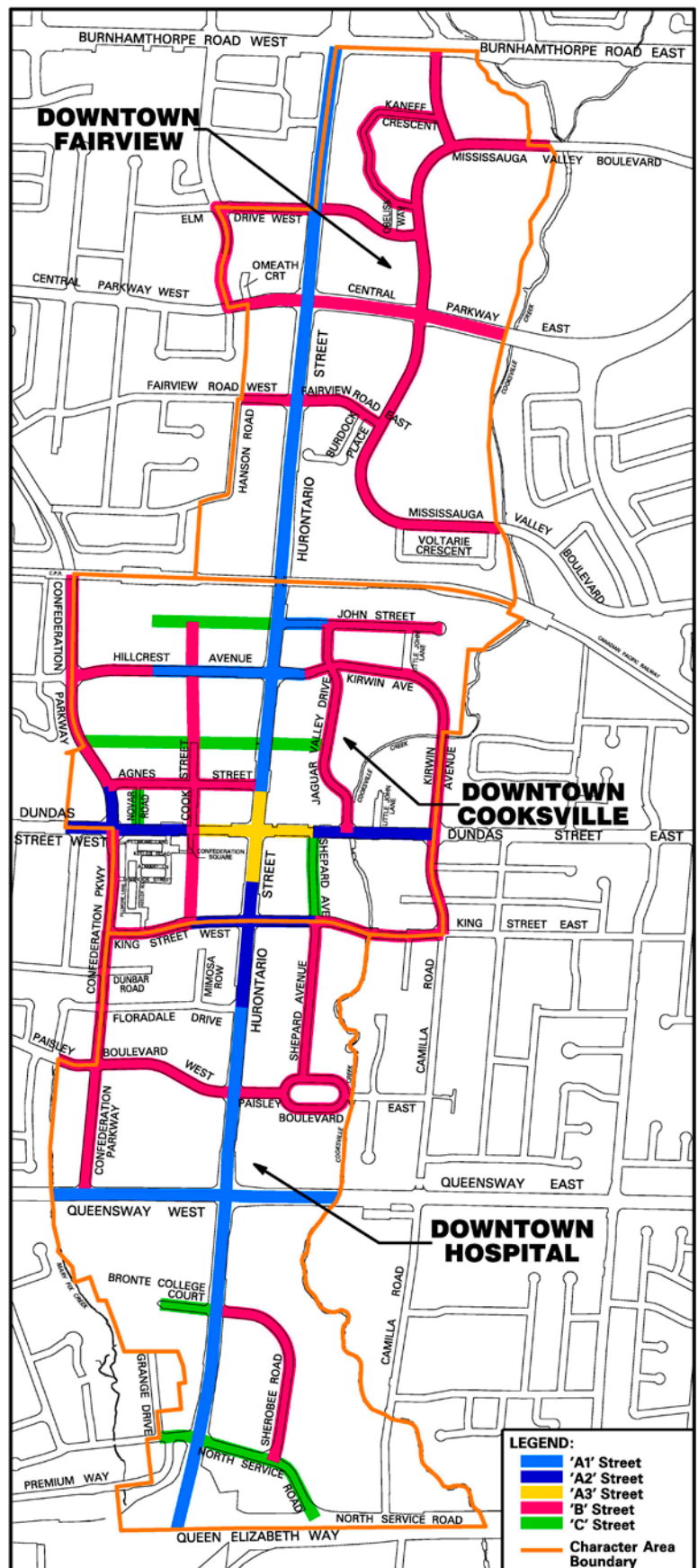
'A' Streets are considered primary arteries with buildings that integrate at grade retail, service commercial and other non residential uses. The built form and **streetscape** treatment of 'A' streets will be designed to foster active uses and a pedestrian friendly environment.

The built form along 'B' Streets will be primarily residential in character. The design of the built form and **streetscape** along 'B' streets will establish a unified character while ensuring adequate light and skyviews, and a seamless interface and transition between the public and private realms.

'C' Streets are tertiary streets that connect to 'A' or 'B' Streets. 'C' Streets are intended to support a pedestrian environment. These streets will also provide for vehicular access to on site service, loading, parking and garbage storage areas.

#### 12.1.8.2.7 Provisions for 'A' Streets

12.1.8.2.7.4.6 'A' 'A1', 'A2' and 'A3' Streets will have a strong sense of place and a main street, pedestrian friendly character with active, mixed use building frontages. Buildings fronting these streets will reinforce



Map 12-1.2: Downtown Fairview, Cooksville and Hospital Street Types

a distinct, high quality built form, public realm, landscaping and ~~related~~ pedestrian amenities.

12.1.8.2.7-2 Development abutting ~~the street~~ 'A1', 'A2' and 'A3' Streets will incorporate ground floor non-residential uses. Development will ~~address the following~~:

- a. promote a main street character with smaller retail units and frequent entrances accessible from the public sidewalk;
- b. frame and animate streets and public spaces, with active building frontages including storefronts, prominent entrances and residential lobbies linking the building and the sidewalk;
- c. ~~provide~~ a consistent streetwall with building indentations ~~will be provided~~ as visual relief;
- d. design non-residential units at street corner locations with animated frontages that wrap the corner;
- e. ~~provide~~ generous setbacks from the street to accommodate, among other things, landscaping, street furniture, wayfinding, bus shelters, pedestrian-scaled lighting and outdoor patios;
- f. ~~provide~~ appropriate setbacks between the edge of the podium and tower portion of the building; and
- g. ~~coordinate~~ private land abutting the sidewalk ~~should closely coordinate~~ with the public boulevard to create an integrated design character and reinforce the main street context. Opportunities for the development of entry forecourts and plazas will be encouraged in order to create a varied **streetscape**.

#### 12.1.8.2.8 Provisions for 'B' Streets

12.1.8.2.8-4 Development along 'B' Streets will primarily support housing and pedestrian access and movement. Although 'B' Streets are primarily residential in nature, development may include some non-residential uses on the ground floor. ~~The design~~

of the built form and **streetscape** along 'B' Streets will establish a unified character while ensuring adequate light and skyviews, and a seamless interface and transition between the public and private realms. Development will address the following:

- a. a consistent streetwall with building indentations will be provided as visual relief;
- b. generous setbacks from the street to accommodate, among other things, landscaping, terraces, bus shelters, and pedestrian scaled lighting;
- c. a high quality seamless interface and transition between the public sidewalk and ~~principle~~ **principal** building entrance to ensure adequate separation, definition and privacy; and
- d. appropriate setbacks between the edge of the podium and tower portion of the building.

#### 12.1.8.2.9 Provisions for 'C' Streets

12.1.8.2.9-1 Development along 'C' Streets will support a pedestrian environment and vehicular access to parking and other service areas such as deliveries, garbage pick-up, service and loading. 'C' Streets may also support residential, retail and service commercial uses. Development should address the following:

- a. opportunities to consolidate service, parking and loading to serve a number of buildings;
- b. ensure the visual impact of the parking, loading and service areas from the street is minimized and screened from view; and
- c. provide a secondary entrance for pedestrian access, where appropriate.

#### 12.1.8.3 Transportation

12.1.8.3.1 Development will be required to provide pedestrian connections to connect to the ~~Huronario Light Rail Transit~~ **light rail transit** (LRT) ~~line~~, transit routes/stops, trails, and parks and open spaces.

12.1.8.3.2 Public easements will be required where pedestrian connections are proposed on private lands.

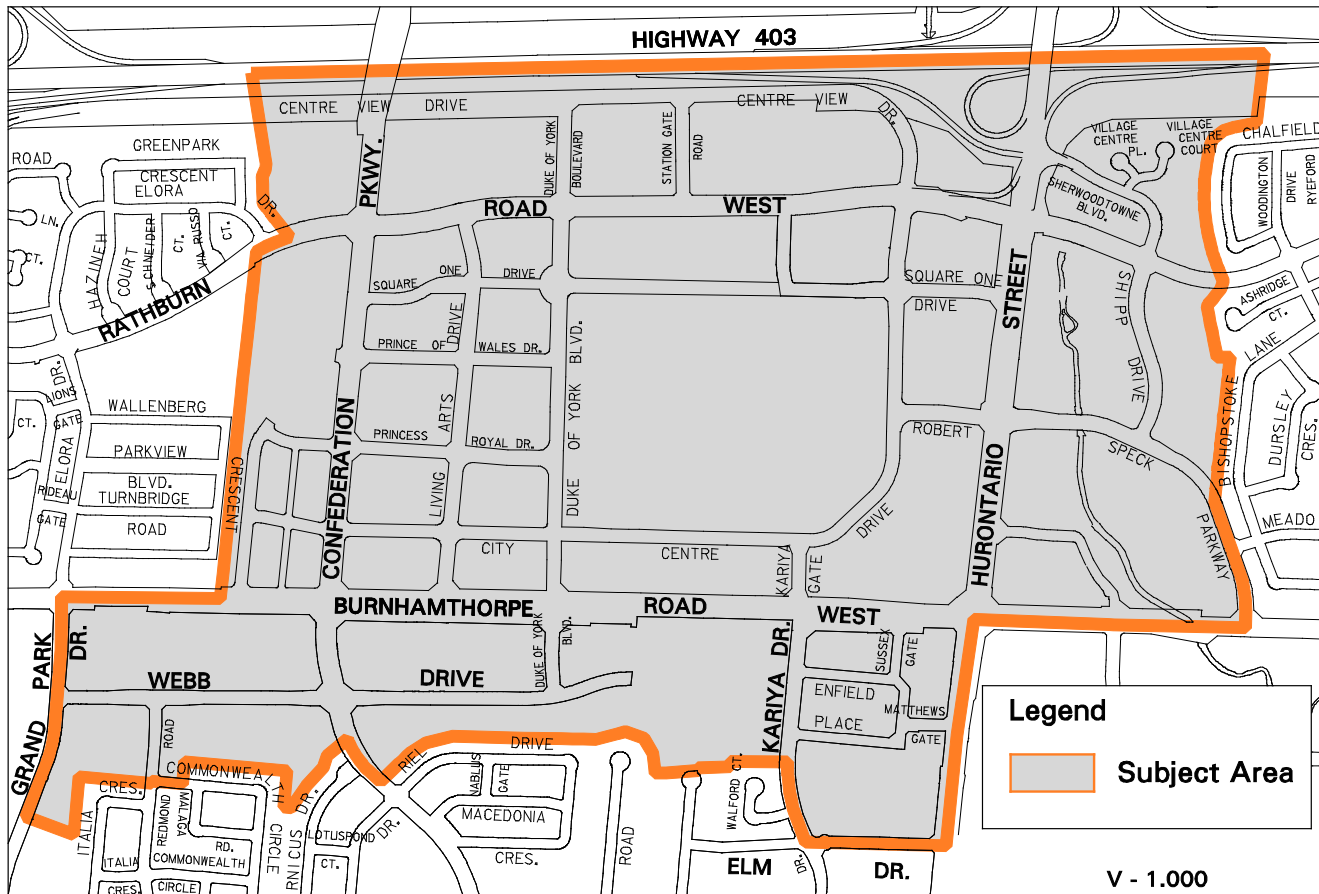
12.1.8.3.3 Vehicular access from Hurontario Street and Dundas Street will be restricted and consolidated to minimize driveways and conflict with pedestrians, cyclists and transit.

12.1.8.3.4 Roads will be designed as complete streets, and incorporate **active transportation** and transit infrastructure.

12.1.8.3.5 Existing arterial and collector roads bisecting and surrounding the Character Areas will incorporate **active transportation** and transit infrastructure when redesigned, as appropriate.

## 12.2 Downtown Core

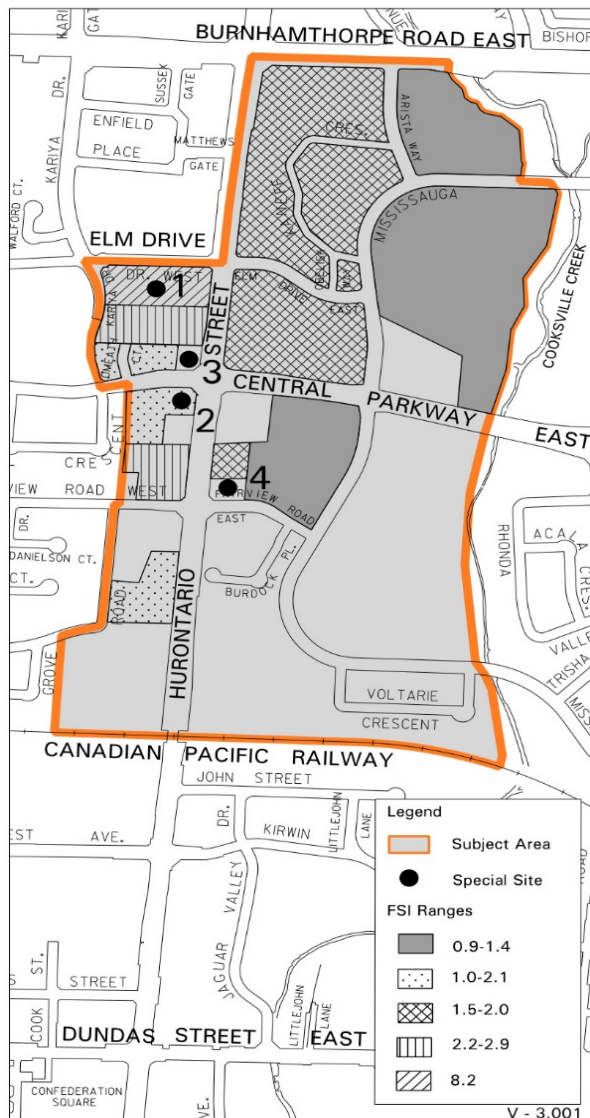
Official Plan policies for lands within the Downtown Core are contained in the Downtown Core Local Area Plan.



Map 12-2: Downtown Core Character Area



## 12.3 Downtown Fairview



Map 12-3: Downtown Fairview Character Area

Changes to Map 12-3:

- Delete FSI Ranges
- Delete Special Sites 1 to 4
- Add new Exempt Site – Exempt Site 1

### 12.3.1 Introduction

Downtown Fairview is situated on the periphery of Mississauga's Downtown Core and provides an area of transition to Downtown Cooksville. Downtown Fairview contains a number of sub-areas, each with its own unique character and built form.

Along Mississauga Valley Boulevard, there are 'tower in the park' developments, with generally 20 to 24 storey apartment buildings are surrounded by large open space areas with connections to trails and the Cooksville Creek. The area at Hurontario Street and Elm Drive West contains the greatest densities in Downtown Fairview, with point towers as tall as 50 storeys. The townhouses located in the southern end of Downtown Fairview are the predominant built form and provide housing for families.

There are limited retail and commercial uses along Hurontario Street, however with planned **higher order transit**, new developments can transform the street into vibrant places to shop, work and visit. Anchored by a grocery store, the existing retail site located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard provides convenient shopping for nearby residents.

The Cooksville Creek is a significant **natural heritage feature** that runs north-south in Downtown Fairview and drains into Lake Ontario. Opportunities to enhance the ecological features of existing natural habitats and the urban forest will be encouraged.

The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.

### 12.3.2 Vision

Downtown Fairview will be a distinct, walkable community that provides a transition to lower heights and densities from the Downtown Core. With more shops and amenities along Hurontario Street, Downtown Fairview will be an attractive place where people can access their daily needs within a short distance from their homes and places of work. Mixed use development located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard will serve the retail and commercial needs of the local community. The area along Mississauga Valley Boulevard will continue to be defined by existing towers in the park and beautiful green



spaces, with improved connections to trails and Cooksville Creek.

An expanded pedestrian network will connect new development with nearby amenities and residential areas, and improve access to transit, parks and open spaces.

The vision is based on the following guiding principles:

1. Heights and densities that are appropriate to the planned context. The greatest heights will be located at Hurontario Street and Elm Drive West, with a transition to lower building heights and densities for new developments south along Hurontario Street and east towards Mississauga Valley Boulevard;
2. An attractive, walkable environment along Hurontario Street with a vibrant mix of shops, restaurants, cafes and service establishments;
3. Accessible public spaces for all, with enhanced connections for walking and cycling; and
4. A mix of housing forms and tenure with a range of housing options that meet the needs of a diverse community.

### 12.3.3 Building Height

12.3.3.1 The greatest building heights will be located in the existing and planned buildings at Elm Drive West and Hurontario Street as shown on Schedule 11-K: Protected Major Transit Station Area Building Heights Schedule. Heights beyond this intersection should transition down to ensure the prominence of the Downtown Core

12.3.3.2 On lands designated Residential High Density and Mixed Use, the maximum permitted building height as shown on Schedule 11-K may be exceeded by up to three storeys without an amendment to this Plan, subject to meeting the building transition policies of this Plan, where a development provides additional non-residential uses, including community infrastructure. One additional storey in building height may be permitted for every 900 square metres of non-residential gross floor area (GFA) provided above the first storey. This does not include amenity space, above grade parking

or ground floor non-residential uses, where required by the policies of this Plan.

### 12.3.1 Land Use

12.3.1.1 For lands designated Residential Medium density, building height should not exceed three storeys.

### 12.3.4 Land Use

#### 12.3.4.1 Residential High Density

12.3.4.1.1 Notwithstanding the policies of this Plan, development along Hurontario Street will provide non-residential uses on the ground floor where the building is immediately adjacent to the ~~has frontage on~~ Hurontario Street ~~right-of-way~~.

#### 12.3.4.2 Mixed Use

12.3.4.2.1 Notwithstanding the policies of this Plan, development along Hurontario Street ~~will~~:

- a. ~~will~~ provide non-residential uses on the ground floor where the building is immediately adjacent to the ~~has frontage on~~ Hurontario Street; and
- b. may consist of primarily residential uses above the ground floor.

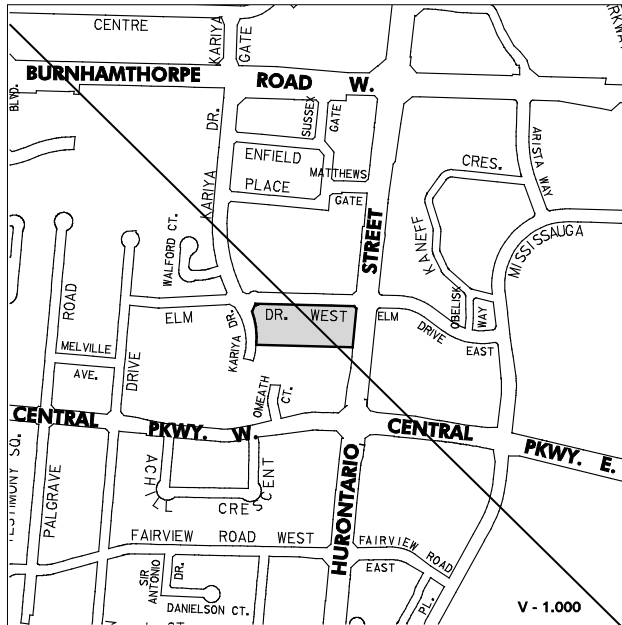
#### 12.3.4.3 Office

12.3.4.3.1 Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office space is retained or replaced through ~~new~~ development.

## 12.3.2 ~~Special Site Policies~~

There are sites within the Character Area that merit special attention and are subject to the following policies:

### 12.3.2.1 ~~Site 1~~

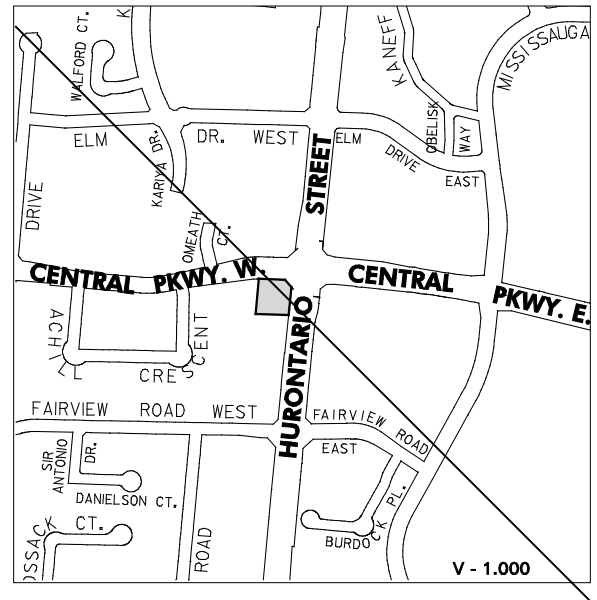


12.3.2.1.1 The lands identified as Special Site 1 are located on the west side of Hurontario Street, south of Elm Drive.

12.3.2.1.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. ~~three apartment buildings with maximum heights of 35, 40 and 50 storeys with a total maximum~~ **floor-space index (FSI)** of 8.2 will be permitted;
- b. ~~commercial uses will be permitted; and~~
- c. ~~access to the site will only be permitted from Kariya Drive and Elm Drive West.~~

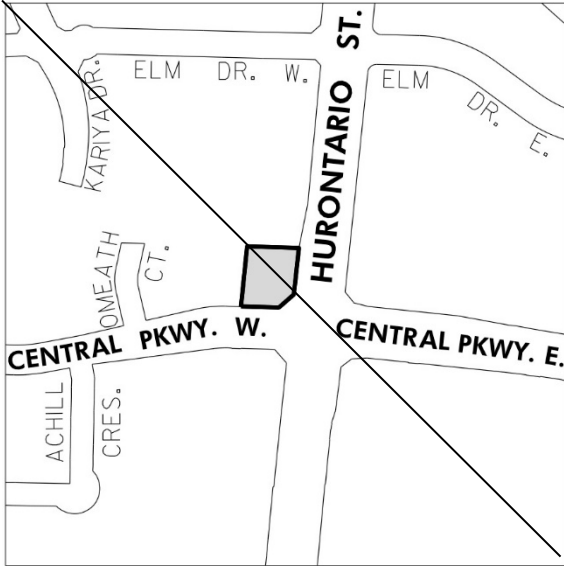
### 12.3.2.2 ~~Site 2~~



12.3.2.2.1 The lands identified as Special Site 2 are located on the west side of Hurontario Street, south of Central Parkway West.

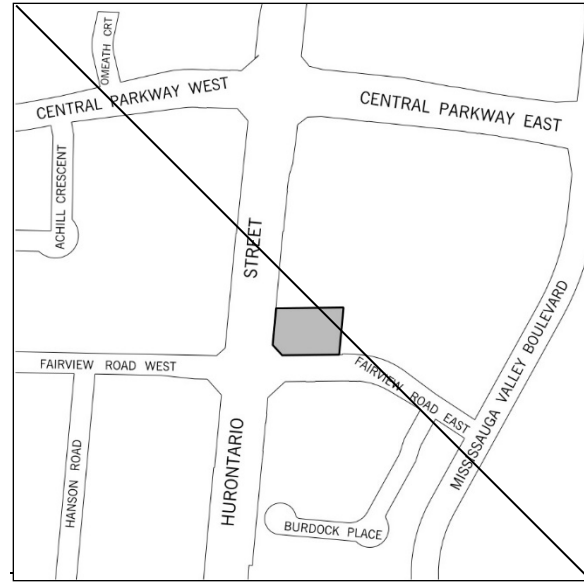
12.3.2.2.2 Notwithstanding the provisions of the Residential High Density designation and applicable policies, the following additional policies will apply:

- a. ~~the total number of units constructed on the site will not exceed 135; and~~
- b. a maximum **floor-space index (FSI)** of 3.75 will be permitted.

**12.3.2.3 Site 3**

~~12.3.2.3.1 The lands identified as Special Site 3 are located at the northwest corner of Central Parkway West and Hurontario Street.~~

~~12.3.2.3.2 Notwithstanding the policies of this Plan, an apartment dwelling with a maximum height of 36 storeys will be permitted.~~

**12.3.2.4 Site 4**

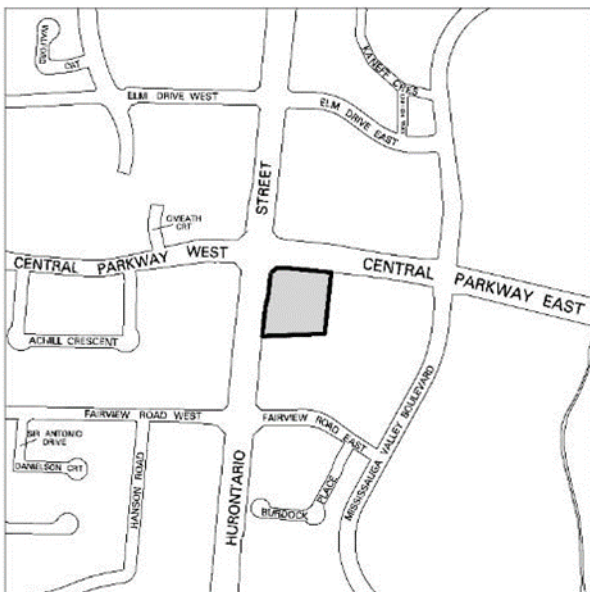
~~12.3.2.4.1 The lands identified as Special Site 4 are located at the northeast corner of Fairview Road East and Hurontario Street.~~

~~12.3.2.4.2 Notwithstanding the policies of this Plan, an apartment building with a maximum height of 32 storeys will be permitted.~~

### 12.3.5 Exempt Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

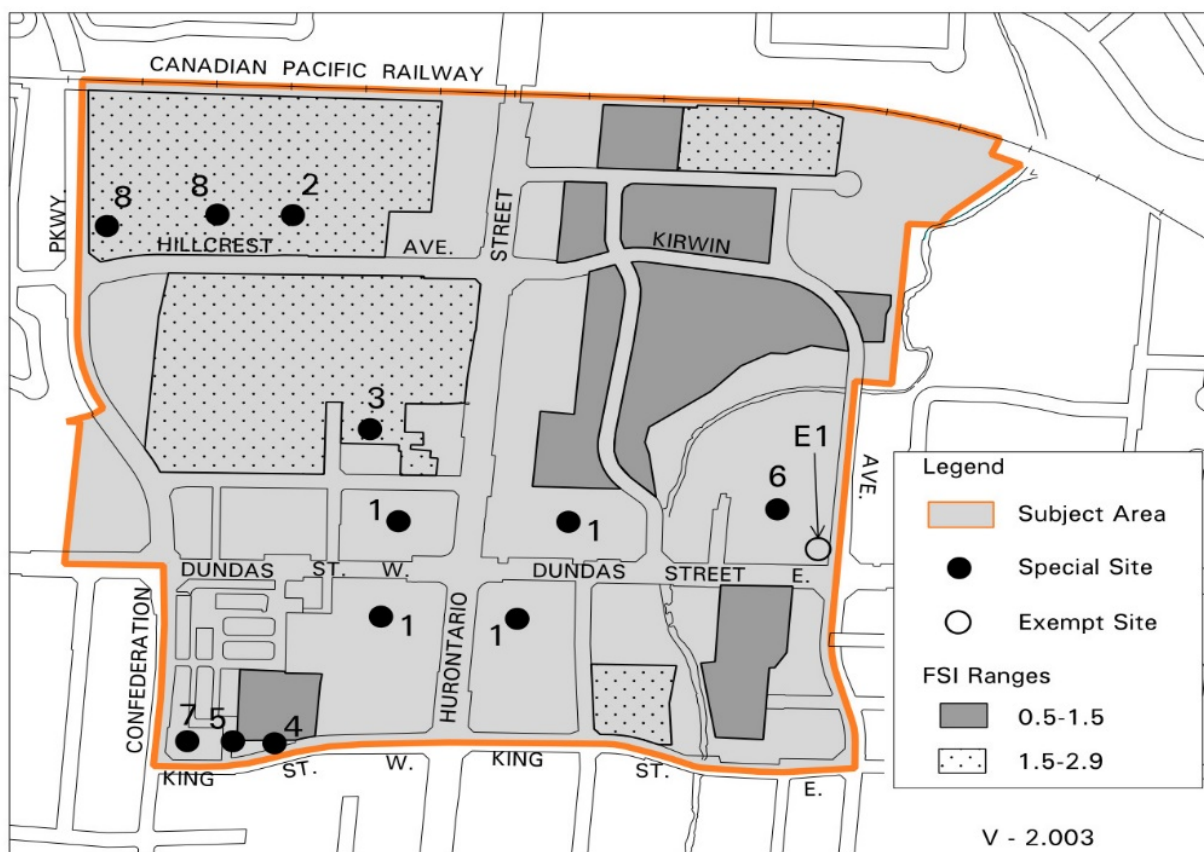
#### 12.3.5.1 Site 1



12.3.5.1.1 The lands identified as Exempt Site 1 are located on the southeast corner of Hurontario Street and Central Parkway East.

12.3.5.1.2 Notwithstanding the policies of this Plan, motor vehicle commercial uses will also be permitted.

## 12.4 Downtown Cooksville



Map 12-4: Downtown Cooksville Character Area

### Changes to Map 12-4:

- Delete FSI ranges
- Delete Special Site 2 and replace with a new Special Site 2
- Delete Special Sites 6 and 8

### 12.4.1 Introduction

Centred at Hurontario Street and Dundas Street, Downtown Cooksville has a rich history as a destination for newcomers to Canada and as a place for entrepreneurs and small businesses to set up shop and thrive. The cultural diversity of the neighbourhood is reflected in the variety of shops, restaurants and services that are available in the area. Immigrant programs and services in Downtown Cooksville provide an important function in helping families transition to a new country.

Hurontario Street and Dundas Street is generally characterized by retail plazas that are one to two storeys with residential and/or office uses above and surrounded with surface parking. There are a few

office and residential buildings along the **corridor** that are generally of a low or mid-rise form.

With the potential for two-way, all-day GO Transit service at Cooksville GO Station, planned Bus Rapid Transit (BRT) along Dundas Street and the Hurontario Light Rail Transit (LRT), significant transit investments will strengthen its connection to the rest of Mississauga and beyond. These transit investments will increase access to employment, shopping, entertainment and other destinations, while also attracting more visitors. Transit-supportive development will ensure transit investments are



supported with greater access to housing, jobs and services.

The Cooksville Creek is a significant **natural heritage feature** in Downtown Cooksville. Cooksville Creek serves as a wildlife corridor and linkage to other features within and beyond Downtown Cooksville. This corridor is also used for recreation with opportunities for restoration, rehabilitation and hazard mitigation along the **watercourse**.

The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.

### 12.4.2 Downtown Cooksville as a 15-Minute City

Downtown Cooksville will continue to reflect its welcoming and multicultural heritage while integrating key elements of a 15-minute city concept. Downtown Cooksville is walkable and liveable, where the basic needs of everyday life are accessible and can easily be reached within a 15-minute walk. It has amenities that serve the community – parks, groceries, jobs, medical clinics, pharmacies, schools, shops, restaurants, and more – that can be conveniently accessed without the use of a car.

### 12.4.3 Vision

Downtown Cooksville will be a walkable mixed use community with places for community amenities, culture and art. Hurontario and Dundas streets will be animated with storefronts and other active uses that support existing and future local businesses. Downtown Cooksville will continue to offer a diversity of local retailers and restaurants. The area immediately surrounding the intersection of Hurontario and Dundas Streets (Cooksville Corners) will be developed with a vibrant main street focus that includes a mix of uses and a human-scaled built form. The Cooksville GO Station area will be a focal point for transit-supportive **higher density** development with a concentration and mix of residential, community, office, retail and service commercial uses.

The vision is based on the following guiding principles:

1. Establish a mixed use, vibrant community – create a range of local and unique businesses along Hurontario and Dundas streets, spaces for educational, cultural and community uses, a vibrant public realm, and a network of connected parks and open spaces;
2. Plan for more people and employment – provide a range of housing options both in type and affordability, and jobs with the greatest heights and densities located in proximity to the Cooksville GO Station;
3. Achieve a walkable, connected community – promote a pedestrian-oriented main street character along Hurontario and Dundas streets, public art and a vibrant public realm, and improve pedestrian and cycling infrastructure; and
4. Plan for high quality transit – provide convenient and efficient transit with seamless connections to local and **higher order transit** services.

### 12.4.4 General

12.4.4.1 Arts and cultural uses are encouraged to concentrate in Downtown Cooksville to add vibrancy to the area.

### 12.4.5 Building Height

12.4.5.1 The greatest building heights will be located in proximity to the Cooksville GO station as shown on Schedule 11-L: Protected Major Transit Station Area **Building Heights Schedule**.

12.4.5.2 On lands designated Residential High Density and Mixed Use and located outside of Special Site 1 in Downtown Cooksville, the maximum permitted building height as shown on Schedule 11-L may be exceeded by up to three storeys without an amendment to this Plan, subject to meeting the building transition policies of this Plan, where a development provides additional non-residential uses, including community infrastructure. One additional storey in building height may be permitted for every 900 square metres of non-residential gross floor area (GFA) provided above the first storey. This does not include amenity space, above grade parking

or ground floor non-residential uses, where required by the policies of this Plan.

## 12.4.1 Urban Design Policies

### Overall Character

12.4.1.1 A high level of urban design, pedestrian amenity, and intensity of development is encouraged along principal street frontages. A sense of entry to the Character Area should be articulated at these locations by prominent built form, landscaping and signage components.

12.4.1.2 The sections of Hurontario Street and Dundas Street within the Character Area should function as a focus for the Cooksville Neighbourhood, having a strong sense of place and main street character with active mixed use building frontages and highly pedestrianized nature. These street frontages should reinforce a distinctive, quality image with high standards in built form, landscaping and related pedestrian amenities.

### Character of Dundas Street and Hurontario Street

12.4.1.3 Street Edge Uses Development abutting the street should encourage a high level of activity along the street by incorporating grade related retail with residential and/or offices above. Retail units should be clearly oriented to, and accessed from, the public sidewalk.

12.4.1.4 Street Scale and Enclosure Development should be closely related to, and integrated with, the public sidewalk to focus activity on the street and provide a sense of spatial enclosure for the street. Development should address the following:

- a. limited building setback range of three to five metres from the street line, with the larger setback in areas of high transit or pedestrian use;
- b. minimum building height of two to four storeys and maximum of six storeys directly abutting the street line;

e. maximum continuity of street walls with built form occupying a minimum of 80% of the street frontage; and

d. a minimum setback of ten metres from the street line is required for buildings exceeding six storeys in height.

12.4.1.5 Bulk and Massing Development should be generally consistent in its bulk, massing and scale within the Character Area with use of taller more prominent buildings to highlight the Dundas and Hurontario Street intersection. A general height progression should be encouraged from this intersection to the Character Area edges and abutting the Downtown Hospital Character Area. A ratio of 1:2 (building height to eventual street width) should be provided by built form abutting Dundas and Hurontario Streets.

12.4.1.6 Development Fabric Development fronting on the Hurontario Street and Dundas Street frontages should reinforce patterns and characteristics supportive of a main street role with highly animated pedestrian spaces. Development should address the following:

- a. commercial frontages should be broken up into smaller retail units and accessed directly from the public sidewalk with frequent access doorways;
- b. no parking should be provided between the buildings and street line;
- c. blank walls should be avoided along the street in favour of fenestration;
- d. service, loading and garbage storage areas should be accessed from rear lanes or abutting side streets;
- e. split level commercial frontages should be avoided;
- f. periodic building indentations should be provided as relief to long building walls and to provide opportunities for pedestrian spaces;
- g. storefront signage should respect the pedestrian scale and architectural character of development;

- ~~h. pedestrian weather protection should be provided in the form of canopies and/or inset arcades; and~~
- ~~i. the development and integration of rear yard parking lots to reduce the number of driveways along Hurontario Street and Dundas Street.~~

~~12.4.1.7 **Streetscape** (Open Space and Landscaping)  
Development of private land abutting the sidewalk should closely coordinate with the public boulevard to create an integrated design character at the pedestrian level and to reinforce the sense of a community main street. Opportunities for the development of entry forecourts and plazas will be encouraged in order to create a varied **streetscape**. Any existing vegetation will be considered for preservation through the redevelopment/infill process to enhance the liveability of a space.~~

~~12.4.1.8 Private Development Zone Development should address the following:~~

- ~~a. front yard setback areas should be provided with site furnishings, landscaping and paving materials, coordinate with the public boulevard design, and extend the usability of the public sidewalk; and~~
- ~~b. opportunities for pedestrian spaces and circulation areas should be promoted at intersection corners, major building entrances and terminations of vistas.~~

~~12.4.1.9 Public Realm: The Design of the Public Boulevard The following features should be encouraged to reduce the perceived visual width of the street and improve the level of pedestrian comfort, safety and convenience within the public boulevard:~~

- ~~a. common paving materials and patterns, street furniture and signage;~~
- ~~b. road crossings defined by special paving;~~
- ~~c. intersection design to moderate speed of turning traffic in favour of pedestrian movements, i.e. avoid the construction of designated right turn lanes with islands;~~

- ~~d. setting back of bus bays from intersection corners to allow sidewalks to project;~~
- ~~e. curb edge parking; and~~
- ~~f. provision of street trees, feature lighting and related pedestrian amenities.~~

## 12.4.2 ~~Transportation~~

~~12.4.2.1 All development applications are required to address the relationship with adjacent properties and the need to minimize access onto Hurontario Street and Dundas Street.~~

~~12.4.2.2 A future signalized intersection with Hurontario Street is proposed in the vicinity of the former Melissa Street right of way along the southerly boundary of T.L. Kennedy Secondary School. This will connect with Cook Street initially and, with the potential redevelopment of the school site, may extend to Confederation Parkway in conjunction with the possible extension of Cook Street northerly to the east GO Access Road. Redevelopment proposals in this area will consider these options.~~

~~12.4.2.3 The City of Mississauga Parking Strategy, carried out in 1993, made recommendations on the short, medium and long term strategies to meet the parking requirements and to achieve urban design objectives. These recommendations will be considered in conjunction with development applications and implementation of High Occupancy Vehicle (HOV) lanes on Hurontario Street and Dundas Street East.~~

## 12.4.6 ~~Transportation~~

~~12.4.6.1 The road network identified on Map 12-4.2 will support improved connectivity within Downtown Cooksville for pedestrian and cycling movement and to transit.~~

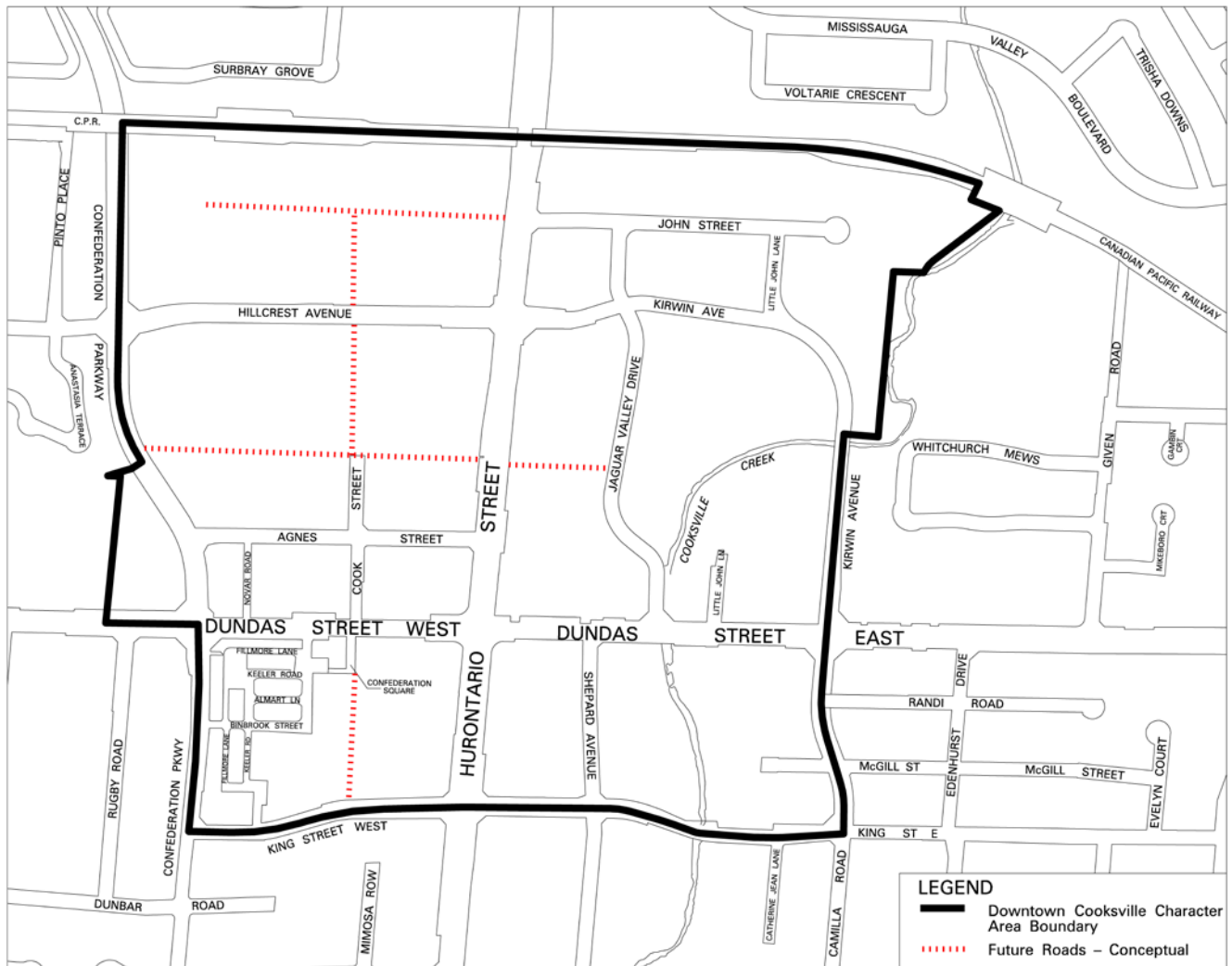
~~12.4.6.2 Improvements to the road network will be achieved through development. Future additions to the road network include, but are not limited to the following:~~

- ~~• a new east-west road from Confederation Parkway to Jaguar Valley Drive on the southern boundary of T.L. Kennedy Secondary School and former Melissa Street, aligning with a new road on the east side of Hurontario Street;~~

- extension of Cook Street **northerly** from the southern boundary of T.L. Kennedy Secondary School to connect with John Street; and
- extension of Cook Street **southerly** from Dundas Street West to connect with King Street West.

12.4.6.3 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. all roads will be public;
- b. the design and access requirements for roads will be determined through the development application review process; and
- c. minor adjustments to the road network may be made without an amendment to Map 12-4.2 at the City's discretion to accommodate development and break up large blocks with roads and pedestrian connections.



Map 12-4.2: Downtown Cooksville Character Area Future Roads

## 12.4.7 Land Use

### 12.4.7.1 Residential High Density

12.4.7.1.1 Notwithstanding the policies of this Plan, development will provide non-residential uses on the ground floor where the building ~~has frontage on~~ **is immediately adjacent to any of the following streets:** Hurontario Street, Dundas Street, Hillcrest Avenue (Cook Street to Hurontario Street) and King Street (new Cook Street to Shepard Avenue).

### 12.4.7.2 Mixed Use

12.4.7.2.1 Notwithstanding the policies of this Plan, development along Hurontario Street, Dundas Street, Hillcrest Avenue (Cook Street to Hurontario Street)

and King Street (new Cook Street to Shepard Avenue) **will:**

- will** provide non-residential uses on the ground floor where the building **is immediately adjacent to has frontage on** a street listed above; and
- may consist of primarily residential uses above the ground floor if not subject to a special site policy.

12.4.7.2.2 Notwithstanding the policies of this Plan, the following additional uses will be permitted:

- banquet hall;
- conference centre; and
- entertainment, recreation and sports facilities.



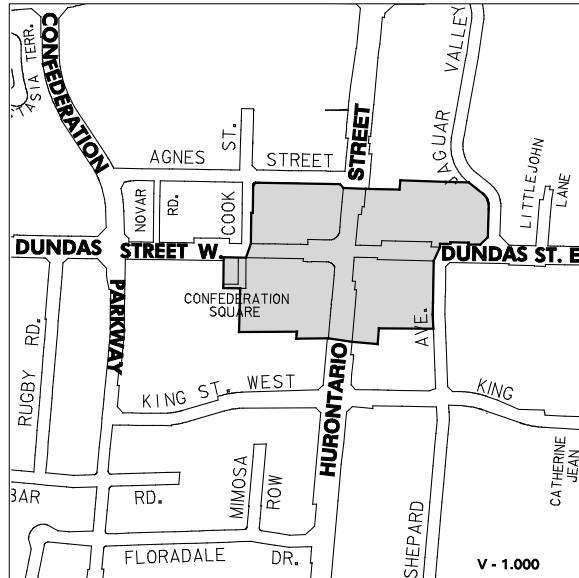
### 12.4.7.3 Office

12.4.7.3.1 Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office space is retained or replaced through new development.

### 12.4.38 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 12.4.38.1 Site 1



12.4.38.1.1 The lands identified as Special Site 1, known as Cooksville Corners, are the lands surrounding the intersection of Hurontario Street and Dundas Street.

12.4.38.1.2 Notwithstanding the provisions of the Mixed Use and Utility designations, the Urban Design Policies, and other provisions policies of this Plan, the following additional policies will apply:

- a. development up to a **floor space index (FSI)** of 1.0 will be permitted for street related retail, office, and community uses. Development between an **floor space index (FSI)** of 1.0 and 2.9 will be permitted at a ratio of 80% residential, office, community uses and overnight accommodation uses and 20% retail uses;

a. minimum building heights of three storeys and a maximum of 18 storeys directly abutting Dundas Street and Hurontario Street frontages to create an appropriate main street scale of development;

~~b. principal building entries and related forecourts are to be provided at the Dundas and Hurontario streets intersection. Dundas and Hurontario street frontages are to be developed with retail uses having direct access to the public street and continuous weather protection by an arcade and/or projecting awnings;~~

**b. *tall buildings* will incorporate podiums that are reflective of a main street character; buildings along Hurontario Street and Dundas Street will incorporate a generous setback between the edge of the podium and tower portion of the building;**

~~c. internal pedestrian courtyards with retail/restaurant uses will be permitted subject to: the provision of integral walkways to the public street; visibility from the sidewalk; and, such courtyards being clearly secondary to street related retail uses;~~

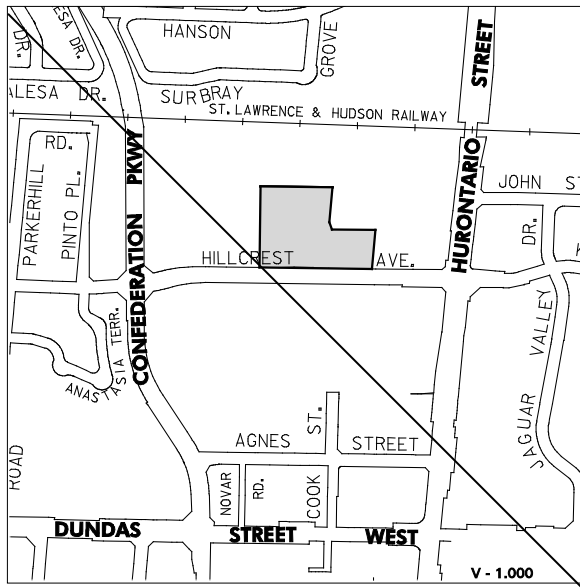
**c. to achieve vibrant walkable spaces, buildings along pedestrian connections will promote animation at-grade with outdoor patios and storefront entrances to individual retail and service commercial units; and**

~~d. minimum building heights of three storeys and a maximum of six storeys directly abutting Dundas and Hurontario street frontages are encouraged to create an appropriate street scale and enclosure relationship. Development exceeding six storeys to a maximum of 18 storeys will be considered subject to a minimum setback of ten metres from the building edge abutting the public sidewalk. Terracing of taller built form will be encouraged; and~~

**d. a minimum of three floors of non-residential uses will be required for buildings on lands designated Mixed Use or Residential High Density that are immediately adjacent to have frontage on Hurontario Street and Dundas Street.**

~~e. the lands located at the southeast corner of Agnes Street and Cook Street can be utilized on an interim basis for parking for the Mixed Use development to the west. When these lands are developed, they are to be in compliance with the Residential High Density designation. The majority of parking for the mixed retail and residential development shall be accommodated in a below grade parking structure.~~

### 12.4.3.2 Site 2

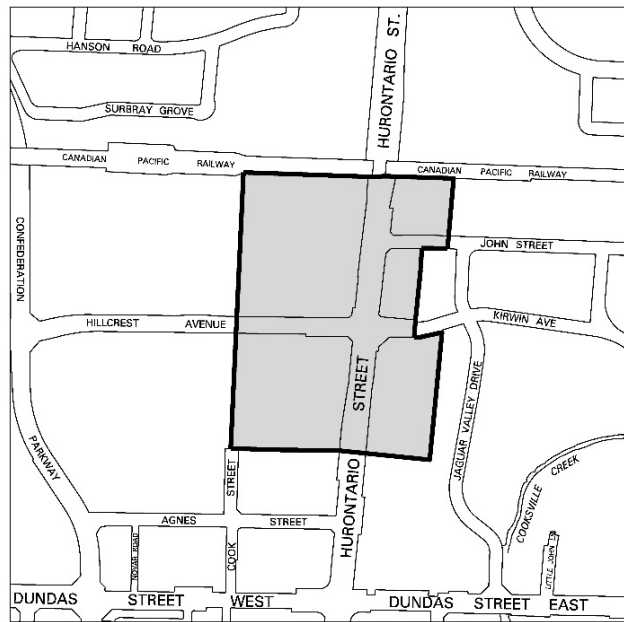


12.4.3.2.1 The lands identified as Special Site 2 are located on the north side of Hillcrest Avenue, west of Hurontario Street.

12.4.3.2.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:

- a. office development will be permitted at a maximum Gross Floor Area (GFA) of 61 439 m<sup>2</sup>, of which a maximum of 9 290 m<sup>2</sup> will be used for accessory uses; and
- b. apartments will be permitted at a maximum **floor space index (FSI)** of 1.5 – 2.9.

### 12.4.8.2 Site 2

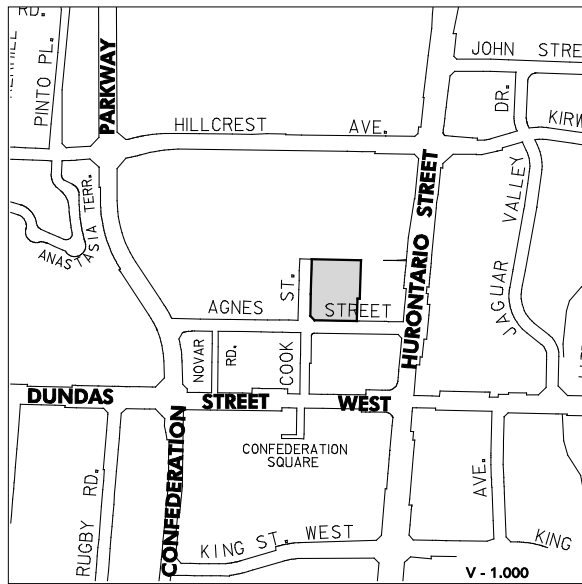


12.4.8.2.1 The lands identified as Special Site 2 are generally located on the east and west sides of Hurontario Street between the Canadian Pacific Railway and north of Agnes Street.

12.4.8.2.2 Notwithstanding the policies of this Plan, the following additional policy will apply:

- a. a minimum of three floors of non-residential uses will be required for buildings on lands designated Mixed Use or Residential High Density that have frontage on are immediately adjacent to Hillcrest Avenue and Hurontario Street.

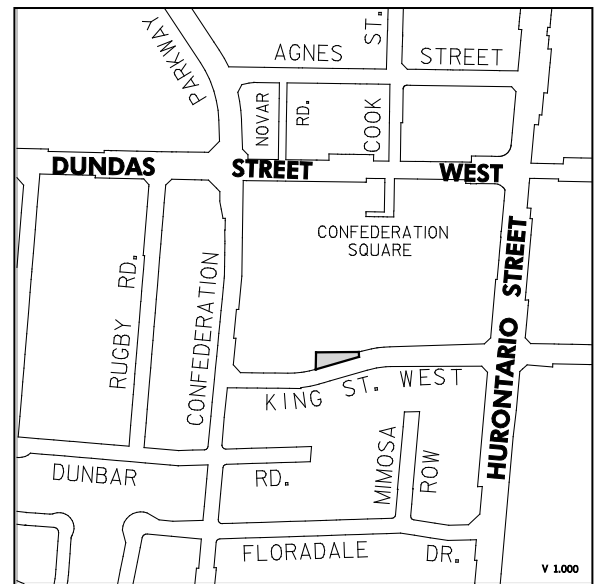
### 12.4.38.3. Site 3



12.4.38.3.1 The lands identified as Special Site 3 are located at the northeast corner of Agnes Street and Cook Street.

12.4.38.3.2 Notwithstanding the policies of this Plan, a maximum of 27 townhouse dwellings and a 28 storey apartment building will be permitted.

### 12.4.38.4. Site 4

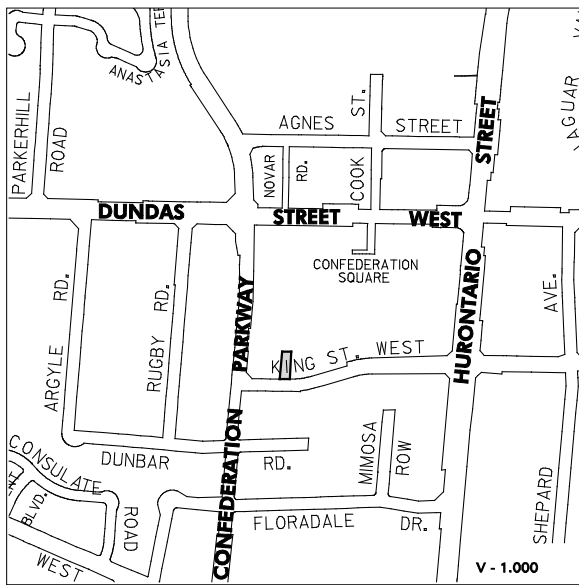


12.4.38.4.1 The lands identified as Special Site 4 are located on the north side of King Street West, east of Confederation Parkway.

12.4.38.4.2 Notwithstanding the provisions of this Plan, these lands must only be developed as part of a comprehensive development with adjacent lands designated Residential High Density.

## 12.4.3.5

## Site 5

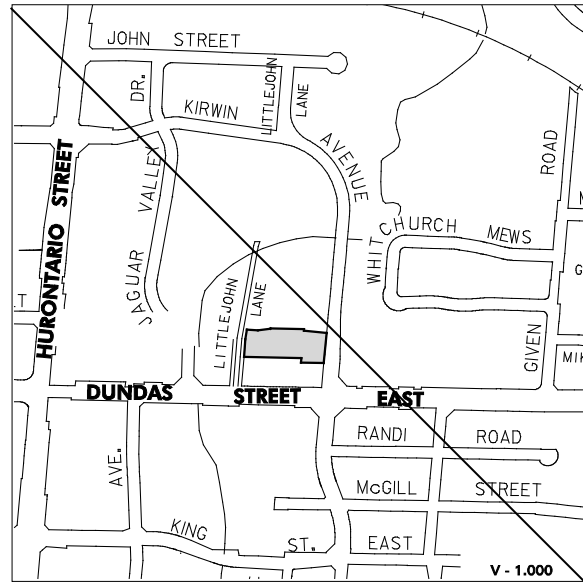


12.4.3.5.1 The lands identified as Special Site 5 are located on the north side of King Street West, east of Confederation Parkway.

12.4.3.5.2 Notwithstanding the provisions of the Residential Low Density II designation, offices will be permitted.

## 12.4.3.6

## Site 6



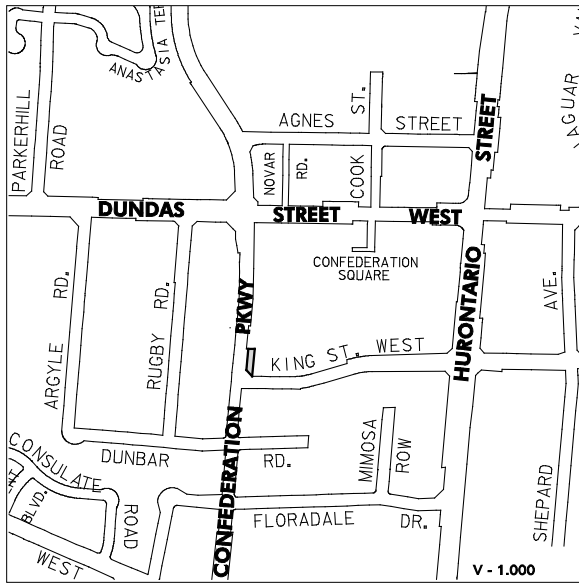
12.4.3.6.1 The lands identified as Special Site 6 are located on the west side of Kirwin Avenue, north of Dundas Street East.

12.4.3.6.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:

- a. a hotel not exceeding 42 storeys will be permitted.

## 12.4.3.8.7

## Site 7

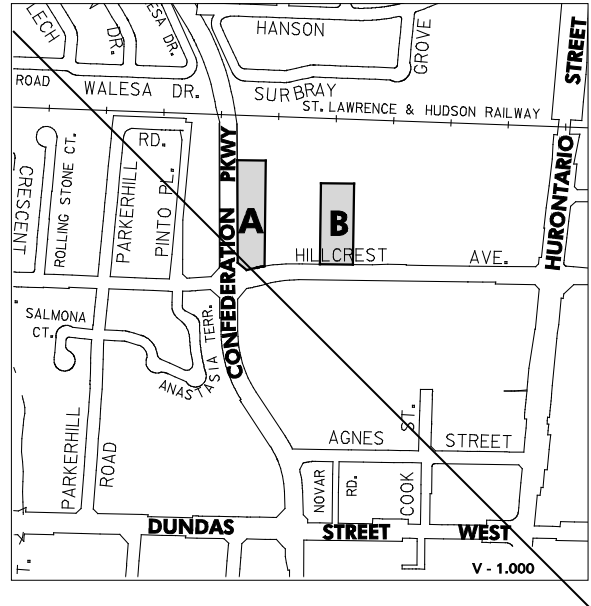


12.4.3.8.7.1 The lands identified as Special Site 7 are located on the northeast corner of King Street West and Confederation Parkway.

12.4.3.8.7.2 Notwithstanding the provisions of the Residential Low Density II designation, offices will be permitted.

## 12.4.3.8

## Site 8



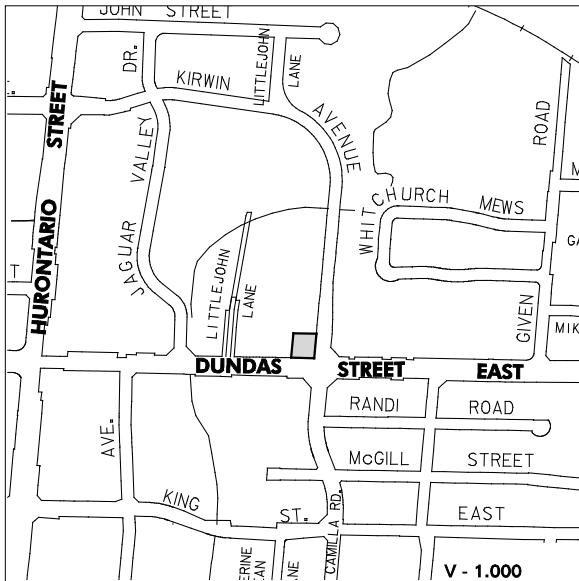
12.4.3.8.1 The lands identified as Special Site 8 are located on the north side of Hillcrest Avenue, east of Confederation Parkway.

12.4.3.8.2 Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 8A will be permitted to develop to a maximum **floor space index (FSI)** of 3.5 and the lands identified as Area 8B will be permitted to develop to a maximum **floor space index (FSI)** of 4.0.



## 12.4.49 Exempt Site Policies

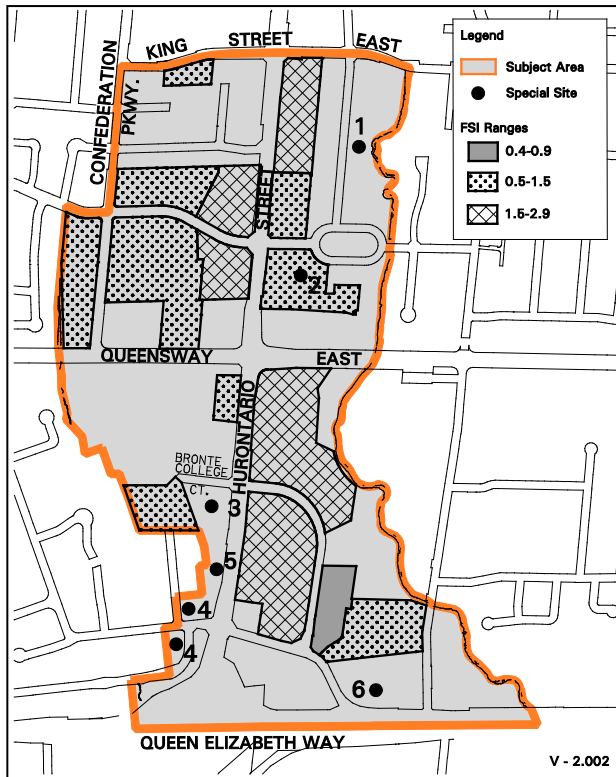
### 12.4.49.1 Site 1



12.4.49.1.1 The lands identified as Exempt Site 1 are located at the northwest corner of Dundas Street East and Kirwin Avenue.

12.4.49.1.2 Notwithstanding the provisions of the Mixed Use designation, motor vehicle commercial uses will also be permitted

## 12.5 Downtown Hospital



Map 12-5: Downtown Hospital Character Area

### Changes to Map 12-5:

- Delete FSI Ranges
- Delete Special Site 2 and replace with a new Special Site 2
- Delete Special Sites 3, 4 and 5

~~12.5.1.1 For lands designated Residential Medium Density, building height should not exceed a maximum of four storeys unless otherwise specified in the Urban Design policies of this Plan.~~

### 12.5.1 Introduction

The focal point of Downtown Hospital continues to be the Mississauga Hospital, serving as one of Canada's leading health centres with specialized regional programs. Downtown Hospital consists primarily of residential high density uses. Small pockets of low rise dwellings are located on the edges of the Character Area. Office uses are located in the area surrounding the hospital along Hurontario Street and Queensway. Mixed commercial uses are clustered at three main intersections along

Hurontario Street at King Street, Queensway and North Service Road.

Cooksville Creek is a significant **natural heritage feature** and portions of the creek have been impacted by channelization associated with flood and erosion control and storm water management. Enhancements to the ecological features of existing natural habitats and the urban forest will be encouraged.

The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.

### 12.5.2 Vision

Downtown Hospital will be developed as a health district, anchored by the hospital with a broad range of uses clustered around the Mississauga Hospital, supporting more retail and service commercial uses, research and development, health facilities, offices, parks and residential uses.

Transit-supportive development along Hurontario Street and Queensway **with maximum heights of 25 storeys for new residential development** will ensure greater access to housing, jobs and services.

The vision is based on the following guiding principles:

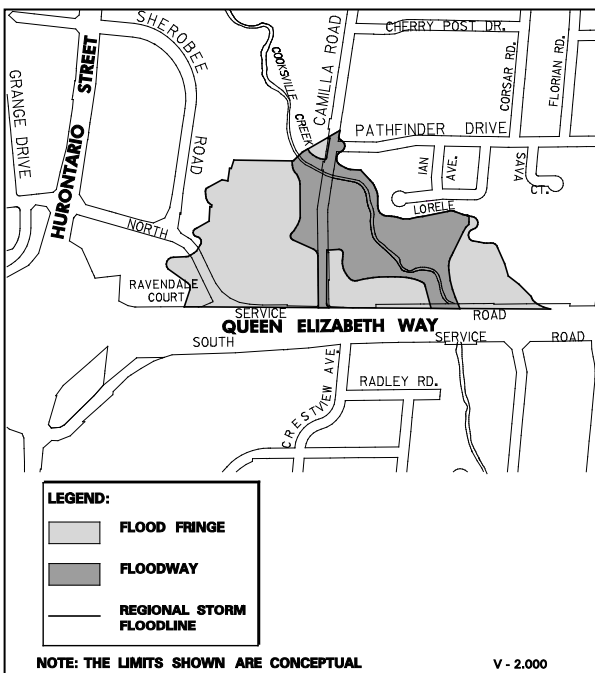
1. Support health care services – promote a range of health care services, supportive uses and research and development that create a healthier community; and
2. Create a more walkable and transit-supportive community – provide pedestrian connections to Hurontario Street and Queensway, and street related retail and service commercial uses along these corridors.

### 12.5.3 Building Height

12.5.3.1 The greatest building heights will be located at the transit stop at Hurontario Street and Queensway as shown on Schedule 11-L: Protected Major Transit Station Area **Building Heights Schedule**.

12.5.3.2 On lands designated Residential High Density and Mixed Use, the maximum permitted building height as shown on Schedule 11-L may be exceeded by up to three storeys without an amendment to this Plan, subject to meeting the building transition policies of this Plan, where a development provides additional non-residential uses, including community infrastructure. One additional storey in building height may be permitted for every 900 square metres of non-residential gross floor area (GFA) provided above the first storey. This does not include amenity space, above grade parking or ground floor non-residential uses, where required by the policies of this Plan.

### 12.5.24 Environmental Planning Area



Map 12-5.1: Cooksville Creek Floodplain Management Concept

12.5.24.1 The lands identified above are located generally between the North Service Road and where the Cooksville Creek crosses Camilla Road. The area subject to these policies within the Downtown

Hospital Character Area, is generally located west of Cooksville Creek. The lands shown are subject to the **two-zone floodplain management concept**, which divides the regulatory floodplain into two portions known as the floodway and the flood fringe. The limits of the flood fringe and the floodway are conceptual, the exact limits of which will be determined through further study.

12.5.24.2 Notwithstanding the Natural Hazards policies of this Plan, the following policies will apply to those lands within the regulatory floodplain:

~~a. the lands within the floodway are designated Residential High Density, Office, Greenlands and Utility. Notwithstanding the Residential High Density, Office, Greenlands and Utility policies of this Plan, the following uses will be permitted within the floodway subject to the satisfaction of the City and Credit Valley Conservation:~~

- ~~• flood and/or erosion works;~~
- ~~• facilities which by their nature must locate near water or traverse **watercourse** (i.e. bridges, storm sewer outlets and stormwater management facilities); and~~
- ~~• passive recreation activities; and~~

~~b. a. floodway lands will be zoned in an appropriate hazard category in the implementing Zoning By-law;~~

~~e. b. the lands within the flood fringe are subject to their respective land use designations and the following additional policies:~~

- development may be permitted provided the use, building or structure is flood proofed to the regulatory flood level as required by Credit Valley conservation **the conservation authority**;
- ingress/egress for all development located in the flood fringe will be such that emergency vehicular and pedestrian movement is not prevented during times of flooding in order that safe access/evacuation is ensured. The determination of safe access shall be made by ~~Credit Valley Conservation~~ **the**

conservation authority based on the depth and velocity factors;

- enclosed underground parking will be subject to the installation of stringent flood proofing measures to the elevation of the regulatory flood level; and
- the zoning of lands may utilize a holding zone to provide direction as to future permitted uses while ensuring provision to ensure flood proofing and safe access are addressed prior to development to the satisfaction of the City and Credit Valley conservation the conservation authority. The Zoning By-law will be amended to remove the holding symbol when the requirements for flood proofing, the provision of safe access to the proposed development and a detailed spill assessment and a financing agreement for the reconstruction of the culvert at the QEW has been completed to the satisfaction of the City, Credit Valley Conservation the conservation authority and the Ministry of Transportation; and

d. c. the following uses will not be allowed within the floodplain:

- institutional services such as hospitals, nursing homes, and schools where there would be a significant threat to the safety of inhabitants involved in an emergency evacuation situation as a result of flooding or failure of flood proofing measures;
- new uses associated with the manufacture, storage, disposal and/or consumption of hazardous substances or the treatment, collection and disposal of sewage, which would pose an unacceptable threat to public safety if they were to escape their normal containment/use as a result of flooding or failure of flood proofing measures; and
- emergency services such as those provided by fire, police, and ambulance stations and electrical substations, which would be impaired during a flood emergency as a result

of flooding or failure of flood proofing measures.

### 12.5.3 Transportation

~~12.5.3.1 The City of Mississauga Parking Strategy, carried out in 1993, made recommendations on the short, medium and long term strategies to meet the parking requirements and to achieve urban design objectives. These recommendations will be considered in conjunction with development applications and implementation of High Occupancy Vehicle (HOV) lanes on Hurontario Street and Dundas Street East.~~

## 12.5.5 Land Use

12.5.5.1 Development proposals in the vicinity of the Mississauga Hospital must demonstrate that new buildings and structures do not interfere with the flight path to the hospital heliport.

### 12.5.5.2 Residential High Density

12.5.5.2.1 Notwithstanding the policies of this Plan, development will provide non-residential uses on the ground floor where the building **has frontage on is immediately adjacent to any of the following streets:** Hurontario Street, King Street (new Cook Street to Hurontario) and Queensway.

### 12.5.5.3 Mixed Use

12.5.5.3.1 Notwithstanding the policies of this Plan, development along Hurontario Street, King Street (new Cook Street to Hurontario Street) and Queensway **will:**

- will** provide non-residential uses on the ground floor where the building **has frontage on is immediately adjacent to** a street listed above; and
- may consist of primarily residential uses above the ground floor if not subject to a special site policy.

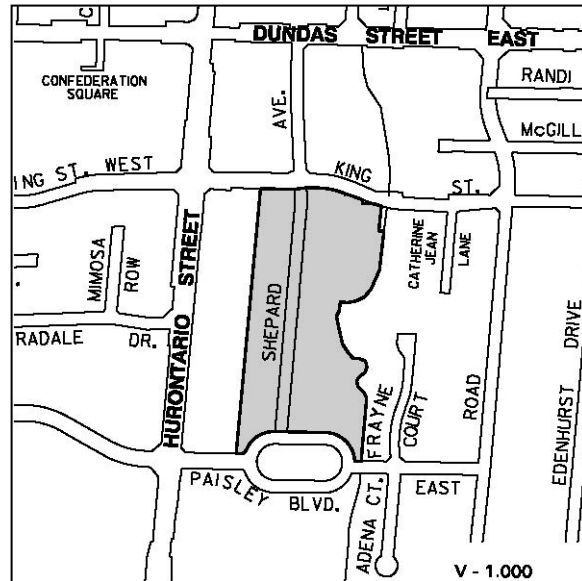
### 12.5.5.4 Office

12.5.5.4.1 Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office space is retained or replaced through **new** development.

## 12.5.4.6 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

### 12.5.4.6.1 Site 1



12.5.4.6.1.1 The lands identified as Special Site 1 are located on Shepard Avenue between King Street East and Paisley Boulevard East.

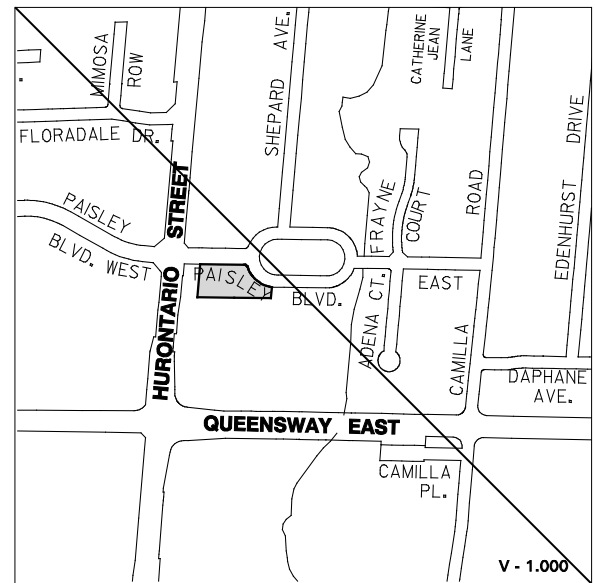
12.5.4.6.1.2 Notwithstanding the ~~provisions of the Residential Low Density I, Residential Medium Density and Greenlands designations~~ **policies of this Plan**, the following additional policies will apply:

- any proposal to alter the natural alignment of the Cooksville Creek and associated Greenlands lands through filling or channelization will require ~~the appropriate approvals from Credit Valley Conservation~~ **approvals from the appropriate conservation authority** and the City of Mississauga. The final design of such alteration works will address the following:
  - criteria for erosion measures; and
  - incorporation of the recommendations of the Cooksville Creek Rehabilitation Study for the reach of the Cooksville Creek between King Street East and Paisley Boulevard East; and

- b. in the event that structural management options are not feasible and, subject to a detailed planning design and supporting engineering studies for the Special Site 1 lands, a **two-zone floodplain management concept**, or combination of a **two-zone floodplain management concept** and structural management options, may be implemented;
- c. the limits of the development will be determined through detailed studies to address limits of flooding. Lands not suitable for development will be redesignated to Greenlands; and
- d. the preparation of a concept plan promoting the integrated redevelopment of the existing residential lots will be required in conjunction with any redevelopment applications. The concept plan will be required to address the following:
  - naturalization of the Cooksville Creek and retention of existing vegetation; **and**
  - an alternate street access and design to limit access to the lands from only King Street East; **and**
  - housing forms that provide for the stepping of densities and building heights from higher at the north and west ends of the site to lower at the south and east ends of the site.

***The policies in this section and the land use designation on Schedule 10 – Land Use Designations with respect to Site 1 are under appeal under OMB Case No. PL980724 (OPA 3).***

#### **12.5.4.2 Site 2**

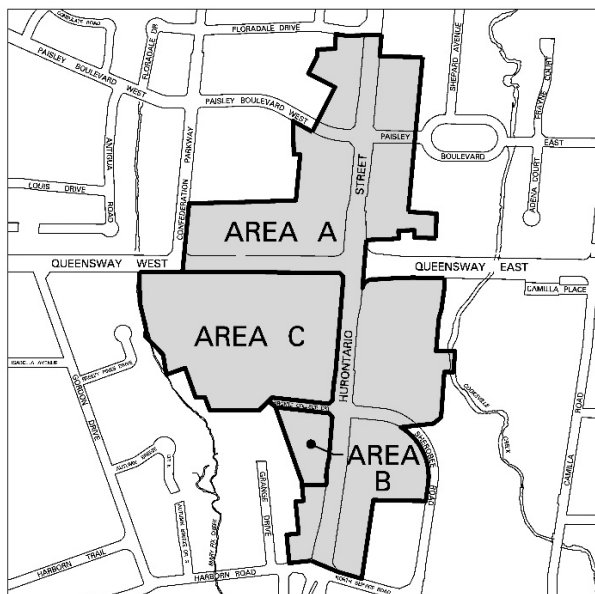


~~12.5.4.2.1 The lands identified as Special Site 2 are located on the south side of Paisley Boulevard, east of Hurontario Street.~~

~~12.5.4.2.2 Notwithstanding the provisions of the Residential High Density designation, apartments will be permitted at a maximum **floor space index (FSI)** of 0.5 – 1.5.~~



### 12.5.6.2 Site 2



- education and training facility;
- financial institution;
- major and **secondary office**;
- overnight accommodation;
- personal service establishment;
- research and development;
- restaurant;
- retail store;
- retirement building to a maximum building height of 25 storeys; and
- special needs housing, including a long-term care facility and a hospice to a maximum building height of 25 storeys.

12.5.6.2.1 The lands identified as Special Site 2 are generally located on the east and west sides of Hurontario Street between Floradale Drive and Harbourn Road/North Service Road and identified as a health district.

12.5.6.2.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

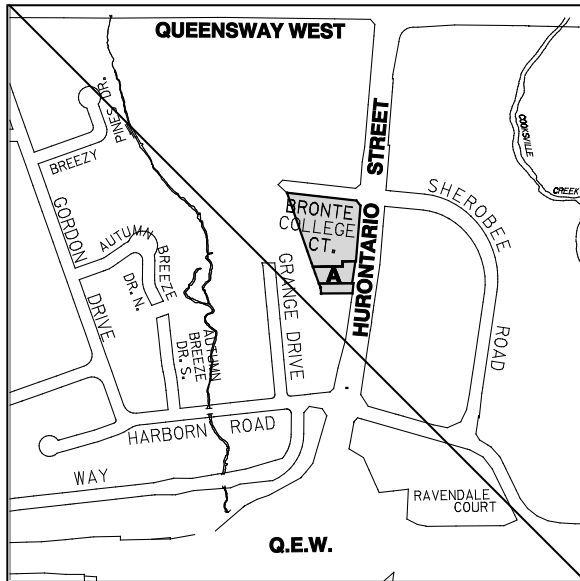
a. on lands identified as Area A and Area B a minimum of three floors of non-residential uses will be required for buildings on lands designated Mixed Use or Residential High Density that have frontage on Hurontario Street and Queensway West;

lands identified as Area A and Area B that are designated Mixed Use or Residential High Density will provide a minimum of three floors of non-residential uses in buildings immediately adjacent to Hurontario Street and Queensway West;

- b. lands identified as Area B will also permit funeral homes and nursing homes; and
- c. lands identified as Area C will provide a potential location for an urban plaza and will also permit the following uses:

- conference centre;

### 12.5.4.3 ~~Site 3~~



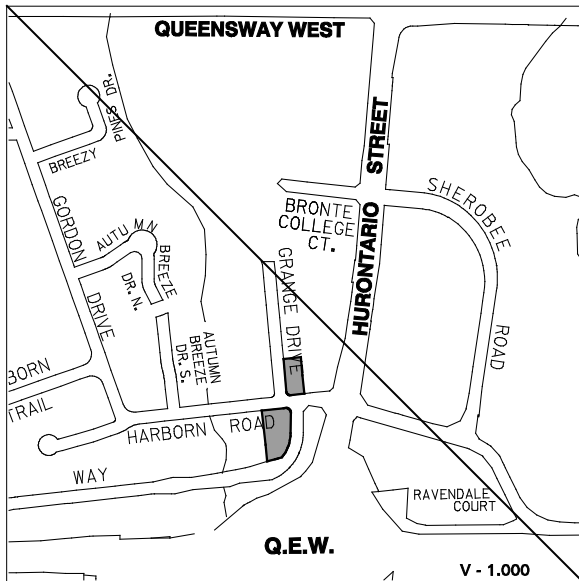
- f. ~~the lands identified as Area A, located at 2150 Hurontario Street, will be permitted to develop at a maximum **floor space index (FSI)** of 1.0.~~

~~12.5.4.3.1 The lands identified as Special Site 3 are located along the west side of Hurontario Street, north of Harbourn Road.~~

~~12.5.4.3.2 Notwithstanding the provisions of the Office designation, the following additional policies will apply:~~

- ~~a. funeral homes and nursing homes will also be permitted;~~
- ~~b. vehicular access and internal circulation and parking must respect both the visual and functional quality of the street as well as the privacy of the residential areas to the west;~~
- ~~c. efforts must be made to retain the existing vegetation cover maintaining the continuity of the area character;~~
- ~~d. building height and form should provide a positive scale transition between Hurontario Street and existing residential development to the west;~~
- ~~e. the building setback from the rear lot line should provide for the retention of natural vegetation to function as a buffer from residential uses to the west; and~~

#### 12.5.4.4 Site 4



12.5.4.4.1 The lands identified as Special Site 4 are located west of Hurontario Street, south of Queensway West.

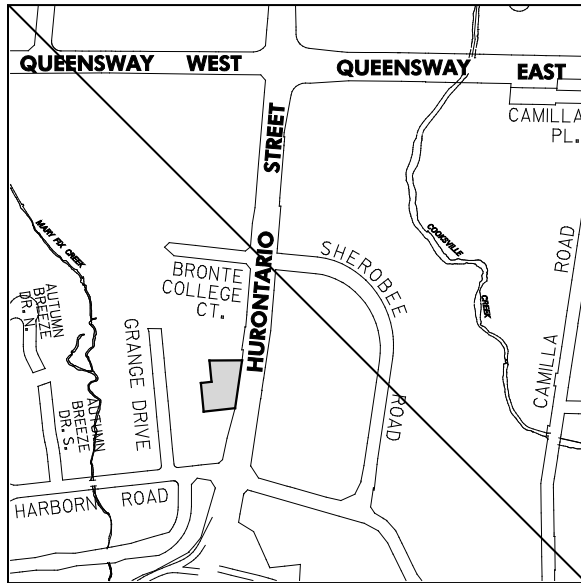
12.5.4.4.2 Notwithstanding the provisions of the Residential Low Density II designation on these lands, the following additional policies will apply:

- a. preserve and enhance the generous front, rear and side yard setbacks;
- b. ensure that existing grades and drainage conditions are preserved;
- c. encourage new housing to fit the scale and character of the surrounding areas, and take advantage of the features of a particular site, i.e. topography, contours, mature vegetation;
- d. garages should be recessed or located behind the main face of the house. Alternatively, garages should be located in the rear of the property;
- e. ensure that new development has minimal impact on its adjacent neighbours with respect to overshadowing and overlook;
- f. encourage buildings to be one to two storeys in height. The design of the building should de-emphasize the height of the house and be

designed as a composition of small architectural elements, i.e. projecting dormers and bay windows;

- g. reduce the hard surface areas in the front yard;
- h. preserve existing mature high quality trees to maintain the existing mature nature of these areas;
- i. house designs which fit with the scale and character of the local area, and take advantage of the particular site are encouraged. The use of standard, repeat designs is strongly discouraged;
- j. the building mass, side yards and rear yards should respect and relate to those of adjacent lots; and
- k. a maximum height of three storeys will be permitted for street townhouses.

## 12.5.4.5 Site 5

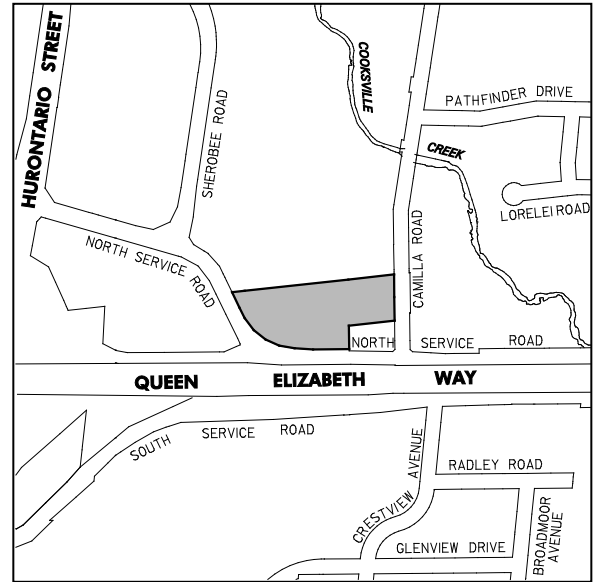


12.5.4.5.1 The lands identified as Special Site 5 are located on the west side of Hurontario Street, north of Harborn Road.

12.5.4.5.2 Notwithstanding the policies of this Plan the following additional policies will apply:

- a. ground level commercial uses will be permitted;
- b. office uses will be permitted; and,
- c. maximum of two towers, one with a maximum height of 26 storeys and the second with a maximum height of 29 storeys, will be permitted.

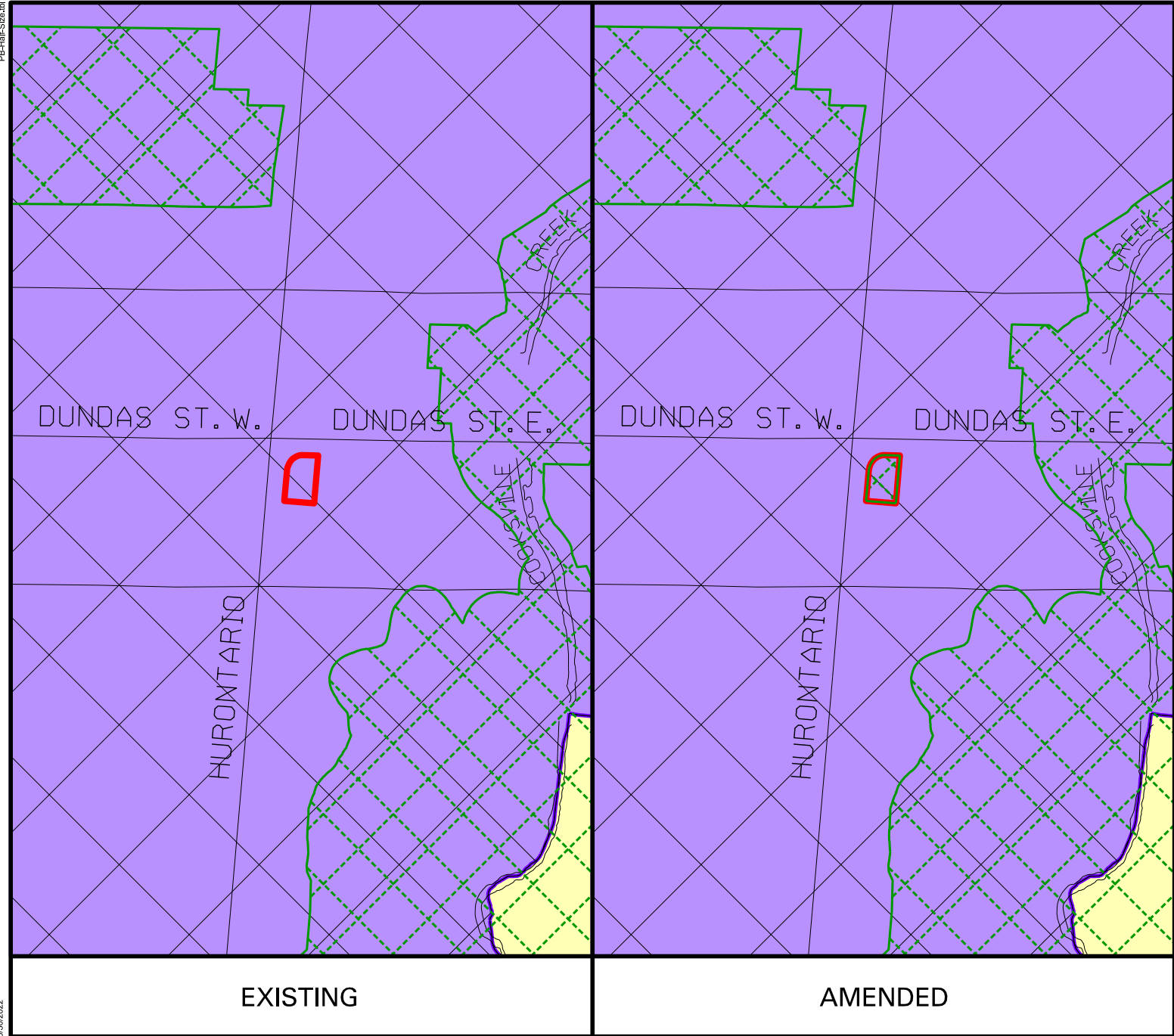
## 12.5.4.6 Site 6



12.5.4.6.1 The lands identified as Special Site 6 are located on the north side of North Service Road and west side of Camilla Road.


12.5.4.6.2 Notwithstanding the policies of this Plan, townhouses will be permitted.

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**LEGEND**


**GREEN SYSTEM**

 Green System

**CITY STRUCTURE**

 Downtown


 Major Node

 Community Node

 Neighbourhood


 Corporate Centre


 Employment Area

 Special Purpose Area

**CORRIDORS**

 Corridor

 Intensification Corridor

 AREA OF AMENDMENT

Note:  
Base map information (e.g. roads, highways, railways, watercourses),  
including any lands or bodies of water outside the city boundaries,  
is shown for information purposes only.



**MAP 'A'**  
Part of  
**Schedule 1 Urban System**  
of Mississauga Official Plan



**EXISTING**

**AMENDED**

5/30/2022

breric

LEGEND:

 AREA OF AMENDMENT

Note:  
Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.



MAP 'B'  
Part of Schedule 1a  
Urban System - Green System  
of Mississauga Official Plan



EXISTING

AMENDED

DUNDAS ST. W.

DUNDAS ST. E.

DUNDAS ST. W.

DUNDAS ST. E.

HURONTARIO

HURONTARIO

COOKSVILLE CREEK

COOKSVILLE CREEK



## LEGEND

-  Public and Private Open Spaces
-  Parkway Belt West
-  Educational Facilities
-  Utilities
-  AREA OF AMENDMENT

### Notes:

1. The entire Green System is shown on Schedule 1a.
2. Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.
3. The Public and Private Open Spaces identified on this Schedule include lands designated Public Open Space, Private Open Space and Greenlands as shown on Schedule 10.



## MAP 'C'

Part of Schedule 4  
Parks and Open Spaces  
of Mississauga Official Plan



EXISTING

AMENDED

HURONTARIO

HURONTARIO

COOKSVILLE

COOKSVILLE

CREEK

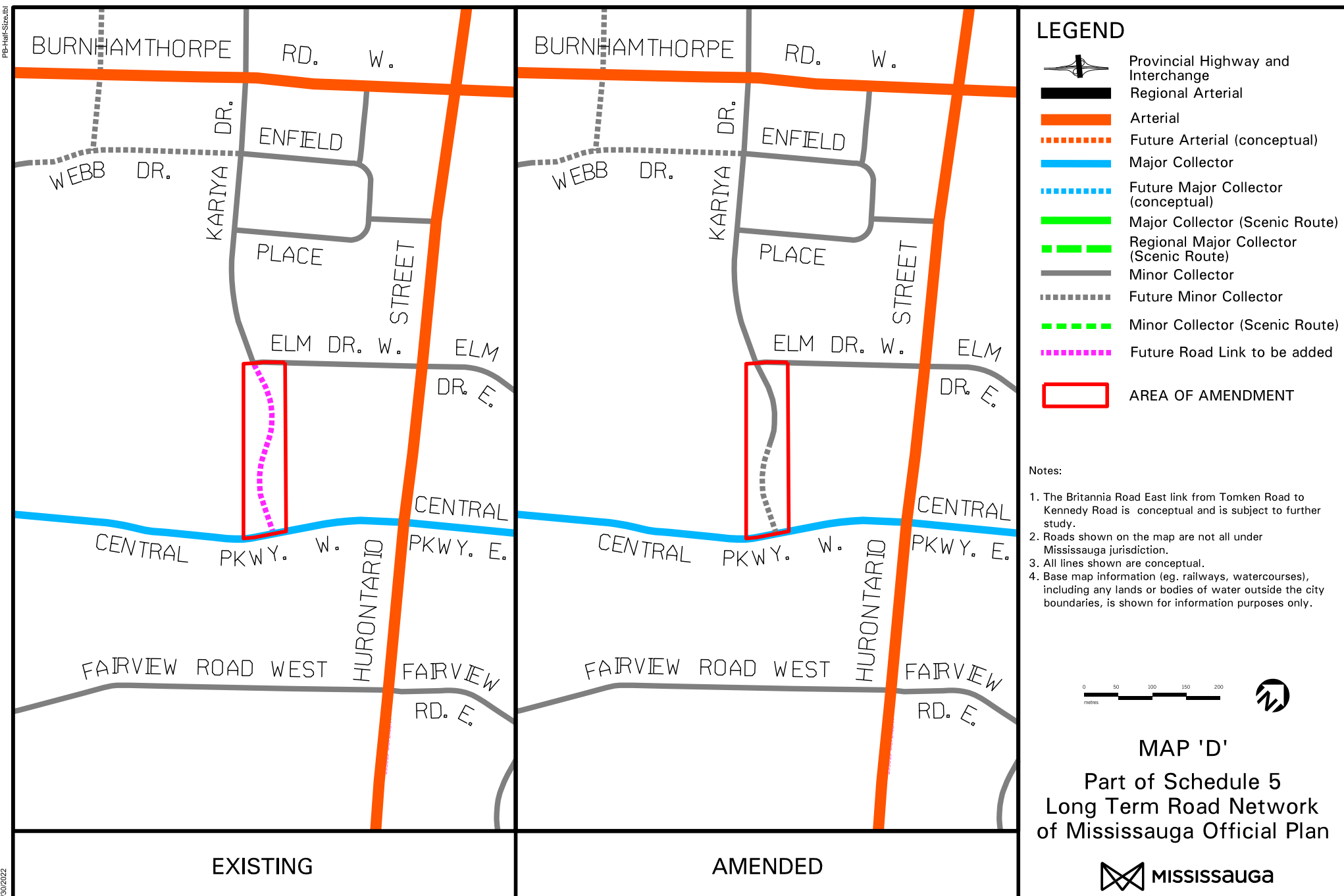
CREEK

DUNDAS ST. E.

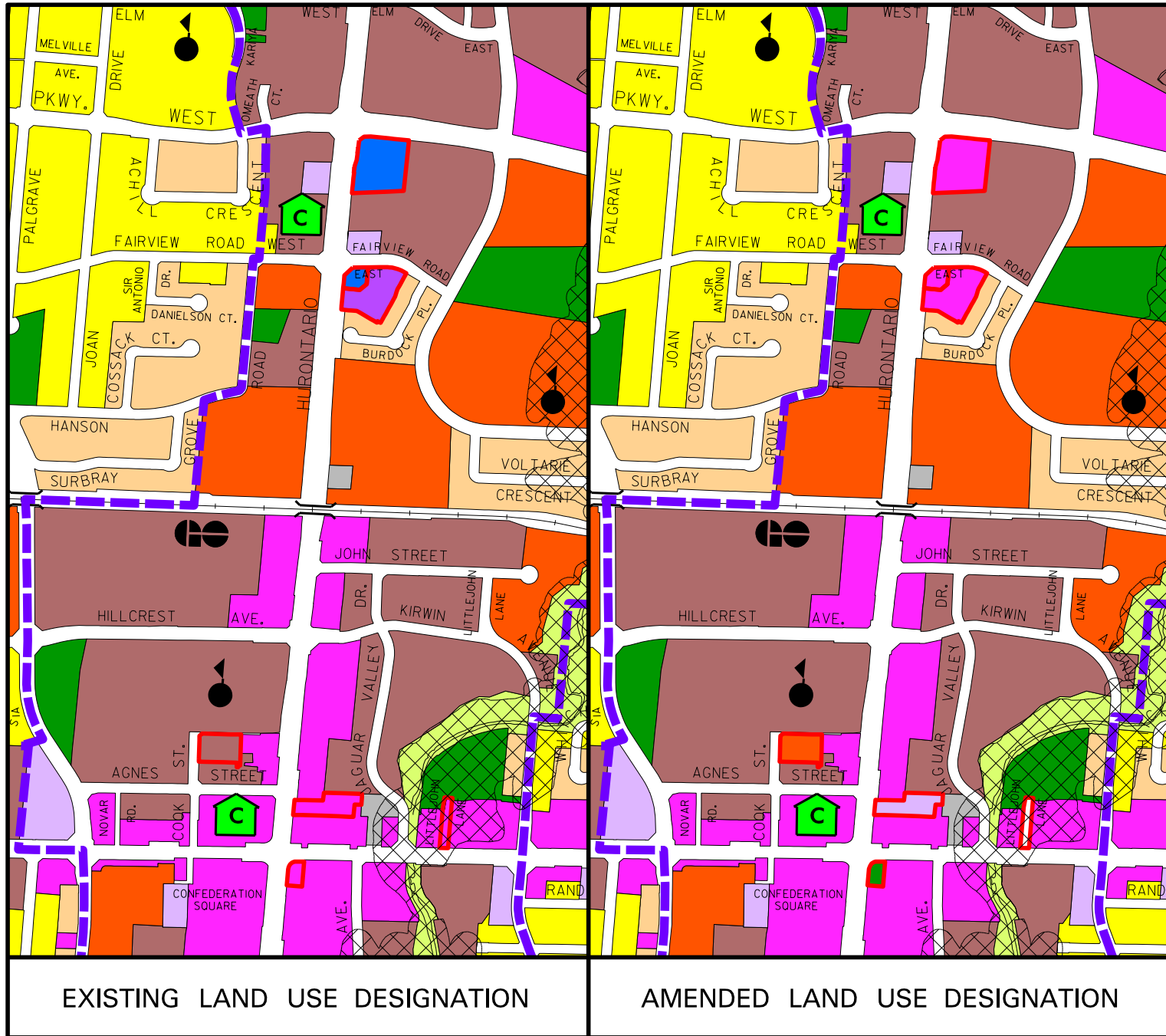
DUNDAS ST. E.

DUNDAS ST. W.

DUNDAS ST. W.



6/9/2022  
breric  
PB-Half-Strze.tbl



### LAND USE DESIGNATIONS

Residential Low Density I	Business Employment
Residential Low Density II	Industrial
Residential Medium Density	Airport
Residential High Density	Institutional
Mixed Use	Public Open Space
Downtown Mixed Use	Private Open Space
Convenience Commercial	Greenlands
Motor Vehicle Commercial	Parkway Belt West
Office	Utility

### BASE MAP INFORMATION

Heritage Conservation District	Civic Centre (City Hall)
1996 NEP/2000 NEF Composite Noise Contours	City Centre Transit Terminal
LBPIA Operating Area Boundary See Aircraft Noise Policies	GO Rail Transit Station
Area Exempt from LBPIA Operating Area	Public School
Natural Hazards	Catholic School
	Hospital
	Community Facilities

### City Structure

Downtown	Corporate Centre
Major Node	Employment Area
Community Node	Special Purpose Area
Neighbourhood	

AREA OF AMENDMENT

FROM:

RESIDENTIAL HIGH DENSITY	MIXED USE
CONVENIENCE COMMERCIAL	
MOTOR VEHICLE COMMERCIAL	

TO:

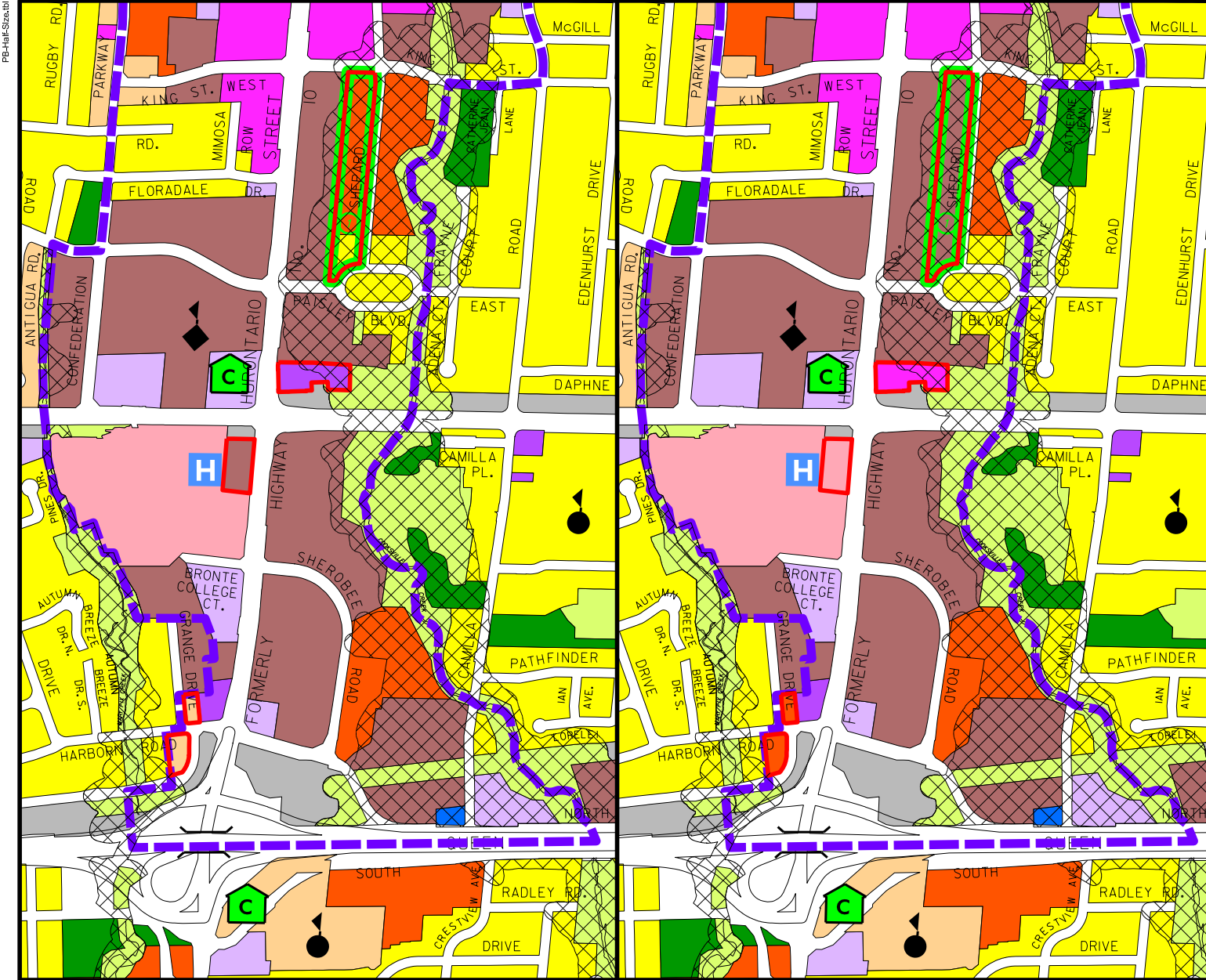
RESIDENTIAL MEDIUM DENSITY	PUBLIC OPEN SPACE
MIXED USE	NO DESIGNATION
OFFICE	

0 100 200 300 400 metres

## MAP 'E1'

Part of Schedule 10  
Land Use Designations  
of Mississauga Official Plan

MISSISSAUGA



EXISTING LAND USE DESIGNATION

AMENDED LAND USE DESIGNATION

## LEGEND

## LAND USE DESIGNATIONS

	Residential Low Density I		Office
	Residential Low Density II		Institutional
	Residential Medium Density		Public Open Space
	Residential High Density		Private Open Space
	Mixed Use		Greenlands
	Convenience Commercial		Utility
	Motor Vehicle Commercial		

## BASE MAP INFORMATION

	Heritage Conservation District		Civic Centre (City Hall)
	1996 NEP/2000 NEF Composite Noise Contours		City Centre Transit Terminal
	LBPIA Operating Area Boundary See Aircraft Noise Policies		GO Rail Transit Station
	Area Exception from LBPIA Operating Area		Transitway Station
	Natural Hazards		Public School
			Catholic School
			Hospital
			Community Facilities

## City Structure

	Downtown		Neighbourhood
--	----------	--	---------------

## AREA OF AMENDMENT

## FROM:

	RESIDENTIAL LOW DENSITY I
	RESIDENTIAL LOW DENSITY II
	RESIDENTIAL MEDIUM DENSITY
	RESIDENTIAL HIGH DENSITY
	CONVENIENCE COMMERCIAL

## TO:

	RESIDENTIAL MEDIUM DENSITY
	RESIDENTIAL HIGH DENSITY
	MIXED USE
	INSTITUTIONAL

Appealed to the Ontario Land Tribunal (OLT)  
OLT Case Number PL980724 (OLT 3 - City Plan)

0 100 200 300 400  
metres



MAP 'E2'

Part of Schedule 10  
Land Use Designations  
of Mississauga Official Plan

MISSISSAUGA

## Appendix 7

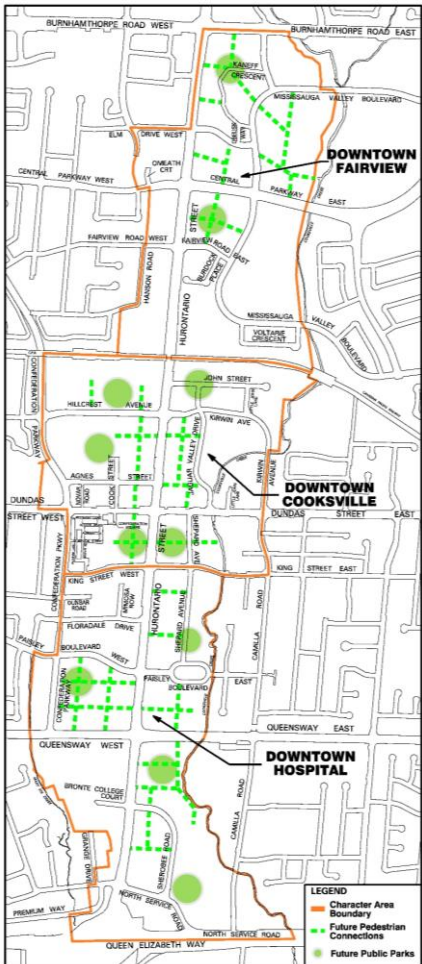
**Planning Rationale for Proposed Amendments to Downtown Fairview, Cooksville and Hospital Character Areas, Mississauga Official Plan (MOP), Chapter 12 Downtown**

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
<b>12.1 Introduction</b>	<i>Added</i>	146	<p>Policies shown in a text box are Protected <b>Major Transit Station Area</b> policies in accordance with the <i>Planning Act</i>. The Protected <b>Major Transit Station Area</b> policies of this section will not come into force and effect until the Major Transit Station Area policies of the Region of Peel Official Plan adopted by Region of Peel By-law 20-2022 is approved by the Minister of Municipal Affairs and Housing, and Mississauga Official Plan Amendment No. 144 is in force and effect.</p> <p>In the event of a conflict between the policies of the Region of Peel Official Plan as adopted by Region of Peel By-law 20-2022 and the policies of Mississauga Official Plan Amendment No. 146, the Region of Peel Official Plan policies will prevail.</p> <p>The Downtown includes 10 Protected <b>Major Transit Station Areas</b> as shown on Schedule 2: Intensification Areas and identified on Schedule 11: Protected Major Transit Station Areas.</p> <ul style="list-style-type: none"> <li>• Burnhamthorpe;</li> <li>• City Centre;</li> <li>• Cooksville GO;</li> <li>• Duke of York;</li> <li>• Dundas;</li> <li>• Fairview;</li> <li>• Robert Speck;</li> <li>• Main;</li> <li>• North Service; and</li> <li>• Queensway.</li> </ul>	Text introduces Major Transit Station Areas (MTSA) in the Downtown, and identifies related Protected Major Transit Station Area policies in Chapter 12 Downtown with a text box around each policy.
<b>12.1.1 General</b>	<i>Deleted</i>	145	12.1.1.1 Proponents of development applications within the Downtown may be required to demonstrate how the new development contributes to the achievement of the residents and jobs density target and the population to employment ratio.	New policies and schedules expand on and replace these policies.

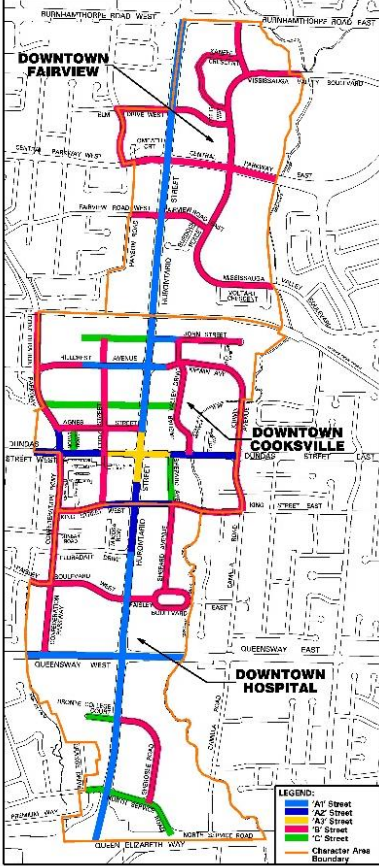
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
	<i>Deleted</i>	146	<p>12.1.1.2 A minimum building height of three storeys is required on lands within the Downtown. Character Area policies may specify maximum building height requirements. Alternative minimum building heights may be established for existing areas with low density residential development.</p> <p>12.1.1.3 Proposals for heights less than three storeys or different than established in the Character Area policies, will only be considered where it can be demonstrated to the City's satisfaction, that:</p> <ol style="list-style-type: none"> <li>an appropriate transition in heights that respects the surrounding context will be achieved;</li> <li>the development proposal enhances the existing or planned development;</li> <li>the City Structure hierarchy is maintained; and</li> <li>the development proposal is consistent with the policies of this Plan.</li> </ol> <p>12.1.1.5 Down-zoning of lands within the Downtown and in the transition area adjacent to the Downtown should not be permitted.</p>	New policies and schedules expand on and replace these policies.
	<i>Added</i>	145	12.1.1.1 Proponents of development applications within the Downtown may be required to demonstrate how new development contributes to a concentration and mix of jobs as a key component of a mixed use transit-supportive development.	Policy emphasizes the importance of a concentration and mix of jobs in achieving mixed use, transit-supportive development in PMTSAs as required by provincial and regional MTSA policies.
	<i>Added</i>	145	12.1.1.4 Prior to development within flood plains, the development proponent will assess impacts, identify flood management measures and mitigate flood hazards to the satisfaction of the City and the appropriate conservation authority.	New policy provides further clarity as to how development should address flood risks within flood plains to ensure public safety.
<b>12.1.2 Residential</b>	<i>Deleted</i>	146	12.1.2.2 Notwithstanding the Residential High Density policies of this Plan, the maximum building height for lands designated Residential High Density will not exceed 25 storeys.	References to maximum building heights will be included in the new City-Wide MTSA OPA. Conflicts with new policies allowing for greater heights on select sites.
<b>12.1.8 Downtown Fairview,</b>	<i>Added</i>	145	The following additional policies apply to the Downtown Fairview, Downtown Cooksville and Downtown Hospital Character Areas.	New section introduces general policies on community infrastructure, public parkland and supporting local businesses. Additional sections



Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
<b>Cooksville and Hospital</b>				on urban form, and transportation policies that apply to Downtown Fairview, Cooksville and Hospital Character Areas are proposed and outlined below.
<b>12.1.8.1 General</b>	<i>Added</i>	145	<p>12.1.8.1.1 Partnerships and collaborations will be encouraged to identify needs and develop community infrastructure such as integrating a school, a daycare facility or a community facility within a building with other uses.</p> <p>12.1.8.1.2 Downtown Fairview, Cooksville and Hospital will develop with a high quality, well-designed and expanded network of public parkland and open space that is green, safe, attractive, and supports a range of social and recreation activities. Parkland may also provide gathering spaces and connections, to existing and future parks and open spaces, commercial developments, community infrastructure and to surrounding neighbourhoods.</p>	<p>New section provides direction on community infrastructure, and public parkland to ensure that they meet the needs of a growing community, and support the vision of a vibrant, 15-minute city in Downtown Fairview, Cooksville and Hospital with access to recreation, parks and open spaces.</p> <p>Small businesses reflect the multicultural heritage and character of the community and provide employment opportunities. The City will explore incentives to retain existing small businesses in new developments.</p>

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			 <p>Figure 1: Future pedestrian connections and public parkland network. These symbols represent the general location of new pedestrian connections, as well as public parkland in the vicinity.</p> <p>12.1.8.1.3 As part of the development application review process, the City will determine the location, configuration and size of new and/or expanded parkland blocks, and the</p>	

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>requirement for land dedication. Opportunities to obtain parkland will also be explored through purchase by the City.</p> <p>12.1.8.1.4 Through development, opportunities to increase the provision of open space will be explored. These open spaces will be complementary to the public parkland system.</p> <p>12.1.8.1.5 Strategies to support the retention of local businesses and increase employment uses may be pursued including Community Improvement Plans and other incentives.</p>	
<b>12.1.8.2 Urban Form and Building Transition</b>	<i>Added</i>	145	<p>12.1.8.2.1 New buildings will achieve a high quality urban design and built form, and will be designed and located to:</p> <ol style="list-style-type: none"> <li>create a transition in height generally consistent with a 45 degree angular plane that is measured from the property line adjacent to residential low and medium density land use designations;</li> <li>generally maintain a minimum separation distance of 30 metres between portions of buildings that are greater than six storeys;</li> <li>add visual interest by varying the massing of buildings; and</li> <li>promote visibility and interest from the street through the use of high quality materials and architectural detailing in the design of the podium.</li> </ol> <p>12.1.8.2.2 For larger developments where more than one tower is proposed, a variation in building heights will be achieved.</p> <p>12.1.8.2.3 <b>Tall buildings</b> will incorporate podiums that are a minimum of three storeys and a maximum of six storeys.</p> <p>12.1.8.2.4 The floor plate of buildings above the podium will vary in size depending on the height of the building. For <b>tall buildings</b>, the tower above the podium will have a smaller floor plate size.</p> <p>12.1.8.2.5 Infill development on lands immediately adjacent to the Hurontario Street right-of-way will have buildings located along the street edge, where possible.</p>	<p>New section proposes policies that promote a walkable built form that is well-designed, encourages a sense of place, and provides for public spaces that are of high quality, safe, accessible, attractive and vibrant.</p> <p>The policies complement existing urban form policies in the Official Plan, and will ensure appropriate building height transitions, building separation distances, a desirable mix of building heights, podium conditions and pedestrian-oriented streetscape.</p> <p>Further direction is provided for existing and new roads to ensure street frontage conditions align with the land use vision for these 3 communities. Three different street types are identified; each with their own function and character, and associated street frontage provisions.</p>

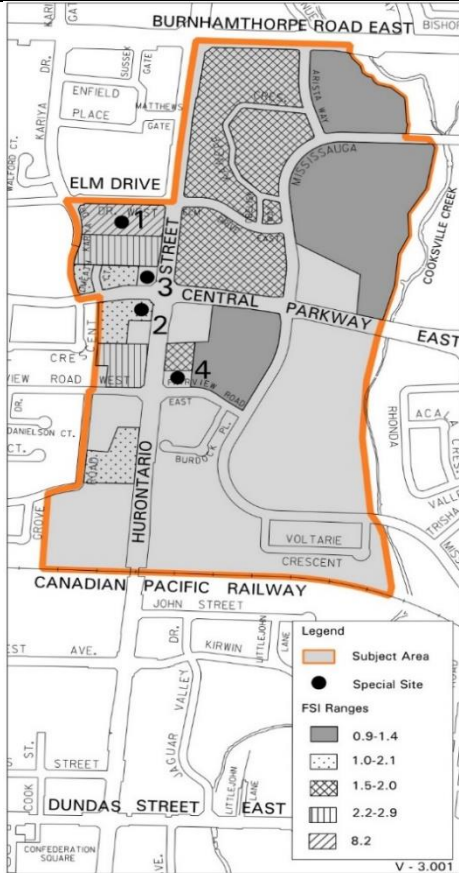
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p><b>Street Frontage Provisions</b></p> <p>The majority of existing and new roads in Downtown Fairview, Downtown Cooksville and Downtown Hospital are categorized as 'A', 'B' or 'C' Streets and are shown on Map 12-1.2:</p> <p>Downtown Fairview, Cooksville and Hospital Street Types.</p>  <p>Map 12-1.2: Downtown Fairview, Cooksville and Hospital Street Types</p>	

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>Provisions for 'A' Streets</p> <p>12.1.8.2.6 'A1', 'A2' and 'A3' Streets will have a strong sense of place and a main street, pedestrian friendly character with active, mixed use building frontages. Buildings fronting these streets will reinforce a distinct high quality built form, public realm, landscaping and pedestrian amenities.</p> <p>12.1.8.2.7 Development abutting 'A1', 'A2' and 'A3' Streets will incorporate ground floor non-residential uses. Development will:</p> <ol style="list-style-type: none"> <li>promote a main street character with smaller retail units and frequent entrances accessible from the public sidewalk;</li> <li>frame and animate streets and public spaces, with active building frontages including storefronts, prominent entrances and residential lobbies linking the building and the sidewalk;</li> <li>provide a consistent streetwall with building indentations as visual relief;</li> <li>design non-residential units at street corner locations with animated frontages that wrap the corner;</li> <li>provide generous setbacks from the street to accommodate, among other things, landscaping, street furniture, wayfinding, bus shelters, pedestrian-scaled lighting and outdoor patios;</li> <li>provide appropriate setbacks between the edge of the podium and tower portion of the building; and</li> <li>coordinate private land abutting the sidewalk with the public boulevard to create an integrated design character and reinforce the main street context. Opportunities for the development of entry forecourts and plazas will be encouraged in order to create a varied <b>streetscape</b>.</li> </ol> <p>Provisions for 'B' Streets</p> <p>12.1.8.2.8 Development along 'B' Streets will primarily support housing and pedestrian access and movement. Although 'B' Streets are primarily residential in nature, development may include some non-residential uses on the ground floor. The design of the built form and streetscape along 'B' streets will</p>	

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>establish a unified character while ensuring adequate light and skyviews, and a seamless interface and transition between the public and private realms. Development will address the following:</p> <ul style="list-style-type: none"> <li>a. a consistent streetwall with building indentations will be provided as visual relief;</li> <li>b. generous setbacks from the street to accommodate, among other things, landscaping, terraces, bus shelters, and pedestrian scaled lighting;</li> <li>c. a high quality seamless interface and transition between the public sidewalk and principal building entrance to ensure adequate separation, definition and privacy; and</li> <li>d. appropriate stepbacks between the edge of the podium and tower portion of the building.</li> </ul> <p>Provisions for 'C' Streets</p> <p>12.1.8.2.9 Development along 'C' Streets will support a pedestrian environment and vehicular access to parking and other service areas such as deliveries, garbage pick-up, service and loading. 'C' Streets may also support residential, retail and service commercial uses. Development should address the following:</p> <ul style="list-style-type: none"> <li>a. opportunities to consolidate service, parking and loading to serve a number of buildings;</li> <li>b. ensure the visual impact of the parking, loading and service areas from the street is minimized and screened from view; and</li> <li>c. provide a secondary entrance for pedestrian access, where appropriate.</li> </ul>	
<b>12.1.8.3 Transportation</b>	<i>Added</i>	145	<p>12.1.8.3.1 Development will be required to provide pedestrian connections to connect to the light rail transit (LRT) line, transit routes/stops, trails, and parks and open spaces.</p> <p>12.1.8.3.2 Public easements will be required where pedestrian connections are proposed on private lands.</p>	<p>Road design and pedestrian connectivity are important elements of a walkable, transit-supportive community. New policies are proposed to achieve an expanded publicly-accessible pedestrian network, minimize vehicular-pedestrian conflict through road access restriction and driveway consolidation,</p>



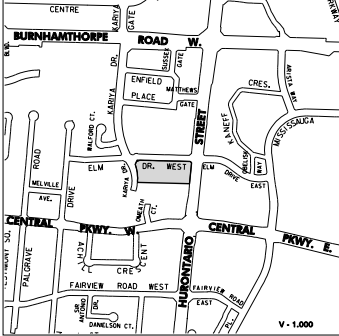
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>12.1.8.3.3 Vehicular access from Hurontario Street and Dundas Street will be restricted and consolidated to minimize driveways and conflict with pedestrians, cyclists and transit.</p> <p>12.1.8.3.4 Roads will be designed as complete streets and incorporate <b>active transportation</b> and transit infrastructure.</p> <p>12.1.8.3.5 Existing arterial and collector roads bisecting and surrounding the Character Areas will incorporate <b>active transportation</b> and transit infrastructure when redesigned, as appropriate.</p>	<p>and integrate complete street design, active transportation and transit infrastructure.</p>

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
<b>12.3 Downtown Fairview</b>	<i>Revised</i>	146	 <p>Map 12-3: Downtown Fairview Character Area</p> <p>Proposed changes to Map 12-3:</p> <ul style="list-style-type: none"> <li>• Delete FSI Ranges</li> <li>• Delete Special Sites 1 to 4</li> <li>• Add a new Exempt Site – Exempt Site 1</li> </ul>	<p>New policies on minimum densities, maximum building heights and urban form replace the need for floor space index (FSI) values on the Character Area Map.</p> <p>Special sites that are no longer needed are deleted. A new exempt site recognizes the existing use with the intent that the lands will be redeveloped over time with the underlying land use designation.</p>
<b>12.3.1 Introduction</b>	<i>Added</i>	145	<p>Downtown Fairview is situated on the periphery of Mississauga's Downtown Core and provides an area of transition to Downtown Cooksville. Downtown Fairview contains a number of sub-areas, each with its own unique character and built form.</p>	<p>New section introduces Downtown Fairview, including its context to the south of the Downtown Core, and the unique character and built form of various sub-areas. There is an</p>

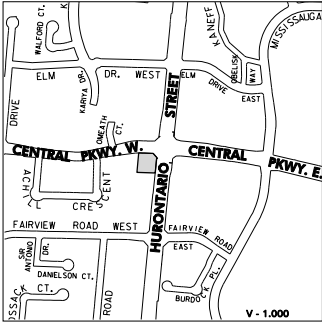
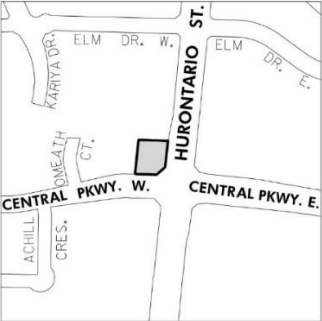
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>Along Mississauga Valley Boulevard, 'tower in the park' developments, with generally 20 to 24 storey apartment buildings are surrounded by large open space areas with connections to trails and the Cooksville Creek. The area at Hurontario Street and Elm Drive West contains the greatest densities in Downtown Fairview, with point towers as tall as 50 storeys. The townhouses located in the southern end of Downtown Fairview are the predominant built form and provides housing for families.</p> <p>There are limited retail and commercial uses along Hurontario Street, however with planned <b>higher order transit</b>, new developments can transform the street into vibrant places to shop, work and visit. Anchored by a grocery store, the existing retail site located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard provides convenient shopping for nearby residents.</p> <p>The Cooksville Creek is a significant <b>natural heritage feature</b> that runs north-south in Downtown Fairview and drains into Lake Ontario. Opportunities to enhance the ecological features of existing natural habitats and the urban forest will be encouraged.</p> <p>The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.</p>	<p>opportunity to build on this context through appropriate development, with a focus on introducing a greater mix of uses along Hurontario Street, as well as opportunities to improve and expand existing parks.</p>
12.3.2 Vision	Added	145	<p>Downtown Fairview will be a distinct, walkable community that provides a transition to lower heights and densities from the Downtown Core. With more shops and amenities along Hurontario Street, Downtown Fairview will be an attractive place where people can access their daily needs within a short distance from their homes and places of work. Mixed use development located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard will serve the retail and commercial needs of the local community. The area along</p>	<p>Setting a vision for Downtown Fairview will provide a clear direction for future growth and development; the vision is reinforced with a series of guiding principles and a new policy framework.</p>

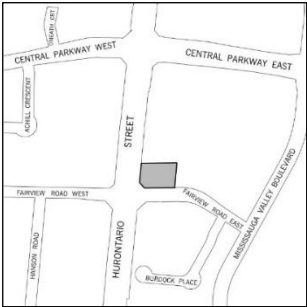
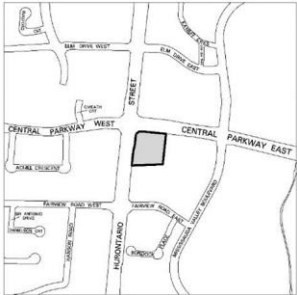
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>Mississauga Valley Boulevard will continue to be defined by existing towers in the park and beautiful green spaces, with improved connections to trails and Cooksville Creek.</p> <p>An expanded pedestrian network will connect new development with nearby amenities and residential areas, and improve access to transit, parks and open spaces.</p> <p>The vision is based on the following guiding principles:</p> <ol style="list-style-type: none"> <li>1. Heights and densities that are appropriate to the planned context. The greatest heights will be located at Hurontario Street and Elm Drive West, with a transition to lower building heights and densities for new developments south along Hurontario Street and east towards Mississauga Valley Boulevard;</li> <li>2. An attractive, walkable environment along Hurontario Street with a vibrant mix of shops, restaurants, cafes and service establishments;</li> <li>3. Accessible public spaces for all, with enhanced connections for walking and cycling; and</li> <li>4. A mix of housing forms and tenure with a range of housing options that meet the needs of a diverse community.</li> </ol>	
<b>12.3.3 Building Heights</b>	<i>Added</i>	146	<p>12.3.3.1 The greatest building heights will be located in the existing and planned buildings at Elm Drive West and Hurontario Street as shown on Schedule 11-K: Protected Major Transit Station Area Building Heights Schedule. Heights beyond this intersection should transition down to ensure the prominence of the Downtown Core.</p> <p>12.3.3.2 On lands designated Residential High Density and Mixed Use, the maximum permitted building height as shown on Schedule 11-K may be exceeded by up to three storeys without an amendment to this Plan, subject to meeting the building transition policies of this Plan, where a development provides additional non-residential uses, including community infrastructure. One additional storey in building height may be permitted for every 900 square metres of non-residential gross floor area (GFA) provided above the first storey. This does not</p>	<p>New maximum building heights are proposed to manage the growth and density within Downtown Fairview and to support a mixed use community. Maximum buildings heights will be implemented through the City-Wide MTSA official plan amendment (MTSA OPA).</p> <p>The basis for the maximum building heights support the following:</p> <ul style="list-style-type: none"> <li>• meets provincial and regional minimum densities and contributes to the City's growth allocation;</li> <li>• supports transit investments;</li> <li>• emphasizes the prominence of the Downtown Core;</li> </ul>

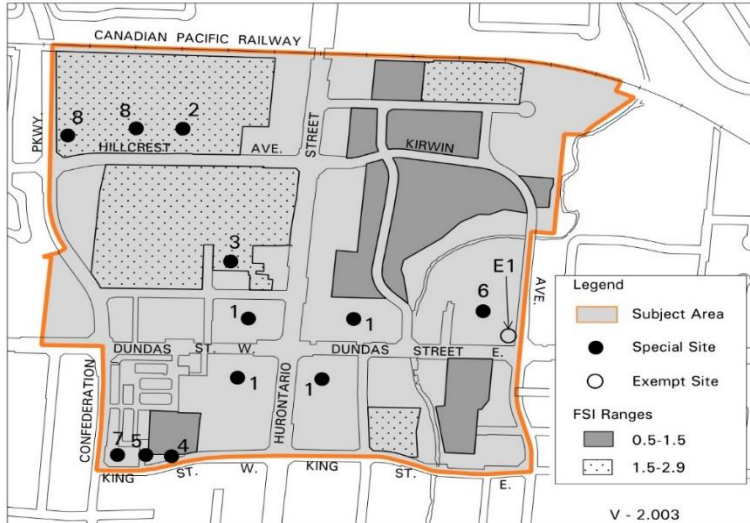
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			include amenity space, above grade parking or ground floor non-residential uses, where required by the policies of this Plan.	<ul style="list-style-type: none"> <li>recognizes that Downtown Fairview is not planned to accommodate the same level of growth and development as the Downtown Core;</li> <li>provides more certainty around maximum heights;</li> <li>aligns with the vision for Downtown Fairview with a transition to lower heights and densities from the Downtown Core;</li> <li>aligns with the planned land use designation; and</li> <li>recognizes approved or existing development.</li> </ul> <p>A new policy is proposed to incentivize additional non-residential uses beyond what is required along key streets on the ground floor. Increases to the prescribed maximum building heights of up to 3 storeys may be permitted. Building transition policy requirements will still apply.</p>
<b>12.3.4 Land Use</b>	<i>Added</i>	146	<p><b>12.3.4.1 Residential High Density</b> 12.3.4.1.1 Notwithstanding the policies of this Plan, development along Hurontario Street will provide non-residential uses on the ground floor where the building is immediately adjacent to the Hurontario Street right-of-way.</p> <p><b>12.3.4.2 Mixed Use</b> 12.3.4.2.1 Notwithstanding the policies of this Plan, development along Hurontario Street:</p> <ol style="list-style-type: none"> <li>will provide non-residential uses on the ground floor where the building is immediately adjacent to Hurontario Street; and</li> <li>may consist of primarily residential uses above the ground floor.</li> </ol>	<p>Downtown Fairview, which forms part of an Urban Growth Centre, and identified as a strategic growth area will support a mix of uses, including a balanced mix of employment and residential uses. The City is challenged with development applications that propose a large number of residential dwelling units, but offer minimal office, community facilities, retail and commercial uses.</p> <p>New policies to activate uses on the ground floor are proposed by requiring new developments to provide a minimum amount of non-residential uses. This will help to achieve the vision, in part, for a vibrant 15-minute city.</p>

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p><b>12.3.4.3 Office</b>            12.3.4.3.1 Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office space is retained or replaced through development.</p>	<p>To help provide continued access to services and employment, new policies will also require developments to replace existing non-residential floor space.</p>
<p><b>Former 12.3.2 Special Site Policies</b></p>	<p><i>Deleted</i></p>	<p>146</p>	<p><b>12.3.2.1 Site 1</b></p>  <p>12.3.2.1.1 The lands identified as Special Site 1 are located on the west side of Hurontario Street, south of Elm Drive.</p> <p>12.3.2.1.2 Notwithstanding the policies of this Plan, the following additional policies will apply:</p> <ol style="list-style-type: none"> <li>three apartment buildings with maximum heights of 35, 40 and 50 storeys with a total maximum <i>floor space index (FSI)</i> of 8.2 will be permitted;</li> <li>commercial uses will be permitted; and</li> <li>access to the site will only be permitted from Kariya Drive and Elm Drive West.</li> </ol>	<p>Special site policies that are no longer needed, or have been expanded on and replaced with new policies, have been deleted.</p>



Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p><b>12.3.2.2 Site 2</b></p>  <p>12.3.2.2.1 The lands identified as Special Site 2 are located on the west side of Hurontario Street, south of Central Parkway West.</p> <p>12.3.2.2.2 Notwithstanding the provisions of the Residential High Density designation and applicable policies, the following additional policies will apply:</p> <ul style="list-style-type: none"> <li>d. the total number of units constructed on the site will not exceed 135; and</li> <li>e. a maximum <i>floor space index (FSI)</i> of 3.75 will be permitted.</li> </ul> <p><b>12.3.2.3 Site 3</b></p> 	

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>12.3.2.3.1 The lands identified as Special Site 3 are located at the northwest corner of Central Parkway West and Hurontario Street.</p> <p>12.3.2.3.2 Notwithstanding the policies of this Plan, an apartment dwelling with a maximum height of 36 storeys will be permitted.</p> <p><b>12.3.2.4 Site 4</b></p>  <p>12.3.2.4.1 The lands identified as Special Site 4 are located at the northeast corner of Fairview Road East and Hurontario Street.</p> <p>12.3.2.4.2 Notwithstanding the policies of this Plan, an apartment building with a maximum height of 32 storeys will be permitted.</p>	
<p><b>12.3.5.1</b> <b>Exempt Site 1</b> <b>Policies</b></p>	<p><i>Added</i></p>	<p>146</p>		<p>New exempt site recognizes existing motor vehicle commercial uses with the intent that the site be redeveloped with a mix of uses, in accordance with the underlying mixed use designation.</p>

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>12.3.5.1.1 The lands identified as Exempt Site 1 are located on the southeast corner of Hurontario Street and Central Parkway East.</p> <p>12.3.5.1.2 Notwithstanding the policies of this Plan, motor vehicle commercial uses will also be permitted.</p>	
<b>12.4 Downtown Cooksville</b>	<i>Revised</i>	146	 <p>Map 12-4: Downtown Cooksville Character Area</p> <p>Proposed changes to Map 12-4:</p> <ul style="list-style-type: none"> <li>• Delete FSI Ranges</li> <li>• Delete Special Site 2 and replace with a new Special Site 2</li> <li>• Delete Special Sites 6 and 8</li> </ul>	<p>New policies on minimum densities, maximum building heights and urban form replace the need for floor space index (FSI) values on the Character Area Map.</p> <p>Special site policies that are no longer needed are deleted. Special Site 2 is deleted and replaced to provide further policy direction for lands located near the Cooksville GO Station.</p>
<b>Former Section 12.4.1 Urban Design Policies</b>	<i>Deleted</i>	145	<p><b>Overall Character</b></p> <p>12.4.1.1 A high level of urban design, pedestrian amenity, and intensity of development is encouraged along principal street frontages. A sense of entry to the Character Area should be articulated at these locations by prominent built form, landscaping and signage components.</p>	<p>New policies expand on and replace the existing urban design policies for Downtown Cooksville.</p>

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>12.4.1.2 The sections of Hurontario Street and Dundas Street within the Character Area should function as a focus for the Cooksville Neighbourhood, having a strong sense of place and main street character with active mixed use building frontages and highly pedestrianized nature. These street frontages should reinforce a distinctive, quality image with high standards in built form, landscaping and related pedestrian amenities.</p> <p><b>Character of Dundas Street and Hurontario Street</b></p> <p>12.4.1.3 Street Edge Uses -Development abutting the street should encourage a high level of activity along the street by incorporating grade related retail with residential and/or offices above. Retail units should be clearly oriented to, and accessed from, the public sidewalk.</p> <p>12.4.1.4 Street Scale and Enclosure - Development should be closely related to, and integrated with, the public sidewalk to focus activity on the street and provide a sense of spatial enclosure for the street. Development should address the following:</p> <ul style="list-style-type: none"> <li>f. limited building setback range of three to five metres from the street line, with the larger setback in areas of high transit or pedestrian use;</li> <li>g. minimum building height of two to four storeys and maximum of six storeys directly abutting the street line;</li> <li>h. maximum continuity of street walls with built form occupying a minimum of 80% of the street frontage; and</li> <li>i. a minimum setback of ten metres from the street line is required for buildings exceeding six storeys in height.</li> </ul> <p>12.4.1.5 Bulk and Massing - Development should be generally consistent in its bulk, massing and scale within the Character Area with use of taller more prominent buildings to highlight the Dundas and Hurontario Street intersection. A general height progression should be encouraged from this intersection to the Character Area edges and abutting the Downtown Hospital Character Area. A ratio of 1:2 (building height to eventual street</p>	

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>width) should be provided by built form abutting Dundas and Hurontario Streets.</p> <p>12.4.1.6 Development Fabric – Development fronting on the Hurontario Street and Dundas Street frontages should reinforce patterns and characteristics supportive of a main street role with highly animated pedestrian spaces. Development should address the following:</p> <ul style="list-style-type: none"> <li>a. commercial frontages should be broken up into smaller retail units and accessed directly from the public sidewalk with frequent access doorways;</li> <li>b. no parking should be provided between the buildings and street line;</li> <li>c. blank walls should be avoided along the street in favour of fenestration;</li> <li>d. service, loading and garbage storage areas should be accessed from rear lanes or abutting side streets;</li> <li>e. split level commercial frontages should be avoided;</li> <li>f. periodic building indentations should be provided as relief to long building walls and to provide opportunities for pedestrian spaces;</li> <li>g. storefront signage should respect the pedestrian scale and architectural character of development;</li> <li>h. pedestrian weather protection should be provided in the form of canopies and/or inset arcades; and</li> <li>i. the development and integration of rear yard parking lots to reduce the number of driveways along Hurontario Street and Dundas Street.</li> </ul> <p>12.4.1.7 <b>Streetscape</b> (Open Space and Landscaping) - Development of private land abutting the sidewalk should closely coordinate with the public boulevard to create an integrated design character at the pedestrian level and to reinforce the sense of a community main street. Opportunities for the development of entry forecourts and plazas will be</p>	

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>encouraged in order to create a varied <b>streetscape</b>. Any existing vegetation will be considered for preservation through the redevelopment/infill process to enhance the liveability of a space.</p> <p>12.4.1.8 Private Development Zone – Development should address the following:</p> <ul style="list-style-type: none"> <li>a. front yard setback areas should be provided with site furnishings, landscaping and paving materials, coordinate with the public boulevard design, and extend the usability of the public sidewalk; and</li> <li>b. opportunities for pedestrian spaces and circulation areas should be promoted at intersection corners, major building entrances and terminations of vistas.</li> </ul> <p>12.4.1.9 Public Realm: The Design of the Public Boulevard - The following features should be encouraged to reduce the perceived visual width of the street and improve the level of pedestrian comfort, safety and convenience within the public boulevard:</p> <ul style="list-style-type: none"> <li>a. common paving materials and patterns, street furniture and signage;</li> <li>b. road crossings defined by special paving;</li> <li>c. intersection design to moderate speed of turning traffic in favour of pedestrian movements, i.e. avoid the construction of designated right turn lanes with islands;</li> <li>d. setting back of bus bays from intersection corners to allow sidewalks to project;</li> <li>e. curb edge parking; and</li> <li>f. provision of street trees, feature lighting and related pedestrian amenities.</li> </ul>	
<b>Former section 12.4.2 Transportation</b>	<i>Deleted</i>	145	12.4.2.1 All development applications are required to address the relationship with adjacent properties and the need to minimize access onto Hurontario Street and Dundas Street.	<p>New policies expand on and replace former transportation policies 12.4.2.1 and 12.4.2.2.</p> <p>The City of Mississauga Parking Strategy from 1993 is an outdated document and the policy</p>

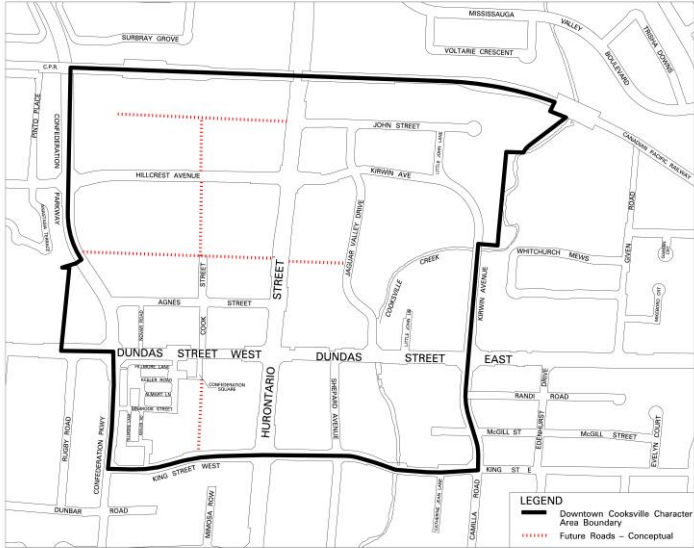


Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>12.4.2.2 A future signalized intersection with Hurontario Street is proposed in the vicinity of the former Melissa Street right-of-way along the southerly boundary of T.L. Kennedy Secondary School. This will connect with Cook Street initially and, with the potential redevelopment of the school site, may extend to Confederation Parkway in conjunction with the possible extension of Cook Street northerly to the east GO Access Road. Redevelopment proposals in this area will consider these options.</p> <p>12.4.2.3 The City of Mississauga Parking Strategy, carried out in 1993, made recommendations on the short, medium and long term strategies to meet the parking requirements and to achieve urban design objectives. These recommendations will be considered in conjunction with development applications and implementation of High Occupancy Vehicle (HOV) lanes on Hurontario Street and Dundas Street East.</p>	can be deleted. Official Plan policies on parking requirements and urban design objectives are contained in Chapter 8: Create a Multi-Modal City.
<b>12.4.1 Introduction</b>	<i>Added</i>	145	<p>Centred at Hurontario Street and Dundas Street, Downtown Cooksville has a rich history as a destination for newcomers to Canada and as a place for entrepreneurs and small businesses to set up shop and thrive. The cultural diversity of the neighbourhood is reflected in the variety of shops, restaurants and services that are available in the area. Immigrant programs and services in Downtown Cooksville provide an important function in helping families transition to a new country.</p> <p>Hurontario Street and Dundas Street is generally characterized by retail plazas that are one to two storeys with residential and/or office uses above and surrounded with surface parking. There are a few office and residential buildings along the <b>corridor</b> that are generally of a low or mid-rise form.</p> <p>With the potential for two-way, all-day GO Transit service at Cooksville GO Station, planned Bus Rapid Transit (BRT) along Dundas Street and the Hurontario Light Rail Transit (LRT), significant transit investments will strengthen its connection to the rest of Mississauga and beyond. These transit investments will increase access to employment, shopping, entertainment</p>	<p>New section introduces Downtown Cooksville, including its history as a destination for newcomers, and a place for small businesses to set up shop and thrive. Existing and planned rapid transit will improve connections to the rest of the City and the Greater Toronto Area.</p> <p>The proposed policy framework builds on this context by providing for transit-supportive development that is walkable, promotes a mix of uses, and improves access to housing, jobs and services.</p>

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>and other destinations, while also attracting more visitors. Transit- supportive development will ensure transit investments are supported with greater access to housing, jobs and services.</p> <p>The Cooksville Creek is a significant <b><i>natural heritage feature</i></b> in Downtown Cooksville. Cooksville Creek serves as a wildlife corridor and linkage to other features within and beyond Downtown Cooksville. This corridor is also used for recreation with opportunities for restoration, rehabilitation and hazard mitigation along the <b><i>watercourse</i></b>.</p> <p>The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.</p>	
<b>12.4.2 Downtown Cooksville as a 15-Minute City</b>	<i>Added</i>	145	Downtown Cooksville will continue to reflect its welcoming and multicultural heritage while integrating key elements of a 15-minute city concept. Downtown Cooksville is walkable and liveable, where the basic needs of everyday life are accessible and can easily be reached within a 15-minute walk. It has amenities that serve the community – parks, groceries, jobs, medical clinics, pharmacies, schools, shops, restaurants, and more – that can be conveniently accessed without the use of a car.	Downtown Cooksville already contains many elements of a 15-minute city that are to be retained and strengthened through redevelopment.
<b>12.4.3 Vision</b>	<i>Added</i>	145	Downtown Cooksville will be a walkable mixed use community with places for community amenities, culture and art. Hurontario and Dundas streets will be animated with storefronts and other active uses that support existing and future local businesses. Downtown Cooksville will continue to offer a diversity of local retailers and restaurants. The area immediately surrounding the intersection of Hurontario and Dundas Streets (Cooksville Corners) will be developed with a vibrant main street focus that includes a mix of uses and a human-scaled built form. The Cooksville GO Station area will be a focal point for transit-supportive development with a concentration and mix of residential, community, office, retail and service commercial uses.	<p>The vision builds on the concept of the 15-minute city. The historic intersection of Hurontario and Dundas Streets will continue to play an important role for the community, with a vibrant, main street focus. Transit-supportive development with a greater concentration of uses is envisioned around the Cooksville GO Station.</p> <p>The vision contains a series of guiding principles for policies that apply to the Character Area that will be supported by a new policy framework.</p>

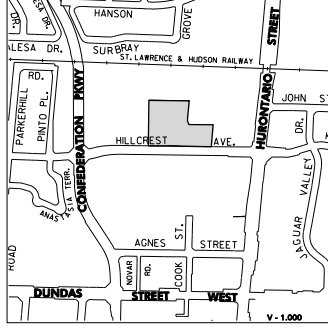
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>The vision is based on the following guiding principles:</p> <ol style="list-style-type: none"> <li>1. Establish a mixed use, vibrant community – create a range of local and unique businesses along Hurontario and Dundas streets, spaces for educational, cultural and community uses, a vibrant public realm, and a network of connected parks and open spaces;</li> <li>2. Plan for more people and employment – provide a range of housing options both in type and affordability, and jobs with the greatest heights and densities located in proximity to the Cookville GO Station;</li> <li>3. Achieve a walkable, connected community – promote a pedestrian-oriented main street character along Hurontario and Dundas streets, public art and a vibrant public realm, and improve pedestrian and cycling infrastructure; and</li> <li>4. Plan for high quality transit – provide convenient and efficient transit with seamless connections to local and <b>higher-order transit</b> services.</li> </ol>	
<b>12.4.4 General</b>	<i>Added</i>	145	12.4.4.1 Arts and cultural uses are encouraged to concentrate in Downtown Cooksville to add vibrancy to the area.	Downtown Cooksville is an ideal location for arts and cultural uses that celebrate its identity.
<b>12.4.5 Building Heights</b>	<i>Added</i>	146	<p>12.4.5.1 The greatest building heights will be located in proximity to the Cooksville GO station as shown on Schedule 11-L: Protected Major Transit Station Area Building Heights Schedule.</p> <p>12.4.5.2 On lands designated Residential High Density and Mixed Use and located outside of Special Site 1 in Downtown Cooksville, the maximum permitted building height as shown on Schedule 11-L may be exceeded by up to three storeys without an amendment to this Plan, subject to meeting the building transition policies of this Plan, where a development provides additional non-residential uses, including community infrastructure. One additional storey in building height may be permitted for every 900 square metres of non-residential gross floor area (GFA) provided above the first storey. This does not include amenity space, above grade parking or ground floor non-residential uses, where required by the policies of this Plan.</p>	<p>New maximum building heights are proposed to manage the growth and density within Downtown Cooksville and to support a mixed use community. Maximum buildings heights will be implemented through the City-Wide MTSA official plan amendment (MTSA OPA).</p> <p>The basis for the maximum building heights support the following:</p> <ul style="list-style-type: none"> <li>• meets provincial and regional minimum densities and contributes to the City's growth allocation;</li> <li>• supports transit investments;</li> <li>• recognizes that Downtown Cooksville is not planned to accommodate the same level of growth and development as the Downtown Core;</li> </ul>

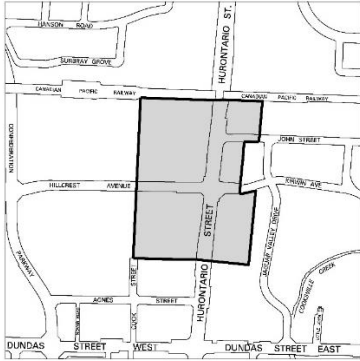
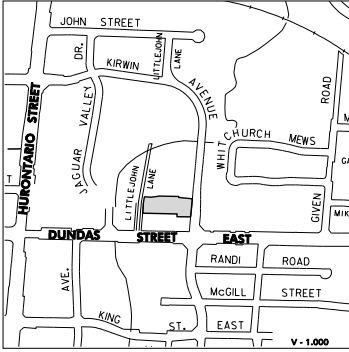
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
				<ul style="list-style-type: none"> <li>• provides more certainty around maximum heights;</li> <li>• aligns with the vision for Downtown Cooksville with taller heights near the Cooksville GO Station;</li> <li>• aligns with the vision for a vibrant, mainstreet focus and human-scaled built form near the Hurontario and Dundas Street intersection, and more generally along Dundas Street;</li> <li>• aligns with the planned land use designation; and</li> <li>• recognizes approved or existing development.</li> </ul> <p>A new policy is proposed to incentivize additional non-residential uses beyond what is required along key streets on the ground floor. The policy does not apply to the lands identified as Special Site 1 to ensure that the vision for a mainstreet character and human-scale built form is maintained for that area. Lands within Special Site 1 contain several existing buildings with more than one storey of non-residential uses.</p> <p>Increases to the prescribed maximum building heights of up to 3 storeys may be permitted. Building transition policy requirements will still apply.</p>
<b>12.4.6 Transportation</b>	<i>Added</i>	145	<p>12.4.6.1 The road network identified on Map 12-4.2 will support improved connectivity within Downtown Cooksville for pedestrian and cycling movement and to transit.</p> <p>Improvements to the road network will be achieved through development. Future additions to the road network include, but are not limited to the following:</p> <ul style="list-style-type: none"> <li>• a new east-west road from Confederation Parkway to Jaguar Valley Drive on the southern boundary of T.L.</li> </ul>	<p>New policies are proposed to improve the road network in Downtown Cooksville and near the Cooksville GO Station with new roads and expansions to existing roads. A more connected road network can reduce overall walking distances to services and amenities, including transit, and dissipate vehicular traffic through an expanded road network. Public ownership of</p>

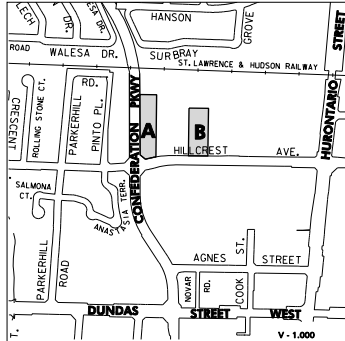
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>Kennedy Secondary School and former Melissa Street, aligning with a new road on the east side of Hurontario Street;</p> <ul style="list-style-type: none"> <li>extension of Cook Street northerly from the southern boundary of T.L. Kennedy Secondary School to connect with John Street; and</li> <li>extension of Cook Street southerly from Dundas Street West to connect with King Street West.</li> </ul>  <p>Map 12-4.2: Downtown Cooksville Character Area Future Roads</p> <p>12.4.6.3 Notwithstanding the policies of this Plan, the following additional policies will apply:</p> <ol style="list-style-type: none"> <li>all roads will be public;</li> <li>the design and access requirements for roads will be determined through the development application review process; and</li> </ol>	<p>roads will ensure that they are accessible to all users.</p>

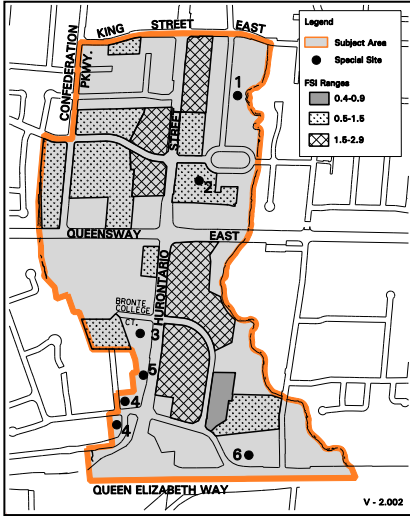
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			c. minor adjustments to the road network may be made without amendment to Map 12-4.2 at the City's discretion to accommodate development and break up large blocks with roads and pedestrian connections.	
<b>12.4.7 Land Use</b>	<i>Added</i>	146	<p><b>12.4.7 Residential High Density</b>  12.4.7.1.1 Notwithstanding the policies of this Plan, development will provide non-residential uses on the ground floor where the building is immediately adjacent to any of the following streets: Hurontario Street, Dundas Street, Hillcrest Avenue (Cook Street to Hurontario Street) and King Street (new Cook Street to Shepard Avenue).</p> <p><b>12.4.7.2 Mixed Use</b>  12.4.7.2.1 Notwithstanding the policies of this Plan, development along Hurontario Street, Dundas Street, Hillcrest Avenue (Cook Street to Hurontario Street) and King Street (new Cook Street to Shepard Avenue):</p> <ol style="list-style-type: none"> <li>will provide non-residential uses on the ground floor where the building is immediately adjacent to a street listed above; and</li> <li>may consist of primarily residential uses above the ground floor if not subject to a special site policy.</li> </ol> <p>12.4.7.2.2 Notwithstanding the policies of this Plan, the following additional uses will be permitted:</p> <ol style="list-style-type: none"> <li>banquet hall;</li> <li>conference centre; and</li> <li>entertainment, recreation and sports facilities.</li> </ol> <p><b>12.4.7.3 Office</b>  12.4.7.3.1 Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office space is retained or replaced through development.</p>	<p>Downtown Cooksville, which forms part of an Urban Growth Centre, and identified as a strategic growth area will support a mix of uses, including a balanced mix of employment and residential uses. The City is challenged with development applications that propose a large number of residential dwelling units, but offer minimal office, community facilities, retail and commercial uses.</p> <p>New policies to activate uses on the ground floor are proposed by requiring new developments to provide a minimum amount of non-residential uses. This will help to achieve the vision, in part, for a vibrant 15-minute city.</p> <p>To help provide continued access to services and employment, new policies will also require developments to replace existing non-residential floor space.</p>
<b>12.4.8.1 Special Site 1 Policies</b>	<i>Revised</i>	145 (policies 'b' and 'c') and 146	<p>12.4.8.1.2 Notwithstanding the policies of this Plan, the following additional policies will apply:</p> <ol style="list-style-type: none"> <li>minimum building heights of three storeys and a maximum of 18 storeys directly abutting Dundas Street and Hurontario</li> </ol>	New special site policies expand on the vision for the lands around the historic intersection of Hurontario and Dundas Street of a vibrant, mainstreet focus and human-scale built form.



Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
		(policies 'a' and 'd')	<p>Street to create an appropriate main street scale of development;</p> <p>b. <b>tall buildings</b> will incorporate podiums that are reflective of a main street character; buildings along Hurontario Street and Dundas Street will incorporate a generous setback between the edge of the podium and tower portion of the building;</p> <p>c. to achieve vibrant walkable spaces, buildings along pedestrian connections will promote animation at-grade with outdoor patios and storefront entrances to individual retail and service commercial units; and</p> <p>d. a minimum of three floors of non-residential uses will be required for buildings on lands designated Mixed Use or Residential High Density that are immediately adjacent to Hurontario Street and Dundas Street.</p>	<p>These policies are an important component of achieving a balanced mix of uses within Downtown Cooksville with a minimum amount of non-residential uses.</p> <p>In this location, three floors of non-residential uses can allow for a range of employment opportunities, services and amenities in Downtown Cooksville. This location is intended to serve as a one-stop destination where the greatest mix of services and amenities are accessible by walking or cycling.</p>
<b>Former 12.4.3.2 Special Site 2 Policies</b>	<i>Deleted</i>	146	 <p>12.4.3.2.1 The lands identified as Special Site 2 are located on the north side of Hillcrest Avenue, west of Hurontario Street.</p> <p>12.4.3.2.2 Notwithstanding the provisions of the Residential High Density designation, the following additional policies will apply:</p> <p>a. office development will be permitted at a maximum Gross Floor Area (GFA) of 61 439 m<sup>2</sup>, of which a maximum of 9 290 m<sup>2</sup> will be used for accessory uses; and</p> <p>b. apartments will be permitted at a maximum <b>floor space index (FSI)</b> of 1.5 - 2.9.</p>	New policies expand on and replace existing Special Site 2 policies.

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
<b>12.4.8.2 Special Site 2 Policies</b>	<i>Added</i>	146	 <p>12.4.8.2.1 The lands identified as Special Site 2 are located on the east and west sides of Hurontario Street between the Canadian Pacific Railway and north of Agnes Street.</p> <p>12.4.8.2.2 Notwithstanding the policies of this Plan, a minimum of three floors of non-residential uses will be required for buildings on lands designated Mixed Use or Residential High Density that are immediately adjacent to Hillcrest Avenue and Hurontario Street.</p>	<p>New special site policies expand on the vision for transit-supportive development with a mix of employment, community facilities, retail and service commercial uses and housing. These policies are an important component of achieving a balanced mix of uses within Downtown Cooksville with a minimum amount of non-residential uses.</p> <p>In this location, three floors of non-residential uses can allow for a range of employment opportunities, services and amenities in Downtown Cooksville. This location is intended to serve as a one-stop destination where the greatest mix of services and amenities are accessible by walking or cycling.</p>
<b>Former 12.4.3.6 Special Site 6 Policies</b>	<i>Deleted</i>	146	 <p>12.4.3.6.1 The lands identified as Special Site 6 are located on the west side of Kirwin Avenue, north of Dundas Street East.</p> <p>12.4.3.6.2 Notwithstanding the provisions of the Mixed Use designation, the following additional policies will apply:</p>	<p>New policies expand on and replace existing Special Site 6 policies.</p>

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			a. a hotel not exceeding 42 storeys will be permitted.	
Former 12.4.3.8 Special Site 8 Policies	Deleted	146	 <p>12.4.3.8.1 The lands identified as Special Site 8 are located on the north side of Hillcrest Avenue, east of Confederation Parkway.</p> <p>12.4.3.8.2 Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 8A will be permitted to develop to a maximum <b>floor space index (FSI)</b> of 3.5 and the lands identified as Area 8B will be permitted to develop to a maximum <b>floor space index (FSI)</b> of 4.0.</p>	New policies expand on and replace existing Special Site 8 policies.

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
<b>12.5 Downtown Hospital</b>	<i>Revised</i>	146	 <p>Map 12-5: Downtown Hospital Character Area</p> <p>Proposed changes to Map 12-5:</p> <ul style="list-style-type: none"> <li>• Delete FSI Ranges</li> <li>• Delete Special Site 2 and replace with a new Special Site 2</li> <li>• Delete Special Sites 3, 4 and 5</li> </ul>	<p>New policies on minimum densities, maximum building heights and urban form replace the need for floor space index (FSI) values on the Character Area Map.</p> <p>Special site policies that are no longer needed are deleted. Special Site 2 is deleted and replaced to provide further policy direction for lands located near the Mississauga Hospital.</p>
	<i>Deleted</i>	146	12.5.1.1 For lands designated Residential Medium Density, building height should not exceed a maximum of four storeys unless otherwise specified in the Urban Design policies of this Plan.	New policies expand on and replace this policy.
<b>Former section 12.5.3 Transportation</b>	<i>Deleted</i>	145	12.5.3.1 The City of Mississauga Parking Strategy, carried out in 1993, made recommendations on the short, medium and long term strategies to meet the parking requirements and to achieve urban design objectives. These recommendations will be considered in conjunction with development applications and implementation of High Occupancy Vehicle (HOV) lanes on Hurontario Street and Dundas Street East.	The City of Mississauga Parking Strategy from 1993 is an outdated document and the policy can be deleted. Official Plan policies on parking requirements and urban design objectives are contained in Chapter 8: Create a Multi-Modal City.
<b>12.5.1 Introduction</b>	<i>Added</i>	145	The focal point of Downtown Hospital continues to be the Mississauga Hospital, serving as one of Canada's leading health centres with specialized regional programs. Downtown Hospital	New section introduces Downtown Hospital as home to the Mississauga Hospital that is

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			<p>consists primarily of residential high density uses. Small pockets of low rise dwellings are located on the edges of the Character Area. Office uses are located in the area surrounding the hospital along Hurontario Street and Queensway. Mixed commercial uses are clustered at three main intersections along Hurontario Street at King Street, Queensway and North Service Road.</p> <p>Cooksville Creek is a significant <b><i>natural heritage feature</i></b> and portions of the creek have been impacted by channelization associated with flood and erosion control and storm water management. Enhancements to the ecological features of existing natural habitats and the urban forest will be encouraged.</p> <p>The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.</p>	<p>supported by a surrounding mix of office, commercial and residential uses.</p> <p>Opportunities exist to enhance the natural features of the existing Cooksville Creek, and expand existing parks.</p>
<b>12.5.2 Vision</b>	<i>Added</i>	145	<p>Downtown Hospital will be developed as a health district, anchored by the hospital with a broad range of uses clustered around the Mississauga Hospital, supporting more retail and service commercial uses, research and development, health facilities, offices, parks and residential uses.</p> <p>Transit-supportive development along Hurontario Street and Queensway will ensure greater access to housing, jobs and services.</p> <p>The vision is based on the following guiding principles:</p> <ol style="list-style-type: none"> <li>1. Support health care services – promote a range of health care services, supportive uses and research and development that create a healthier community; and</li> <li>2. Create a more walkable and transit-supportive community – provide pedestrian connections to Hurontario Street and Queensway, and street related retail and service commercial uses along these corridors.</li> </ol>	<p>The vision emphasizes the creation of a health district with a mix of employment, commercial and residential uses. The area has the potential to become a significant centre for health care services, research and development and supportive uses. The vision is supported by guiding principles and new policies that provide direction for how the health district can be achieved.</p>

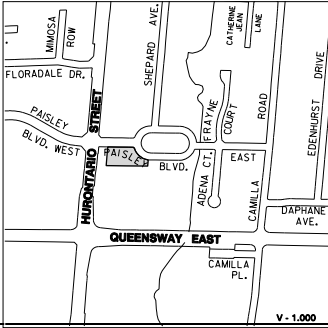
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
<b>12.5.3 Building Heights</b>	<i>Added</i>	146	<p>12.5.3.1 The greatest building heights will be located at the rapid transit stop at Hurontario Street and Queensway as shown on Schedule 11-L: Protected Major Transit Station Area Building Heights Schedule.</p> <p>12.5.3.2 On lands designated Residential High Density and Mixed Use, the maximum permitted building height as shown on Schedule 11-L may be exceeded by up to three storeys without an amendment to this Plan, subject to meeting the building transition policies of this Plan, where a development provides additional non-residential uses, including community infrastructure. One additional storey in building height may be permitted for every 900 square metres of non-residential gross floor area (GFA) provided above the first storey. This does not include amenity space, above grade parking or ground floor non-residential uses, where required by the policies of this Plan.</p>	<p>New maximum building heights are proposed to manage the growth and density within Downtown Hospital and to support a mixed use community. Maximum buildings heights will be implemented through the City-Wide MTSA official plan amendment (MTSA OPA).</p> <p>The basis for the maximum building heights support the following:</p> <ul style="list-style-type: none"> <li>• meets provincial and regional minimum densities and contributes to the City's growth allocation;</li> <li>• supports transit investments;</li> <li>• recognizes that Downtown Hospital is not planned to accommodate the same level of growth and development as the Downtown Core;</li> <li>• provides more certainty around maximum heights;</li> <li>• facilitates transit-supportive development along Hurontario and Queensway with maximum building heights of 25 storeys for new residential development;</li> <li>• allows for greater heights and density for office designated sites to help achieve the vision of a health district;</li> <li>• aligns with the planned land use designation; and</li> <li>• recognizes approved or existing development.</li> </ul> <p>A new policy is proposed to incentivize additional non-residential uses beyond what is required along key streets on the ground floor. Increases to the prescribed maximum building heights of up to 3 storeys may be permitted. Building transition policy requirements will still apply.</p>

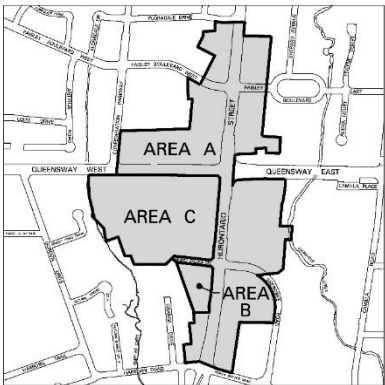


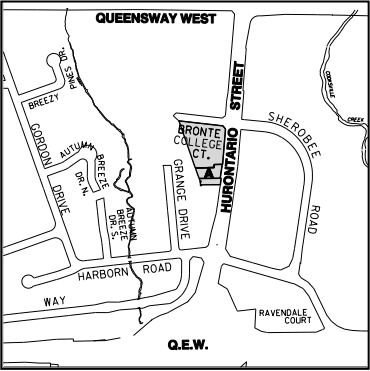
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
<b>12.5.4 Environmental Protection Area</b>	<i>Deleted</i>	145	<p>a. the lands within the floodway are designated Residential High Density, Office, Greenlands and Utility. Notwithstanding the Residential High Density, Office, Greenlands and Utility policies of this Plan, the following uses will be permitted within the floodway subject to the satisfaction of the City and Credit Valley Conservation:</p> <ul style="list-style-type: none"> <li>• flood and/or erosion works;</li> <li>• facilities which by their nature must locate near water or traverse watercourse (i.e. bridges, storm sewer outlets and stormwater management facilities); and</li> <li>• passive recreation activities; and</li> </ul>	The requirements of this policy are addressed and expanded on in other parts of the section, and as such this policy is not needed.
	<i>Revised</i>	145	<p>12.5.4.2 Notwithstanding the Natural Hazards policies of this Plan, the following policies will apply to those lands within the regulatory floodplain:</p> <p>a. floodway lands will be zoned in an appropriate hazard category in the implementing Zoning By-law;</p> <p>b. the lands within the flood fringe are subject to their respective land use designations and the following additional policies:</p> <ul style="list-style-type: none"> <li>• development may be permitted provided the use, building or structure is flood proofed to the regulatory flood level as required by the conservation authority;</li> <li>• ingress/egress for all development located in the flood fringe will be such that emergency vehicular and pedestrian movement is not prevented during times of flooding in order that safe access/evacuation is ensured. The determination of safe access shall be made by the conservation authority based on the depth and velocity factors;</li> <li>• enclosed underground parking will be subject to the installation of stringent flood proofing measures to the elevation of the regulatory flood level; and</li> <li>• the zoning of lands may utilize a holding provision to ensure flood proofing and safe access are addressed prior to development to the satisfaction of the City and the conservation authority. The Zoning By-law will be amended to remove the holding symbol when the requirements for flood proofing, the provision of safe</li> </ul>	Minor changes to provide clarity on holding provisions and consistency with the terminology used in the Mississauga Official Plan (i.e., the conservation authority).

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			<p>access to the proposed development and a detailed spill assessment and a financing agreement for the reconstruction of the culvert at the QEW has been completed to the satisfaction of the City, the conservation authority and the Ministry of Transportation; and</p> <p>c. the following uses will not be allowed within the floodplain:</p> <ul style="list-style-type: none"> <li>• institutional services such as hospitals, nursing homes, and schools where there would be a significant threat to the safety of inhabitants involved in an emergency evacuation situation as a result of flooding or failure of flood proofing measures;</li> <li>• new uses associated with the manufacture, storage, disposal and/or consumption of hazardous substances or the treatment, collection and disposal of sewage, which would pose an unacceptable threat to public safety if they were to escape their normal containment/use as a result of flooding or failure of flood proofing measures; and</li> <li>• emergency services such as those provided by fire, police, and ambulance stations and electrical substations, which would be impaired during a flood emergency as a result of flooding or failure of flood proofing measures.</li> </ul>	
<b>12.5.5 Land Use</b>	<i>Added</i>	146	<p><b>12.5.5 Land Use</b></p> <p>12.5.5.1 Development proposals in the vicinity of the Mississauga Hospital must demonstrate that new buildings and structures do not interfere with the flight path to the hospital heliport.</p> <p><b>12.5.5.2 Residential High Density</b></p> <p>12.5.5.2.1 Notwithstanding the policies of this Plan, development will provide non-residential uses on the ground floor where the building is immediately adjacent to any of the following streets: Hurontario Street, King Street (new Cook Street to Hurontario) and Queensway.</p>	<p>Downtown Hospital, which forms part of an Urban Growth Centre, and identified as a strategic growth area will support a mix of uses, including a balanced mix of employment and residential uses. The City is challenged with development applications that propose a large number of residential dwelling units, but offer minimal office, community facilities, retail and commercial uses.</p> <p>New policies to activate uses on the ground floor are proposed by requiring new developments to provide a minimum amount of non-residential</p>

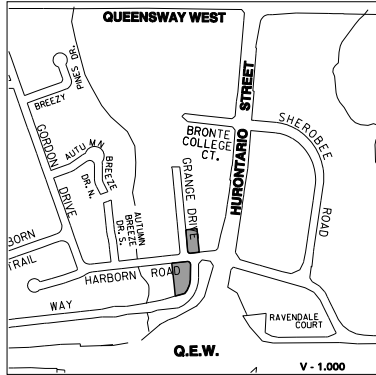
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p><b>12.5.5.3 Mixed Use</b>  12.5.5.3.1 Notwithstanding the policies of this Plan, development along Hurontario Street, King Street (new Cook Street to Hurontario Street) and Queensway:</p> <ol style="list-style-type: none"> <li>will provide non-residential uses on the ground floor where the building is immediately adjacent to a street listed above; and</li> <li>may consist of primarily residential uses above the ground floor if not subject to a special site policy.</li> </ol> <p><b>12.5.5.4 Office</b>  12.5.5.4.1 Redevelopment of existing office buildings that results in the loss of office floor space will not be permitted, unless the same amount of office space is retained or replaced through development.</p>	<p>uses. This will help to achieve the vision, in part, for a vibrant 15-minute city.</p> <p>To help provide continued access to services and employment, new policies will also require developments to replace existing non-residential floor space.</p> <p>A new policy also ensures safe and seamless operations of the hospital helipad.</p>
<b>12.5.6.1 Special Site 1</b>	<i>Revised</i>	145	<p>12.5.6.1.2 Notwithstanding the policies of this Plan, the following additional policies will apply:</p> <ol style="list-style-type: none"> <li>any proposal to alter the natural alignment of the Cooksville Creek and associated Greenlands lands through filling or channelization will require approvals from the appropriate conservation authority and the City of Mississauga. The final design of such alteration works will address the following: <ul style="list-style-type: none"> <li>criteria for erosion measures; and</li> <li>incorporation of the recommendations of the Cooksville Creek Rehabilitation Study for the reach of the Cooksville Creek between King Street East and Paisley Boulevard East; and</li> </ul> </li> <li>in the event that structural management options are not feasible and, subject to a detailed planning design and supporting engineering studies for the Special Site 1 lands, a <b>two-zone floodplain management concept</b>, or combination of a <b>two-zone floodplain management concept</b> and structural management options, may be implemented;</li> <li>the limits of the development will be determined through detailed studies to address limits of flooding. Lands not</li> </ol>	<p>New building transition policies are introduced in this amendment and existing official plan policies on urban form would apply to this site. As such, the building transition policy in this Special Site policy can be deleted.</p> <p>Minor wording changes reflect terminology that is consistent with other policies of the Mississauga Official Plan.</p>

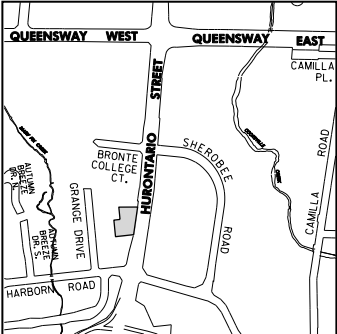
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>suitable for development will be redesignated to Greenlands; and</p> <p>d. the preparation of a concept plan promoting the integrated redevelopment of the existing residential lots will be required in conjunction with any redevelopment applications. The concept plan will be required to address the following:</p> <ul style="list-style-type: none"> <li>• naturalization of the Cooksville Creek and retention of existing vegetation; and</li> <li>• an alternate street access and design to limit access to the lands from only King Street East.</li> </ul>	
<p><b>Former 12.5.4.2 Special Site 2 Policies</b></p>	<p><i>Deleted</i></p>	<p>146</p>	 <p>12.5.4.2.1 The lands identified as Special Site 2 are located on the south side of Paisley Boulevard, east of Hurontario Street.</p> <p>12.5.4.2.2 Notwithstanding the provisions of the Residential High Density designation, apartments will be permitted at a maximum <i>floor space index (FSI)</i> of 0.5 – 1.5.</p>	<p>New policies expand on and replace existing Special Site 2 policies.</p>

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
12.5.6.2 Special Site 2 Policies	Added	146	 <p>12.5.6.2.1 The lands identified as Special Site 3 are located on the east and west sides of Hurontario Street between Floradale Drive and Harbourn Road/North Service Road and identified as a health district.</p> <p>12.5.6.2.2 Notwithstanding the policies of this Plan, the following additional policies will apply:</p> <ol style="list-style-type: none"> <li>lands identified as Area A and Area B that are designated Mixed Use or Residential High Density will provide a minimum of three floors of non-residential uses in buildings immediately adjacent to Hurontario Street and Queensway West;</li> <li>lands identified as Area B will also permit funeral homes and nursing homes; and</li> <li>lands identified as Area C will provide a potential location for an urban plaza and will also permit the following uses: <ul style="list-style-type: none"> <li>conference centre;</li> <li>education and training facility;</li> <li>financial institution;</li> <li>major and <b>secondary office</b>;</li> <li>overnight accommodation;</li> <li>personal service establishment;</li> <li>research and development;</li> <li>restaurant;</li> </ul> </li> </ol>	<p>New special site policies expand on the vision for transit-supportive development around the Mississauga Hospital, with a mix of employment, community facilities, retail and service commercial uses and housing. These policies are an important component of achieving a balanced mix of uses within Downtown Hospital with a minimum amount of non-residential uses.</p> <p>In Areas A and C, three floors of non-residential uses will increase the number of jobs, services and amenities in Downtown Hospital. This location is intended to serve as a hub for health care services, research and development, and supportive uses that are accessible by walking or cycling.</p> <p>The policies also continue to permit funeral homes and nursing homes on lands identified as Area B.</p> <p>Additional land use permissions to allow for a broader mix of uses are proposed to support the continued operation of the Mississauga Hospital, and achieve the vision of a health district.</p>

Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<ul style="list-style-type: none"> <li>• retail store;</li> <li>• retirement building to a maximum building height of 25 storeys; and</li> </ul> <p>special needs housing, including a long-term care facility and a hospice to a maximum building height of 25 storeys.</p>	
<b>Former 12.5.4.3 Special Site 3 Policies</b>	<i>Deleted</i>	146	 <p>12.5.4.3.1 The lands identified as Special Site 3 are located along the west side of Hurontario Street, north of Harborn Road.</p> <p>12.5.4.3.2 Notwithstanding the provisions of the Office designation, the following additional policies will apply:</p> <ol style="list-style-type: none"> <li>funeral homes and nursing homes will also be permitted;</li> <li>vehicular access and internal circulation and parking must respect both the visual and functional quality of the street as well as the privacy of the residential areas to the west;</li> <li>efforts must be made to retain the existing vegetation cover maintaining the continuity of the area character;</li> <li>building height and form should provide a positive scale transition between Hurontario Street and existing residential development to the west;</li> <li>the building setback from the rear lot line should provide for the retention of natural vegetation to function as a buffer from residential uses to the west; and</li> </ol>	New policies expand on and replace existing Special Site 3 policies.



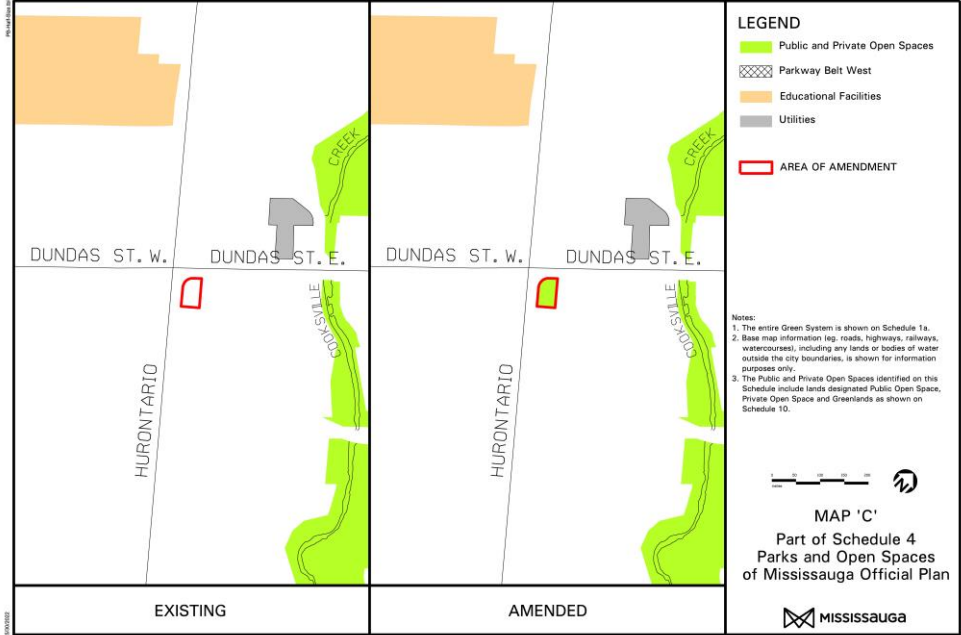
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			f. the lands identified as Area A, located at 2150 Hurontario Street, will be permitted to develop at a maximum <b>floor space index (FSI)</b> of 1.0.	
<b>Former 12.5.4.4 Special Site 4 Policies</b>	<i>Deleted</i>	146	 <p>12.5.4.4.1 The lands identified as Special Site 4 are located west of Hurontario Street, south of Queensway West.</p> <p>12.5.4.4.2 Notwithstanding the provisions of the Residential Low Density II designation on these lands, the following additional policies will apply:</p> <ol style="list-style-type: none"> <li>preserve and enhance the generous front, rear and side yard setbacks;</li> <li>ensure that existing grades and drainage conditions are preserved;</li> <li>encourage new housing to fit the scale and character of the surrounding areas, and take advantage of the features of a particular site, i.e. topography, contours, mature vegetation;</li> <li>garages should be recessed or located behind the main face of the house. Alternatively, garages should be located in the rear of the property;</li> <li>ensure that new development has minimal impact on its adjacent neighbours with respect to overshadowing and overlook;</li> <li>encourage buildings to be one to two storeys in height. The design of the building should de-emphasize the height of the</li> </ol>	Townhouses on lands in Special Site 4 are built, and as such, the Special Site policies are no longer needed.

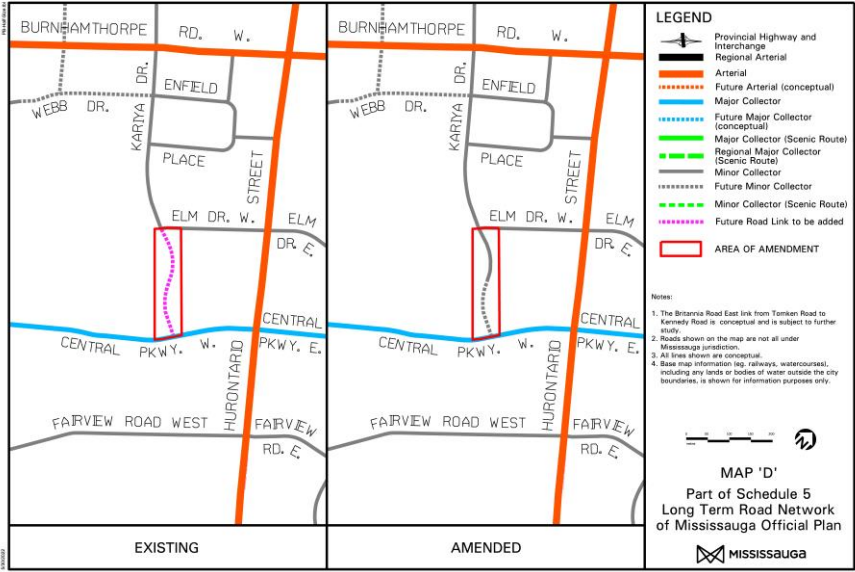
Section	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>house and be designed as a composition of small architectural elements, i.e. projecting dormers and bay windows;</p> <p>g. reduce the hard surface areas in the front yard;</p> <p>h. preserve existing mature high quality trees to maintain the existing mature nature of these areas;</p> <p>i. house designs which fit with the scale and character of the local area, and take advantage of the particular site are encouraged. The use of standard, repeat designs is strongly discouraged;</p> <p>j. the building mass, side yards and rear yards should respect and relate to those of adjacent lots; and</p> <p>k. a maximum height of three storeys will be permitted for street townhouses.</p>	
<b>Former 12.5.4.5 Special Site 5 Policies</b>	<i>Deleted</i>	146	 <p>12.5.4.5.1 The lands identified as Special Site 5 are located on the west side of Hurontario Street, north of Harbourn Road.</p> <p>12.5.4.5.2 Notwithstanding the policies of this Plan the following additional policies will apply:</p> <p>l. ground level commercial uses will be permitted;</p> <p>m. office uses will be permitted; and,</p> <p>n. maximum of two towers, one with a maximum height of 26 storeys and the second with a maximum height of 29 storeys, will be permitted.</p>	New policies expand on and replace existing Special Site 5 policies.

# Planning Rationale for Proposed Amendments to Mississauga Official Plan (MOP), Schedules

Schedule	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
<b>Schedule 1: Urban System</b>	<i>Revised</i>	146	<p><b>LEGEND</b></p> <p><b>GREEN SYSTEM</b></p> <p>Green System</p> <p><b>CITY STRUCTURE</b></p> <p>Downtown</p> <p>Major Node</p> <p>Community Node</p> <p>Neighbourhood</p> <p>Corporate Centre</p> <p>Employment Area</p> <p>Special Purpose Area</p> <p><b>CORRIDORS</b></p> <p>Corridor</p> <p>Intensification Corridor</p> <p><b>AREA OF AMENDMENT</b></p> <p><small>Note: Base map information (e.g. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.</small></p> <p><b>MAP 'A'</b> Part of Schedule 1 Urban System of Mississauga Official Plan</p> <p><b>MISSISSAUGA</b></p> <p><small>EXISTING AMENDED</small></p>	Schedule 1 is proposed to be revised to recognize the existing Cooksville Four Corners park.
<b>Schedule 1a: Urban System – Green System</b>	<i>Revised</i>	146		Schedule 1a is proposed to be revised to recognize existing Cooksville Four Corners park.

Schedule	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<p>LEGEND:</p> <p>AREA OF AMENDMENT</p> <p>Note: Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.</p> <p>MAP 'B' Part of Schedule 1a Urban System - Green System of Mississauga Official Plan</p> <p>MISSISSAUGA</p>	
<b>Schedule 4: Parks and Open Space</b>	<i>Revised</i>	146		Schedule 4 is proposed to be revised to recognize the existing Cooksville Four Corners park.

Schedule	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<div><p>LEGEND</p><ul style="list-style-type: none"><li>Public and Private Open Spaces</li><li>Parkway Belt West</li><li>Educational Facilities</li><li>Utilities</li><li>AREA OF AMENDMENT</li></ul><p>Notes:</p><ol style="list-style-type: none"><li>1. The entire Green System is shown on Schedule 1a.</li><li>2. Base map information (eg. roads, highways, railways, watercourses), including any lands or bodies of water outside the city boundaries, is shown for information purposes only.</li><li>3. The Public and Private Open Spaces identified on this Schedule include lands designated Public Open Space, Private Open Space and Greenlands as shown on Schedule 10.</li></ol><p>MAP 'C' Part of Schedule 4 Parks and Open Spaces of Mississauga Official Plan</p><p>MISSISSAUGA</p></div>	

Schedule	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
<b>Schedule 5: Long Term Road Network</b>	<i>Revised</i>	146		Schedule 5 is proposed to be revised to recognize the existing road segment of Kariya Drive, and its extension southwards to Central Parkway West, as a Minor Collector and Future Minor Collector road, respectively.



Schedule	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
Schedule 10: Land Use Designations	Revised	146		<p>Schedule 10 is proposed to be revised to:</p> <ul style="list-style-type: none"> <li>facilitate mixed use, transit-supportive development at 2 locations – a) the southeast corner of Hurontario Street and Central Parkway East; and b) southeast corner of Hurontario Street and Fairview Road East;</li> <li>protect for office employment uses by re-designating the lands with an existing office building located north of Dundas Street East along Hurontario Street;</li> <li>recognize townhouses located northeast of Agnes Street and Cook Street; and</li> <li>recognize existing uses at Cooksville Four Corners park and at Little John Lane.</li> </ul>
	Revised	146		<p>Schedule 10 is proposed to be revised to:</p> <ul style="list-style-type: none"> <li>facilitate higher density development on lands located on the western portion of Shepard Avenue, south of King Street East;</li> <li>facilitate mixed use development at the existing commercial plaza</li> </ul>

Schedule	Change	OPA No.	Proposed Revisions, Deletions or Additions to MOP	Planning Rationale
			<div><p>EXISTING LAND USE DESIGNATION</p><p>AMENDED LAND USE DESIGNATION</p></div>	<p>located on the northeast corner of Hurontario Street and the Queensway;</p> <ul style="list-style-type: none"><li>• reflect Minister's Zoning Order for the Mississauga Hospital lands; and</li><li>• recognize existing townhouses west of Hurontario Street, north of the QEW.</li></ul>

# **Downtown Fairview, Cooksville and Hospital Built Form Standards**

**Revised: June, 2022**

## **City of Mississauga**

Planning and Building Department  
Development and Design Division  
Urban Design Section

**June, 2022**

**All photos and illustrations provided by the Urban Design Division  
of the City of Mississauga, except where noted.**

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# Introduction

- 1.1** Purpose of the Standards
- 1.2** Objectives of the Standards
- 1.3** Expectations of the Standards
- 1.4** How to Read the Standards



## 1.1 Purpose of the Standards

The purpose of the Downtown Fairview, Cooksville and Hospital Built Form Standards (Standards) is to provide direction and guidance for proposed development at the planning application stage in order to assess, promote and fulfill the intent of the City's official plan policies, and zoning by-law - all of which shape and influence the urban structure, built form qualities of the character areas.

Selected content from this document, has been incorporated into the Downtown Fairview, Cooksville and Hospital Character Areas in the Mississauga Official Plan (MOP).

Applicants should also refer to Mississauga Zoning By-law, and Ontario Building Code to ensure that the applicable policies and requirements in these documents have been met. Furthermore, other City initiatives and special projects should be consulted to determine applicability, such as the city's strategic plan, green development strategy, shadow and wind study requirements.

soul of the City;

- Create a framework that promotes downtown development in a coordinated, comprehensive fashion through urban design standards;
- Facilitate the fair and consistent application of design objectives;
- Integrate a mix of uses, through appropriate built form, including commercial uses, offices, residential, cultural, entertainment and institutional uses — the whole of which is intended to put people in close proximity to a broad range of urban amenities and experiences;
- Achieve a high quality built form and strengthen the continuity of buildings that contribute to the emerging urban context of the three character areas;
- Ensure that development is resilient, environmentally friendly, safe and universally accessible; and
- Foster compact, pedestrian and transit-oriented development that achieves vibrant street level activity and a public realm of the highest standard.

## 1.2 Objectives of the Standards

Chapter 9 of MOP, 'Build a Desirable Urban Form' along with the policies contained in Chapter 12 'Downtown' set the urban design and built form policies to support the vision for a vibrant Downtown.

These built form standards act to articulate the policies in addition to the following objectives:

- Support the City of Mississauga's Strategic Plan and its Strategic Pillars for Change. A paramount goal is to create a vibrant downtown that functions as a strong economic centre, while acting as the civic heart and

## 1.3 Expectations of the Standards

The Standards provide further direction on the urban design policies set out in MOP and in addition to other city initiatives that support land use decisions and strategies for Downtown Fairview, Cooksville and Hospital.

The Standards set out detailed requirements to achieve a high quality built form that interfaces with the public realm in a seamless fashion. The Standards have been developed to communicate the design expectations, in advance of an application being filed, related to the quality and outcome of development.

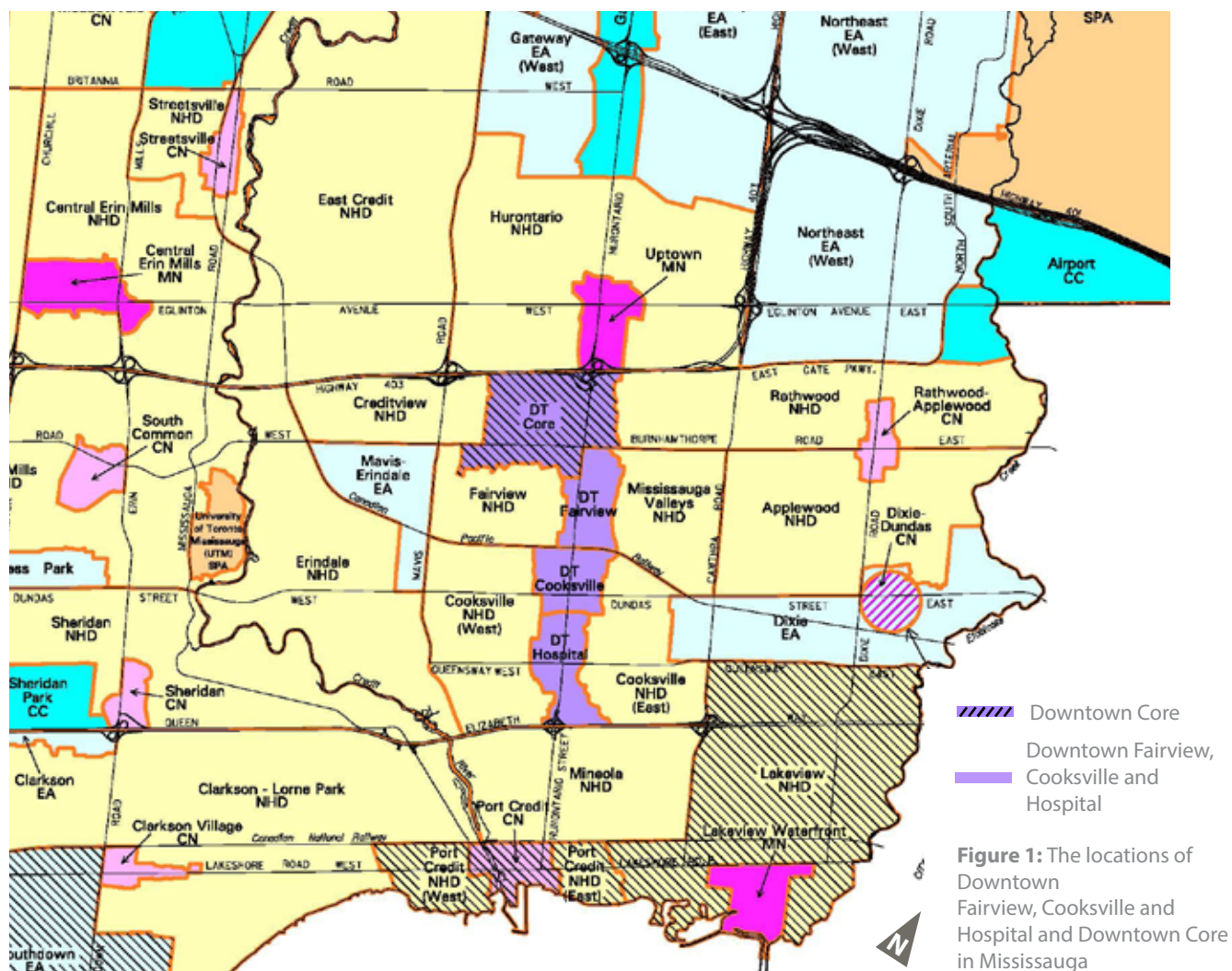
It should be noted that the Standards may be amended, modified or updated on an as-needed basis to provide clarity on the intents of the City of Mississauga's Official Plan, provisions of the zoning by-law including the outcome of other studies or initiatives that affect Downtown Fairview, Cooksville and Hospital.

# 1.4 How to Read The Standards

The rationale of the Standards is best understood by reviewing all sections, text and diagrams, including the policies cited within the Mississauga Official Plan. The Standards reflect an integrated approach to Downtown Fairview, Cooksville and Hospital, through building frontage standards that guide and direct the general disposition of buildings through form-based design.

Moreover, the Standards are also contingent on

an understanding that downtowns are complex urban places that require an overarching organizing structure with a view of creating a coherent, legible, high quality public realm and memorable sense of place.



# General Standards

## **2.1** General Built Form Standards

- 2.1.1** Ground Floor Setbacks
- 2.1.2** Podium Stepbacks
- 2.1.3** Floor Plate Sizes
- 2.1.4** Tower Separation
- 2.1.5** Building Articulation
- 2.1.6** Building Top
- 2.1.7** Height Variation

## **2.2** Architectural Design Guidelines

- 2.2.1** Main Street Character
- 2.2.2** Choice of Materials
- 2.2.3** Building Corners and Sides
- 2.2.4** Parking Structures
- 2.2.5** Service, Parking and Loading

## 2.1 General Built Form Standards

### 2.1.1 Ground Floor Setbacks

Ground floor setbacks vary depending on the required uses on the ground floor, the character and configuration of the adjacent development and the required use on the ground floor. The setback also should provide enough space for appropriate landscape and streetscape treatment.

On streets where retail and service commercial is required, the ground floor setback from the property line should be between 2.0 metre and 4.0 metre from the property line to maintain a frontage that is close to the street.

On residential streets, the setback to a building shall be a minimum of 4.5 metre and a maximum of 6.0 metre to provide an adequate buffer between the private and public realms.

### 2.1.2 Podium Stepbacks

Podium heights will be a minimum of 3 storeys and a maximum of 6 storeys (20 meters) with a minimum 3 - 6 meter stepback between the podium face and tower, depending on the site context.

Podium heights for buildings located in Downtown Cooksville - Special Site 1 (MOP) will be a minimum of 3 storeys and a maximum of 4 storeys (14 meters) with a minimum 6 meter stepback between the podium face and tower.

Further details on Podium and Ground Floor Setbacks are provided in Sections 3 of these Standards.

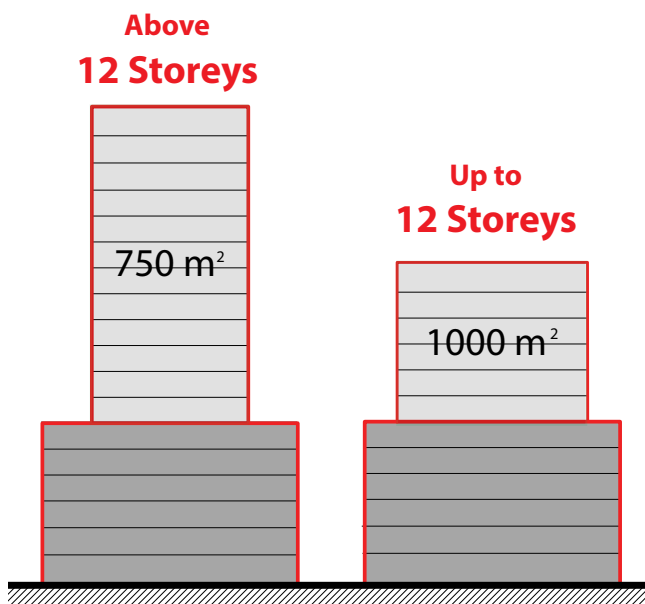
### 2.1.3 Floor Plate Sizes

For buildings up to 12 storeys tall the maximum allowable tower floorplate above the podium is 1000 square meters.

For buildings greater than 12 storeys, the maximum tower floorplate is 750 square metres

### 2.1.4 Tower Separation

Towers must be separated at least 30 metres, measured from the tower face (excluding projections such as balconies).



**Figure 2:** Maximum allowable floorplates above the podium are either 750 square meters or 1000 square meters, depending on building's overall height.



**Figure 3:** Tower separations and setbacks are measured from the outermost edge of the tower or podium face.

To maintain the 30 m separation distance, towers must have a minimum setback of 15 m from the rear and side yards.

### **2.1.5 Building Articulation**

The podium levels must be designed to appear to be broken down into multiple parts by inseting small parts of the floor plan, articulating balconies differently, and varying the massing and materials.

### **2.1.6 Building Top**

The top 2 to 3 levels of the building should be stepped back 3 meters to create visual relief at the top of the building.

The rooftop mechanical penthouse will be designed to stepped back and compliment the architecture of the building.

### **2.1.7 Height Variation**

In a development with multiple towers, the height of no two towers can be the same. The height of the towers must be varied by 3 to 5 storeys.



## 2.2 Architectural Design Guidelines

### 2.2.1 Main Street Character

#### 2.2.1.1 Street Wall Continuity

A street-wall is a single entity that is composed of the different exterior walls of buildings that face the same public right-of-way. A defined streetwall occurs when different building facades are coordinated to maintain visual continuity and a consistency in massing.

To ensure street wall continuity:

- Locate setbacks on development blocks to inform the orientation and placement of buildings and streetwalls;
- Coordinate setbacks with adjacent properties in order to create consistent edges and street walls along frontages;
- Locate buildings parallel to the street, with streetwalls placed at the setback, to contain the street and provide enclosure;
- Ensure a variation in setbacks along the building frontages to articulate façades and allow for visual interest, accommodate outdoor patios, recessed entries and landscaped areas; and
- Avoid blank walls on all street frontages unless designed as an articulated, finished architectural element.

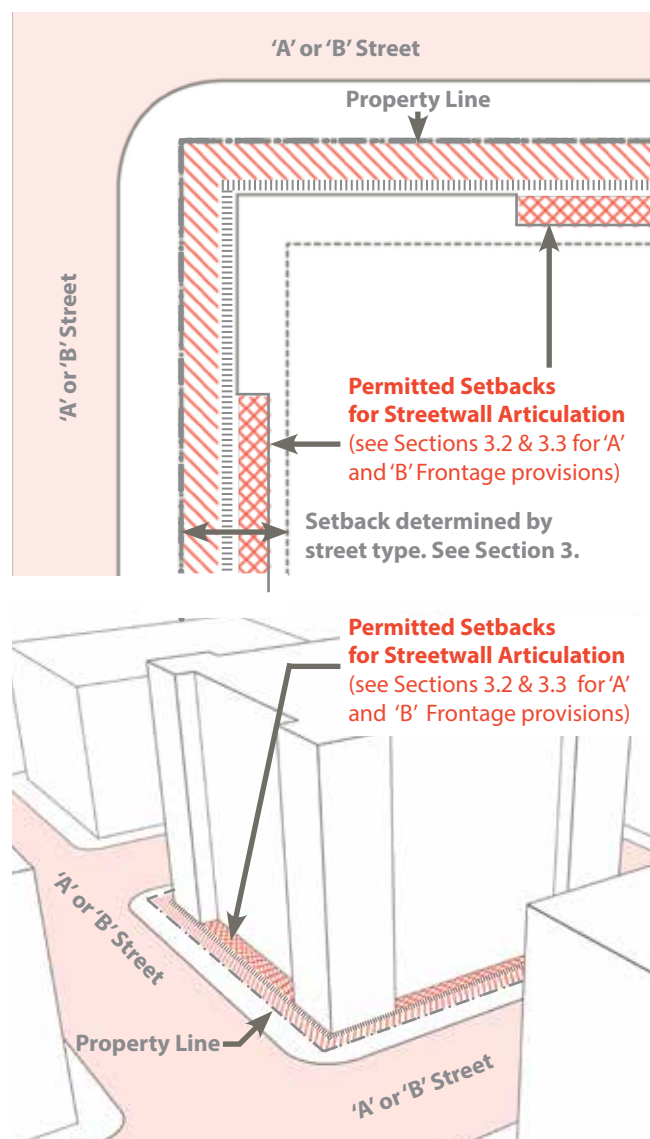
#### 2.2.1.2 Façade Composition

When composing the building's podium ensure that the façade addresses the human scale. Building facades should be parallel with the street and provide periodic indentations for visual relief. Include features such as forecourts, plazas with streetscape amenities (such as trees, seating and public art) to improve the overall pedestrian experience.

Consider distinguishing three portions by having a base, a middle portion and a top portion or cap by:

- Reinforcing ground floor uses and access by integrating retail, entrances and lobbies

**Figure 4:** Development is located parallel to the street and placed at the required setbacks. The streetwall can step-back to accommodate façade articulation, patios and entrances.



together and establishing a clear base;

- Defining a middle portion of the street-wall which has material treatment that is separate from the base and the top portion of the street-wall; and
- Terminating the street-wall with a roof-line, a parapet, a juncture or a storey that further expresses the street-wall and the character of the building.
- Locate main entrances flush with the public sidewalk;
- Ensure site designs relate to and interface with existing, proposed and future transit stops and facilities; and
- Locate main building entrances so that they are clearly identifiable and prominent with direct access to the public sidewalk, pedestrian connections and transit facilities.

### 2.2.1.3 Storefronts and Bays

Storefronts must be designed with a traditional main street character that have regular sized bays with a combination of glazed storefronts between solid piers. Large expanses of glazed areas should be avoided.

- Incorporate a balance in the proportion of glazing to solid materials as well as openings to enclosure in the design of facades in order to achieve pleasing symmetries and legible asymmetries;
- A signage band should be included above the glass storefronts as a controlled location for signage;

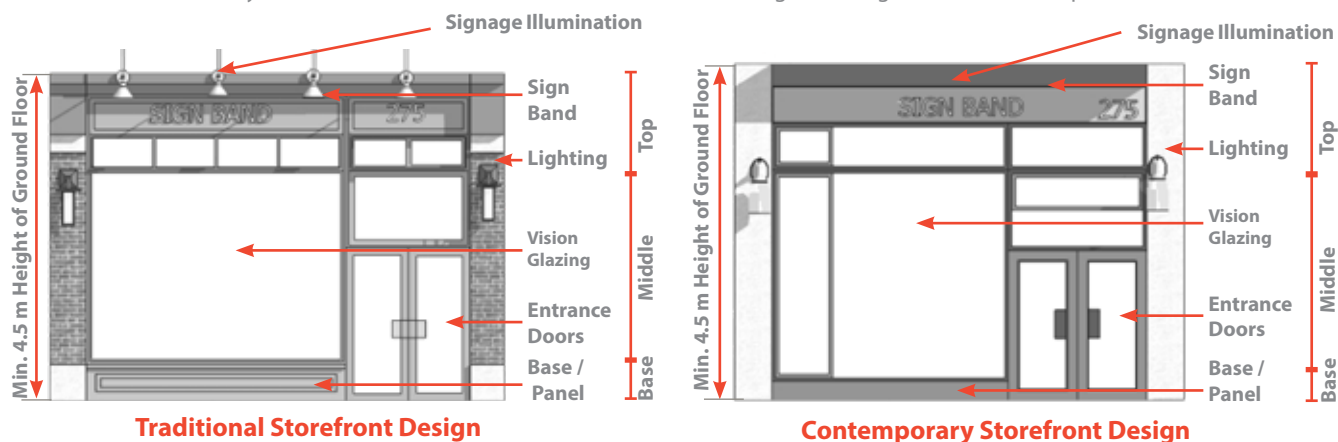
### 2.2.1.4 Awnings and Canopies

Continuous awning and canopies should be located above the storefronts approximately 3 – 5 meters above grade and provide wind protection over the storefronts and entrances.

### 2.2.1.4 Balcony Projections

Balconies should not project over the ground floor storefronts. Balconies must be setback from the face of the storefront. See sections 3.1 and 3.2 for more details.

**Figure 5:** Storefronts can incorporate a variety of styles and character to animate the street edge. When well executed, they contribute to the identity of the street and make retail and commercial frontages inviting and accessible to pedestrians



### 2.2.2 Choice of Materials

Construct building exteriors from durable, natural materials such as clay brick, stone, metal, glass and wood. Energy efficient, salvaged, recycled or reused building materials are also highly encouraged.

Building materials such as stucco, metal siding, embossed face brick panels and vinyl are generally discouraged as they do not age well, are not durable and are prone to weathering.

Deploy a hierarchy of materials with solid, or 'heavier' materials located closer to the ground as a means to visually anchor the building to grade.

### 2.2.3 Building Corners and Sides

Design building located at prominent corners with upgraded elevations and design features to create a focal point.

Articulate corner units with an ample setback to create space for patios and outdoor merchandising that will animate the corner.

Parti walls should be upgraded and not be exposed more than 5 storeys.

### 2.2.4 Parking Structures

Parking facilities will have an important role to play in supporting key uses, attractions and urban amenities. In general, development will locate structured parking and vehicular access to minimize impacts on the property and on surrounding properties and to improve the safety and attractiveness of adjacent streets, parks and open spaces. The following standards will apply:

**2.2.4.1** Above-grade parking structures will not directly front onto 'A' Streets\*, but will be entirely screened by 'liner' buildings incorporating a mix of buildings between the parking structure and street space;

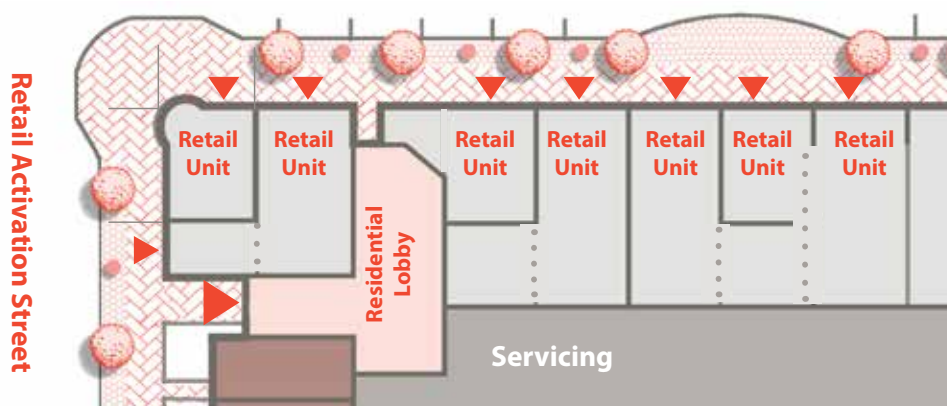
**2.2.4.2** Liner uses should have a minimum depth of 10.0 m;

**2.2.4.3** On the ground floor, parking structures will have active uses such as commercial or retail services with an appropriate scale and architectural expression to support activity on the streets, including those fronting onto parks and/or open spaces;

**2.2.4.4** Parking structures should front C-Streets\*. if not possible, parking structures will

*\*Please refer to Character Area Maps in Section 3 to find the location of 'A', 'B' and 'C' streets.*

**Figure 6:** Interior demising walls for retail units can be configured to promote an expression of narrower units and smaller retail spaces along building frontages, opening up beyond to combine units into larger retail spaces.



only directly front onto one 'B' Street\* where more than one 'B' street frontage exists;

**2.2.4.5** When fronting onto 'B' Streets, parking structure elevations will be designed to the highest level of architectural treatment and animation to screen views of the interior;

**2.2.4.6** Entrances, lobbies and passageways that provide a convenient means of access to parking facilities will be fully enclosed, appropriately signed and integrated into the façade design without appearing as dominant elements on the street frontages;

**2.2.4.7** At grade exhaust vents serving structured parking facilities should be:

- Screened from public view; and
- Integrated into the design of buildings, expressed as part of the architectural character.

## 2.2.5 Service, Parking and Loading

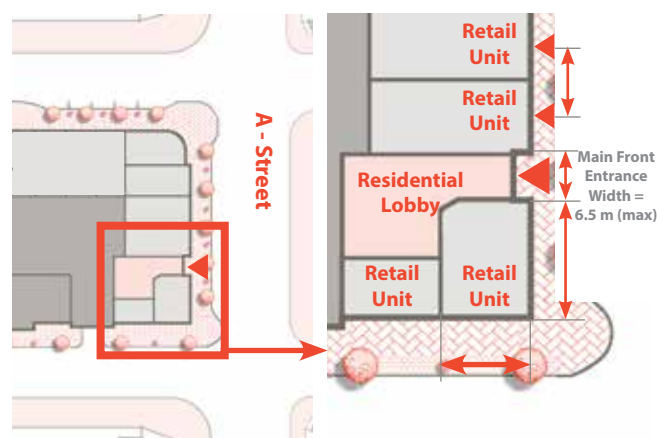
Service, parking and loading should be coordinated on sites by providing consolidated locations that can serve a number of buildings simultaneously from one area;

**2.2.5.1** Loading, garbage and service spaces will:

- Be located internal to the building to avoid noise and visual impacts;
- Prioritize pedestrian safety and the appeal of the public realm by incorporating special architectural treatment and using soft and hard landscape treatments to screen loading and servicing areas; and
- Provide safe levels of illumination and lighting.

**2.2.5.2** The height required for overhead loading for bulk refuse within a collection area should conform to the Region on Peel's standards.

**Figure 7:** Main front entrances serving residential uses above the ground floor will be located to minimize interruption of retail units along A-Streets.



# Character Areas and Street Types

- 3.1** Character Areas
  - 3.1.1** Downtown Fairview
  - 3.1.2** Downtown Cooksville
  - 3.1.3** Downtown Hospital
- 3.2** A-Streets
  - 3.2.1** General Standards for A-Streets
  - 3.2.2** A-Street Subcategories
- 3.3** B-Streets
  - 3.3.1** General Standards for A-Streets
- 3.4** C-Streets and Pedestrian Connections
  - 3.4.1** General Standards for C-Streets
  - 3.4.2** General Standards for Pedestrian Connections
- 3.5** Transition From Established Neighbourhoods

## 3.1 Character Areas

### 3.1.1 Downtown Fairview

#### Introduction

Downtown Fairview is situated on the periphery of Mississauga's Downtown Core and provides an area of transition to Downtown Cooksville. Downtown Fairview contains a number of sub-areas, each with its own unique character and built form.

Along Mississauga Valley Boulevard, 'tower in the park' developments, with generally 20 to 24 storey apartment buildings are surrounded by large open space areas with connections to trails and the Cooksville Creek. The area at Hurontario Street and Elm Drive West contains the greatest densities in Downtown Fairview, with point towers as tall as 50 storeys. The townhouses located in the southern end of Downtown Fairview are the predominant built form and provide housing for families.

There are limited retail and commercial uses along Hurontario Street, however with planned higher order transit, new developments can transform the street into vibrant places to shop, work and visit. Anchored by a grocery store, the existing retail site located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard provides convenient shopping for nearby residents.

The Cooksville Creek is a significant natural heritage feature that runs north-south in Downtown Fairview and drains into Lake Ontario. Opportunities to enhance the ecological features of existing natural habitats and the urban forest will be encouraged.

The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.

#### Vision

Downtown Fairview will be a distinct, walkable community that provides a transition to lower heights and densities from the Downtown Core. With more shops and amenities along Hurontario Street, Downtown Fairview will be an attractive place where people can access their daily needs within a short distance from their homes and places of work. Mixed use development located at the northeast corner of Central Parkway East and Mississauga Valley Boulevard will serve the retail and commercial needs of the local community. The area along Mississauga Valley Boulevard will continue to be defined by existing towers in the park and beautiful green spaces, with improved connections to trails and Cooksville Creek.

An expanded pedestrian network will connect new development with nearby amenities and residential areas, and improve access to transit, parks and open spaces.

#### Guiding Principles

1. Heights and densities that are appropriate to the planned context. The greatest heights will be located at Hurontario Street and Elm Drive West, with a transition to lower building heights and densities for new developments south along Hurontario Street and east towards Mississauga Valley Boulevard;
2. An attractive, walkable environment along Hurontario Street with a vibrant mix of shops, restaurants, cafes and service establishments;
3. Accessible public spaces for all, with enhanced connections for walking and cycling; and
4. A mix of housing forms and tenure with a range of housing options that meet the needs of a diverse community.



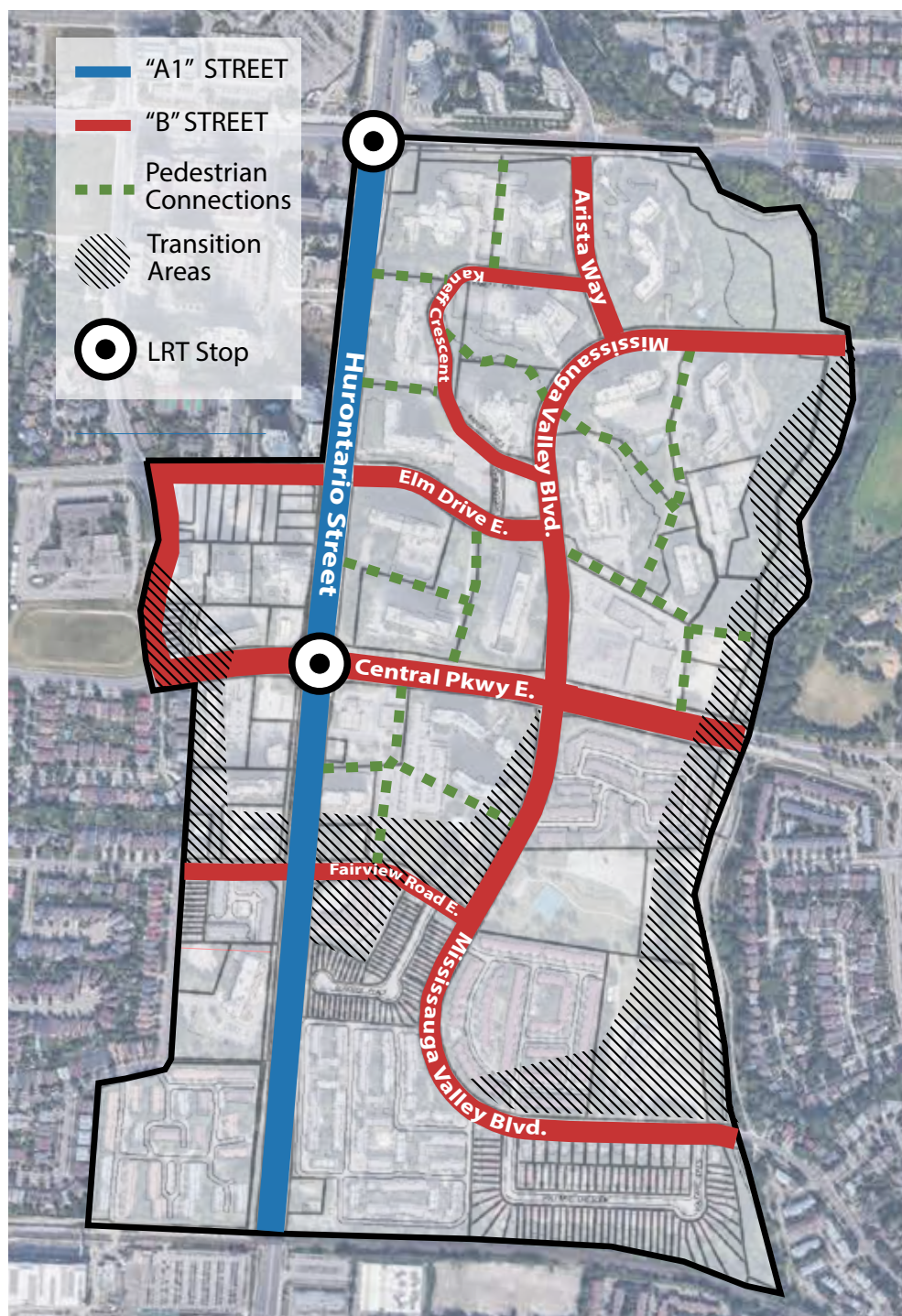


Figure 8: Street Types in the Downtown Fairview Character Area

### 3.1.2 Downtown Cooksville

#### Introduction

Centred at Hurontario Street and Dundas Street, Downtown Cooksville has a rich history as a destination for newcomers to Canada and as a place for entrepreneurs and small businesses to set up shop and thrive. The cultural diversity of the neighbourhood is reflected in the variety of shops, restaurants and services that are available in the area. Immigrant programs and services in Downtown Cooksville provide an important function in helping families transition to a new country.

Hurontario Street and Dundas Street is generally characterized by retail plazas that are one to two storeys with residential and/or office uses above and surrounded with surface parking. There are a few office and residential buildings along the corridor that are generally of a low or mid-rise form.

With the potential for two-way, all-day GO Transit service at Cooksville GO Station, planned Bus Rapid Transit (BRT) along Dundas Street and the Hurontario Light Rail Transit (LRT), significant transit investments will strengthen its connection to the rest of Mississauga and beyond. These transit investments will increase access to employment, shopping, entertainment and other destinations, while also attracting more visitors. Transit-supportive development will ensure transit investments are supported with greater access to housing, jobs and services.

The Cooksville Creek is a significant natural heritage feature in Downtown Cooksville. Cooksville Creek serves as a wildlife corridor and linkage to other features within and beyond Downtown Cooksville. This corridor is also used for recreation with opportunities for restoration, rehabilitation and hazard mitigation along the watercourse.

The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.

#### Vision

Downtown Cooksville will be a walkable mixed use community with places for community amenities, culture and art. Hurontario and Dundas streets will be animated with storefronts and other active uses that support existing and future local businesses. Downtown Cooksville will continue to offer a diversity of local retailers and restaurants. The area immediately surrounding the intersection of Hurontario and Dundas Streets (Cooksville Corners) will be developed with a vibrant main street focus that includes a mix of uses and a human-scaled built form. The Cooksville GO Station area will be a focal point for transit-supportive higher density development with a concentration and mix of residential, community, office, retail and service commercial uses.

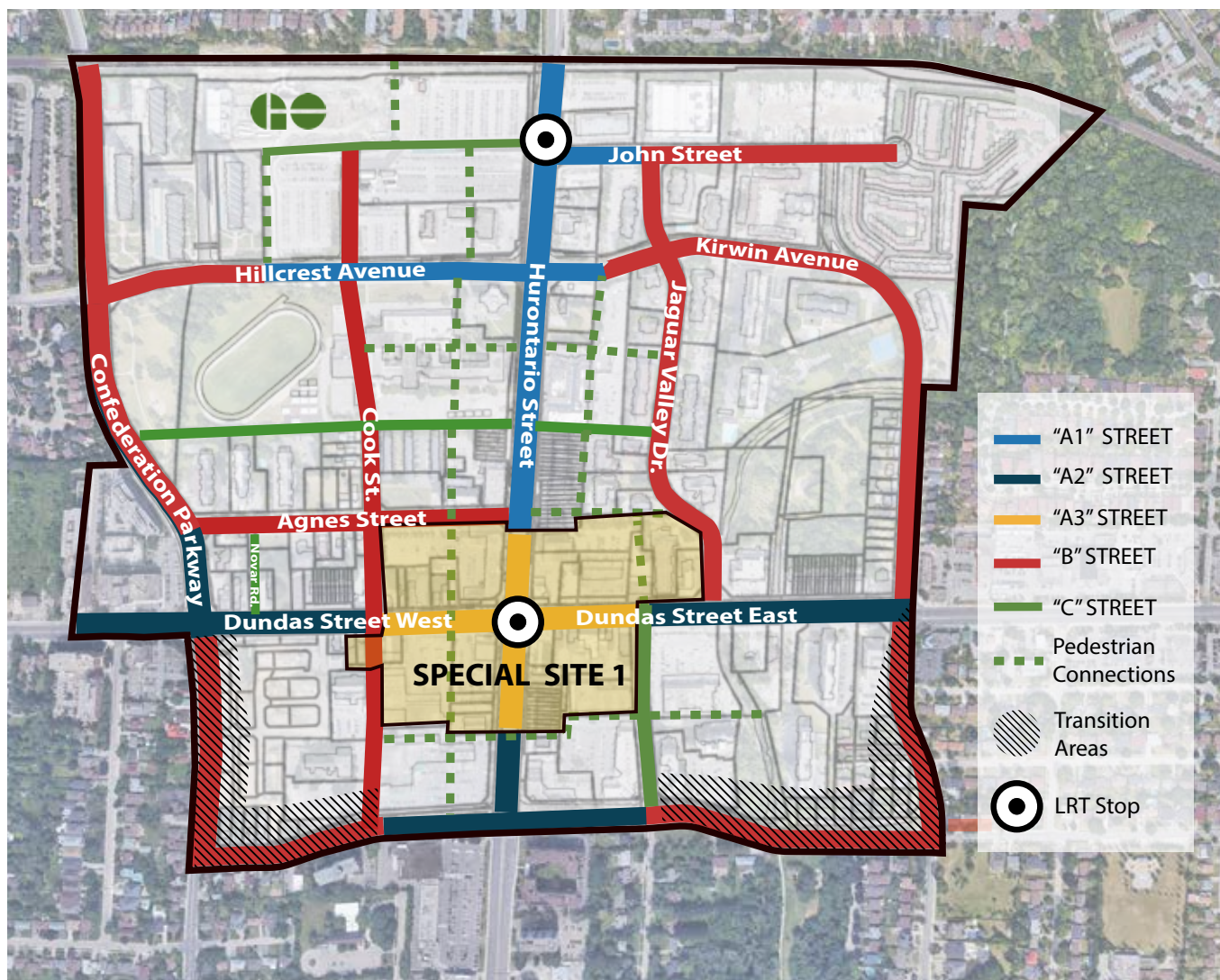
#### Guiding Principles

1. Establish a mixed use, vibrant community – create a range of local and unique businesses along Hurontario and Dundas streets, spaces for educational, cultural and community uses, a vibrant public realm, and a network of connected parks and open spaces;
2. Plan for more people and employment – provide a range of housing options both in type and affordability, and jobs with the greatest heights and densities located in proximity to the Cookville GO Station;
3. Achieve a walkable, connected community – promote a pedestrian-oriented main street



character along Hurontario and Dundas streets, public art and a vibrant public realm, and improve pedestrian and cycling infrastructure; and

4. Plan for high quality transit – provide convenient and efficient transit with seamless connections to local and higher order transit services.



**Figure 9:** Street Types in the Downtown Cooksville Character Area

### 3.1.3 Downtown Hospital

#### Introduction

The focal point of Downtown Hospital continues to be the Mississauga Hospital, serving as one of Canada's leading health centres with specialized regional programs. Downtown Hospital consists primarily of residential high density uses. Small pockets of low rise dwellings are located on the edges of the Character Area. Office uses are located in the area surrounding the hospital along Hurontario Street and Queensway. Mixed commercial uses are clustered at three main intersections along Hurontario Street at King Street, Queensway and North Service Road.

Cooksville Creek is a significant natural heritage feature and portions of the creek have been impacted by channelization associated with flood and erosion control and storm water management. Enhancements to the ecological features of existing natural habitats and the urban forest will be encouraged.

The existing open space network is primarily concentrated adjacent to Cooksville Creek. To meet growth needs, opportunities for additional parkland and improvements to existing parks will be pursued.

#### Vision

Downtown Hospital will be developed as a health district, anchored by the hospital with a broad range of uses clustered around the Mississauga Hospital, supporting more retail and service commercial uses, research and development, health facilities, offices, parks and residential uses.

Transit-supportive development along Hurontario Street and Queensway with maximum heights of 25 storeys for new residential development will ensure greater access to housing, jobs and services.

#### Guiding Principles

1. Support health care services – promote a range of health care services, supportive uses and research and development that create a healthier community; and
2. Create a more walkable and transit-supportive community – provide pedestrian connections to Hurontario Street and Queensway, and street related retail and service commercial uses along these corridors.



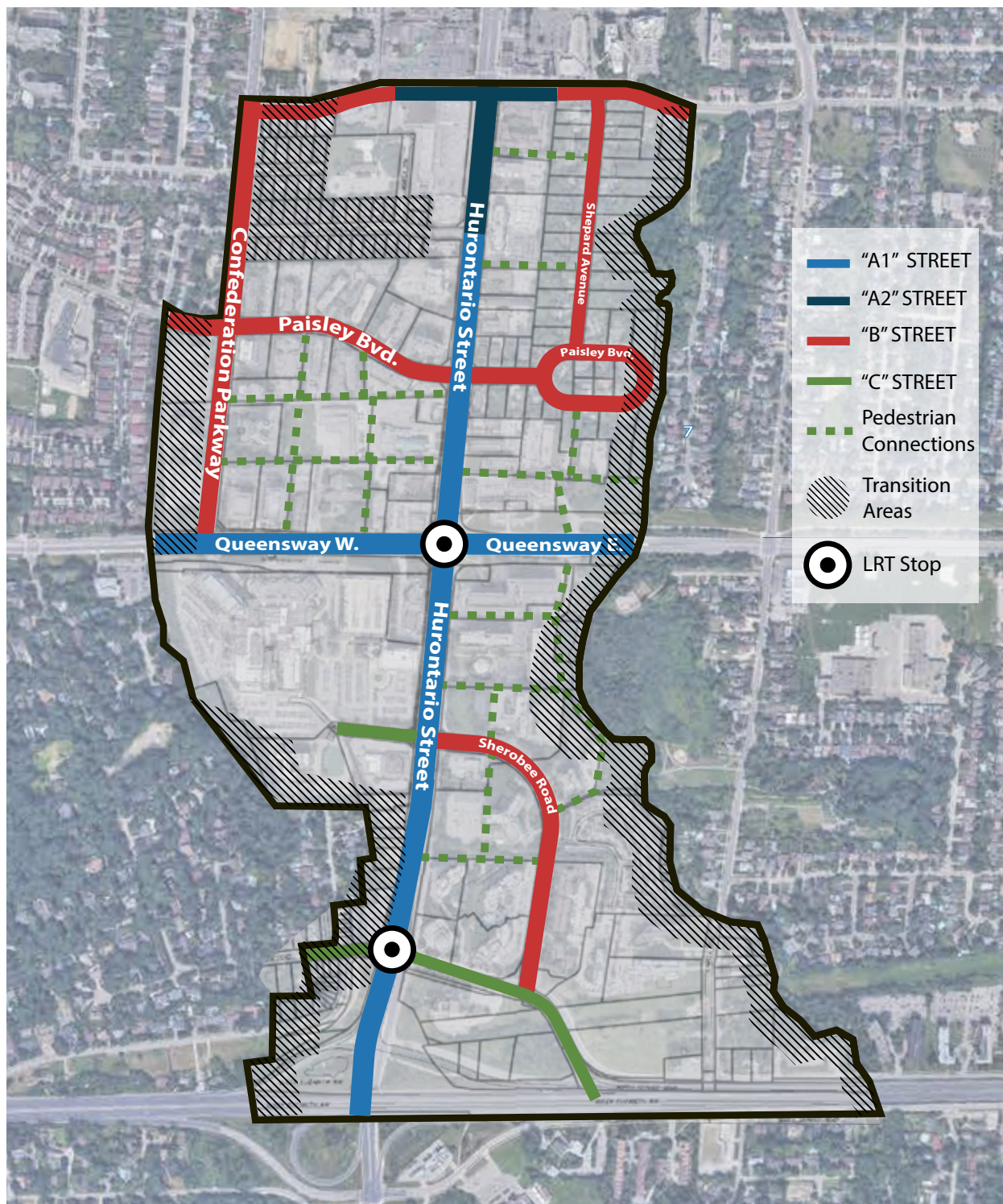


Figure 10: Street Types in the Downtown Hospital Character Area

## 3.2 A-Streets

A-streets are considered to be primary arteries and must incorporate at grade commercial and/or retail uses at grade. The design of A-Streets are critical to develop a lively urban environment that fosters active uses and a pleasant pedestrian environment.

A-Streets are distinct from secondary B-Streets, that are residential in character. B-Street provisions are discussed in Section 3.3

### 3.2.1 General Standards for A-Streets

The following standards will apply to all streets A-Streets in the three Downtown Character Areas:

**3.2.1.1** Provide continuous buildings along 'A' Streets to maintain street continuity. A minimum of 90% of a property's frontage is required to be occupied by the streetwall that is located within 2.0 m. to 4.0 m from the property line. Up to 25% of the building frontage can be further setback to allow for outdoor patios, recessed entries and landscaped areas;

**3.2.1.2** Ground floor heights will be a minimum of 4.5 metres (floor-to-floor, measured from established grade) to accommodate retail and commercial uses with windows that correspond to the height of ground floors;

**3.2.1.3** Retail entrances should be located on primary frontages along A-Streets and provide a variability in scale of retail spaces to contribute to a healthy retail economy along A-Streets.

**3.2.1.4** Where retail units occupy prominent corner locations, units should be designed to:

- address the corner with well designed storefronts that wrap the corner; and
- incorporate corner entrances.

**3.2.1.5** Entrances to retail and commercial tenant spaces will be operational, flush and directly at the grade of the public sidewalk;

**3.2.1.6** Storefronts are articulated externally to breakdown the apparent width with a frequent rhythm that supports the pedestrian experience at the street level;

**3.2.1.7** Entrance doors to retail units built to the property line should be recessed in order to minimize conflicts between door swings and pedestrians;

**3.2.1.8** Ensure that ground floors containing storefronts articulate:

- a strong sense of rhythm and pattern collectively across frontages;
- subtle recesses and reveals to create depth and visual interest; and
- assist pedestrians with spatial perception, orientation and accessibility along retail frontages.

**3.2.1.9** At grade retail and commercial uses will incorporate the highest standard of storefront design consisting of:

- Durable, high quality materials such as metal, steel, glass, natural stone and brick;
- Vision glazing and doors that allow for views into and out of storefronts;
- Elements such as mullions, glazing bars and transoms to help frame, divide and define storefront window sections and apertures;and
- Dark tinted, reflective or opaque glazing placed on storefront windows are discouraged.



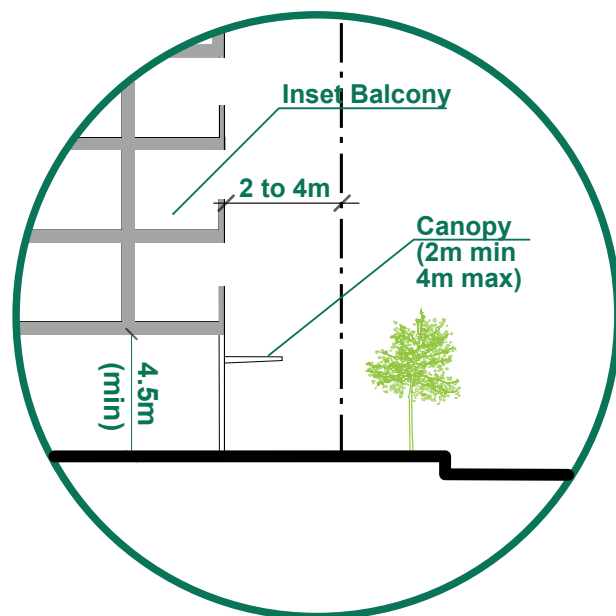
**3.2.1.10** Consistent signage and lighting that integrates with the storefront design, and which complements or acts as an extension of the buildings architectural character;

**3.2.1.11** The incorporation of architectural cantilevers, fixed canopies, awnings or similar features into a proposed development are encouraged to achieve pedestrian scale, comfort and weather sheltered pedestrian routes. These should complement the architectural character of the building and be made of highly durable materials;

**3.2.1.12** Where appropriate, patios should be provided adjacent to retail/commercial frontages or entrances to promote activation of the street and vibrancy; and

**3.2.1.13** Patios should have a depth of at least 3.5 m and can be recessed into the building as part of the permitted setback from the build-to line, or placed beyond the build-to line along the spill-out zone providing that there is sufficient space. A clear sidewalk width is required to ensure pedestrian traffic flow is not disrupted.

**3.2.1.14** Balconies on A-Streets should be inset and cannot project from the podium or tower portion of the building.



**Figure 11:** Storefronts when well executed, promote a vibrant street life and the pedestrian experience.  
*Downtown Mississauga, Living Arts Dr & Square One Dr*



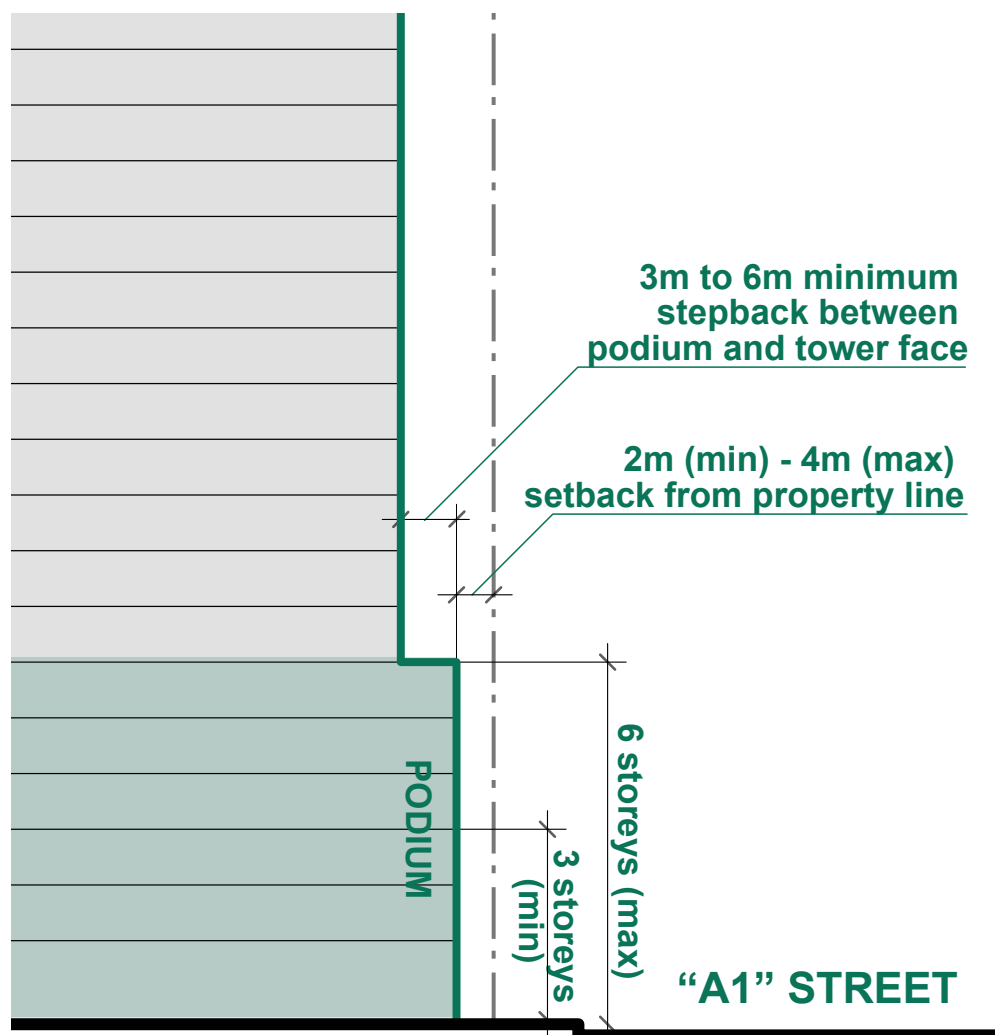
**Figure 12:** Ground floor conditions on all A - Streets. Balconies should be inset and any canopy projections should be between 2 and 4 meters.



### 3.2.2 A-Streets Subcategories

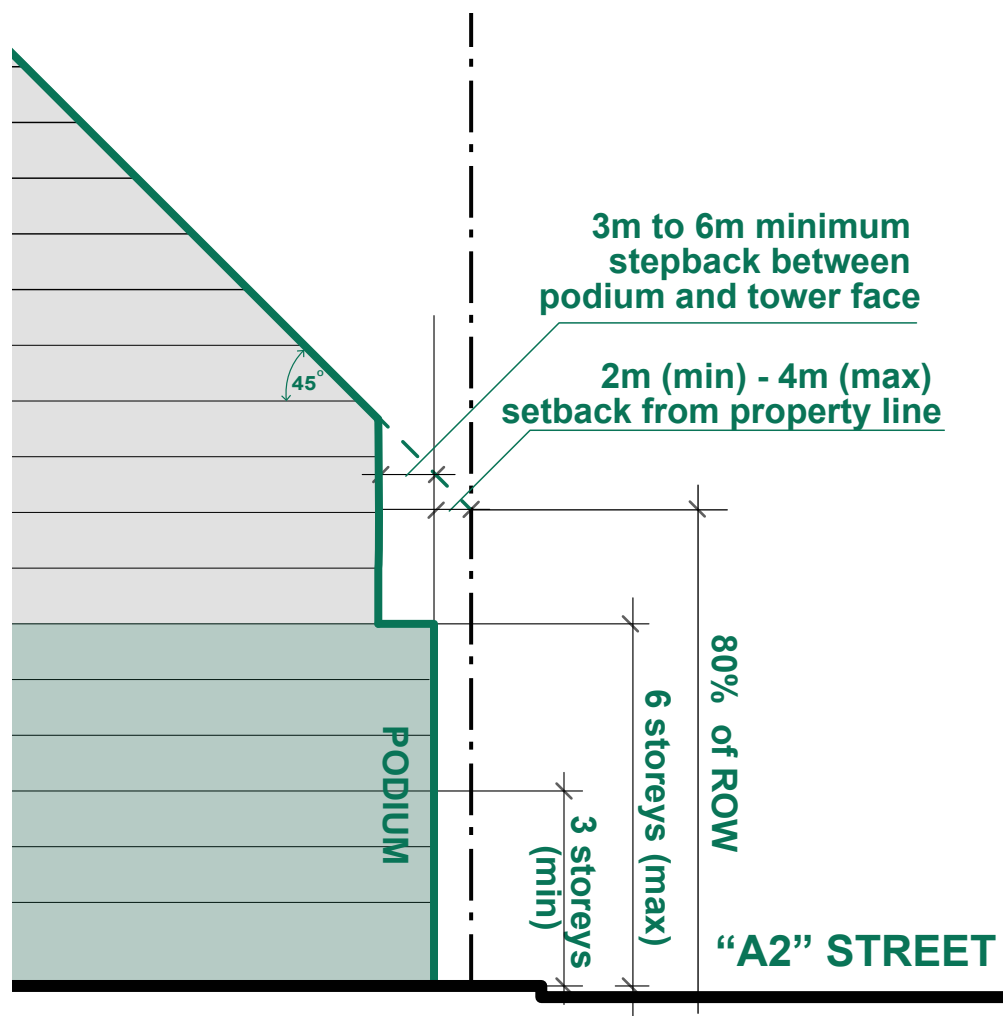
A-Streets are further divided into A1, A2 and A3 Streets with each sub-category having a variation in the built form, the required setback, podium height, step back and angular plane requirements.

These dimensions should always be revised to consider context. For example, where trees cannot be accommodated on boulevards, they will be required to be on private property, as determined during the application review process.



**Figure 13:** Frontage on an A1- Street with the required podium height, setbacks and stepback

### 3.2.2 A-Streets Subcategories

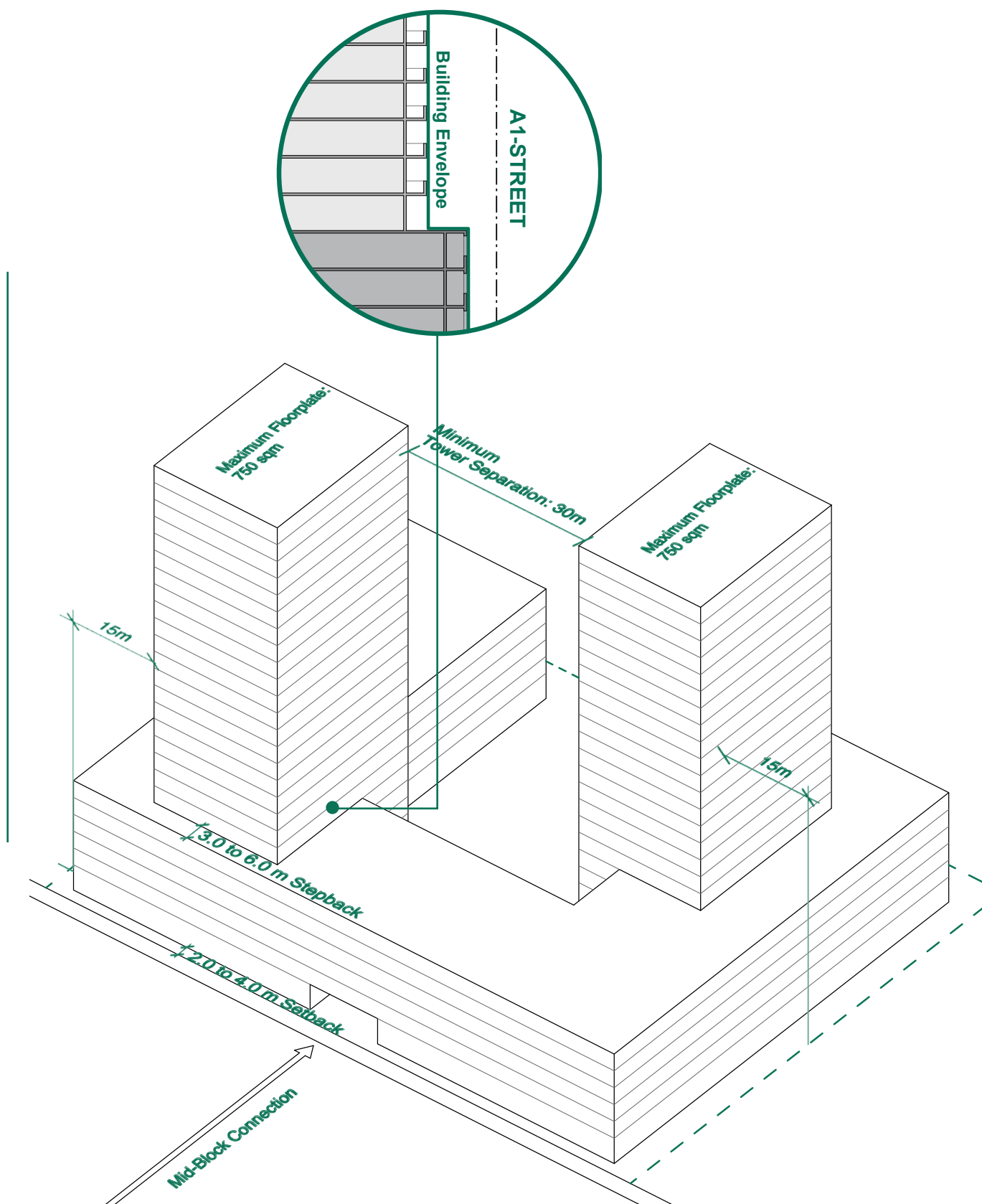


**Figure 14:** Frontage on an A2- Street with the required podium height, setbacks, setback and angular plane.

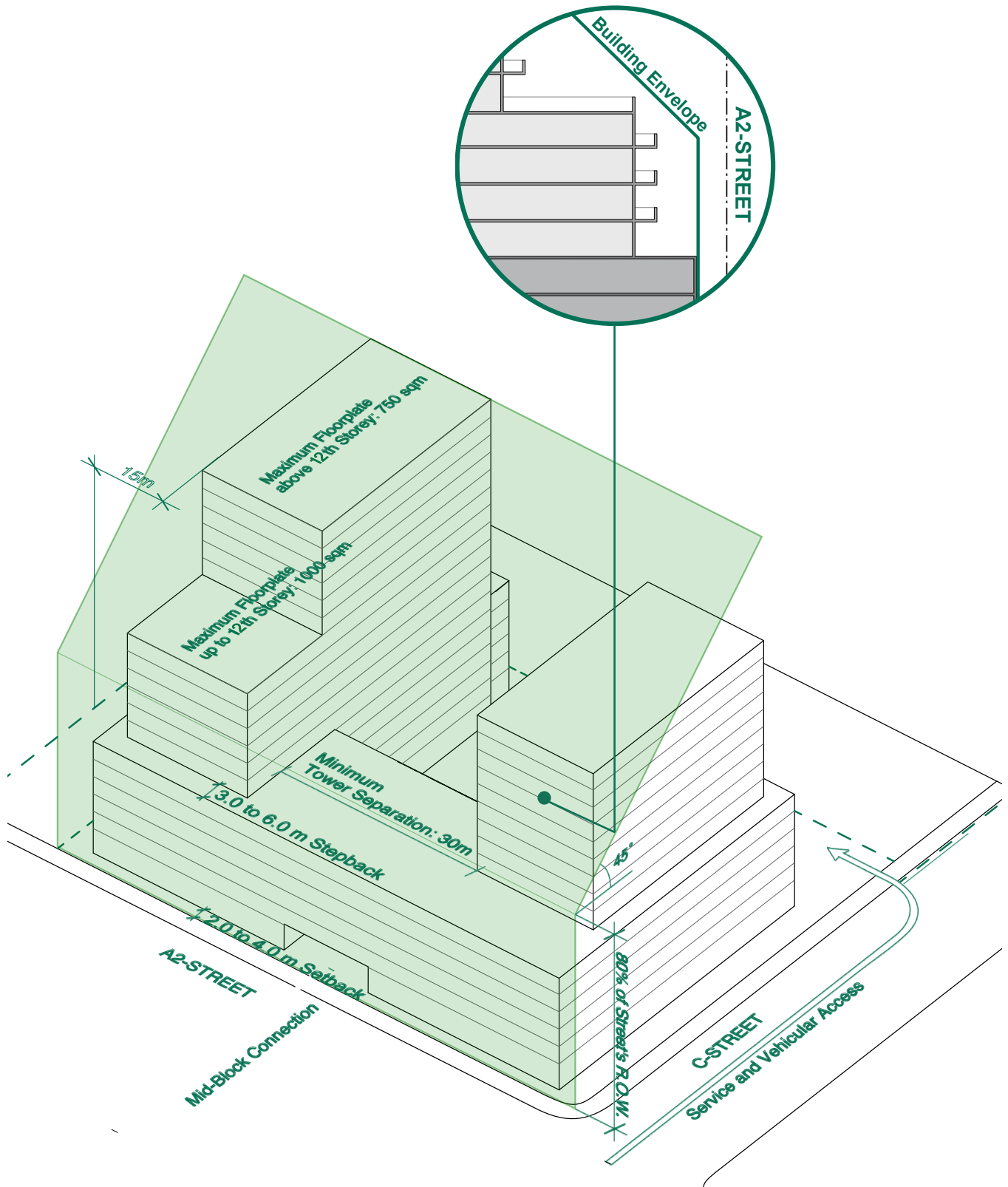
Diagram illustrating building setbacks and height restrictions:

- 6m (min) setback between podium and tower face**
- 2m (min) - 4m (max) setback from property line**
- 80% of ROW**
- PODIUM**
- 3 storeys (min)**
- 4 storeys (max)**
- "A3" STREET**

**Figure 15:** Frontage on an A3- Street with the required podium height, setbacks, stepback and angular plane.



**Figure 16:** Frontage on an A1 Street. Tower separations, setbacks and floor plate sizes are also indicated.



**Figure 17:** Frontage on an A2 Street with tower separations, setbacks, angular plane and floor plate size requirements indicated.



## 3.3 B-Streets

B-streets are secondary streets and are more commonly residential in nature. When the ground floor does not have retail uses, entrances should be raised from the sidewalk between 0.6 meters and 1.2 meters above grade and setbacks and landscaping should be designed in a manner that creates a buffer between the public and the private realms and maintain the residential characteristic of these streets.

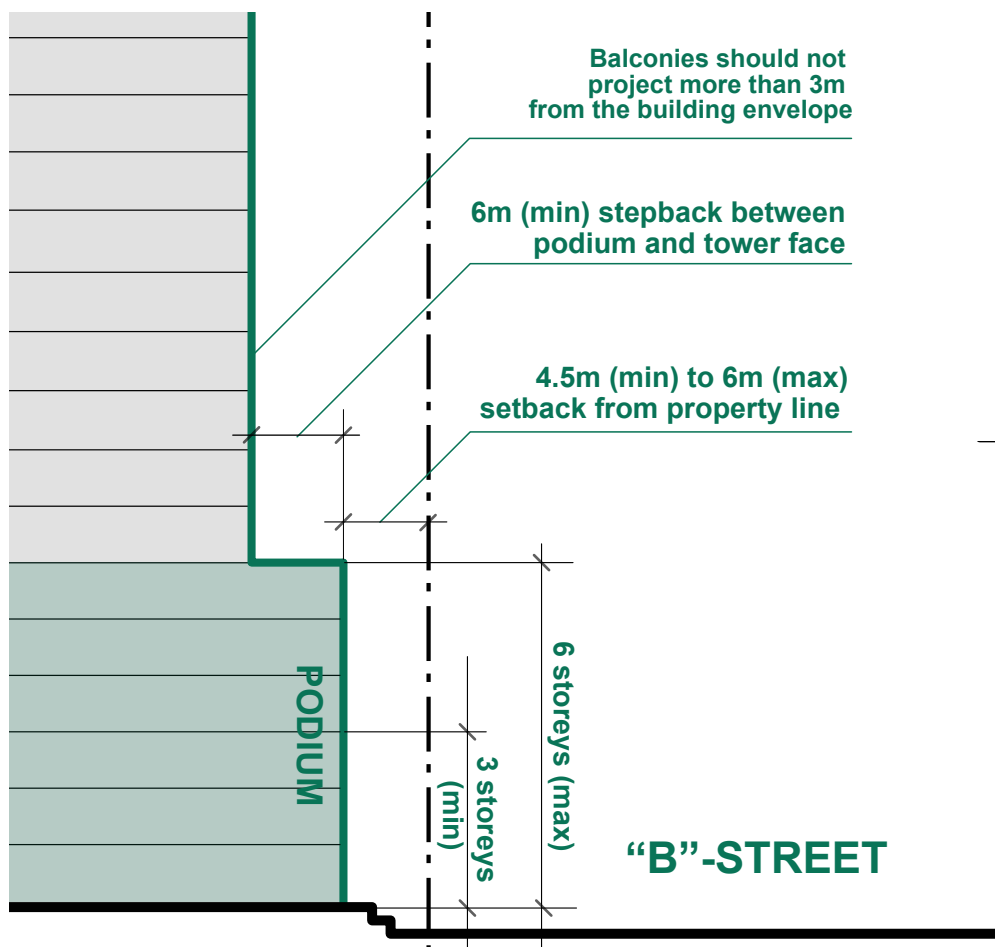
These guidelines ensure that B-streets have a unified residential character, adequate access to light and sky views, and a building massing that respects the scale of the corresponding context.

### 3.3.1 General Standards for B-Streets

The following standards apply to residential frontages on B-Streets:

**3.3.1.1** Create a high quality seamless interface and transition between the public sidewalk and front door to ensure adequate separation, definition and privacy;

**3.3.1.2** Provide individual unit entrances from the sidewalk when possible and animate the frontage with windows on the ground floor that look out onto the street or open space;



**Figure 18:** Frontage on a B Street with the required setbacks and stepbacks.

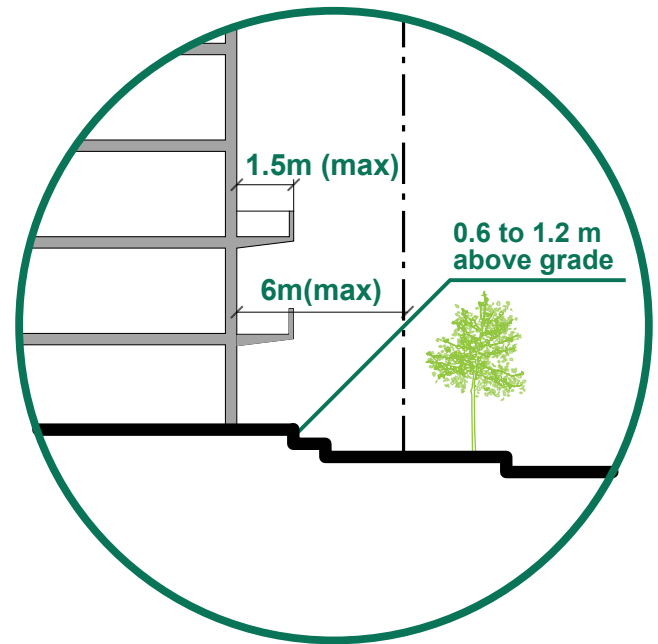
**3.3.1.3** Provide high quality landscape treatments within frontages, transition zones and setbacks;

**3.3.1.4** Incorporate a minimum setback of 3.0 metres and a maximum of 6.0 meters. This should include front steps and a porch;

**3.3.1.5** The ground floor should be raised between 0.6 – 1.2 metres above the sidewalk level;

**3.3.1.6** Residential uses should link to the public sidewalk through individual points of access serving each unit.

**3.3.1.7** Balconies on B-Streets can only project a maximum of 1.5m.

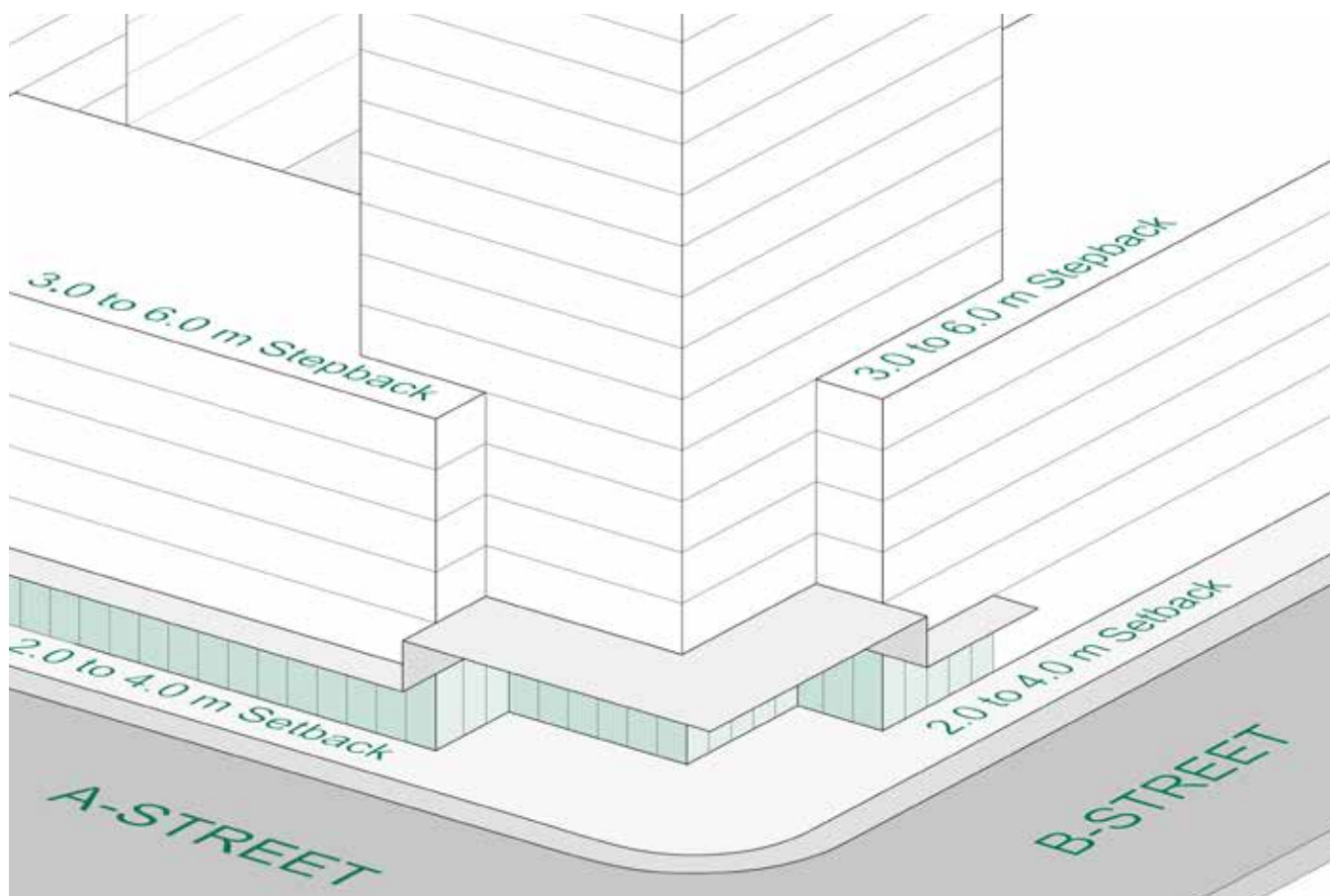


**Figure 13:** Ground floor conditions on B-Streets. Residential uses are elevated above the ground floor, but directly linked to the public sidewalk through individual points of access serving each unit.

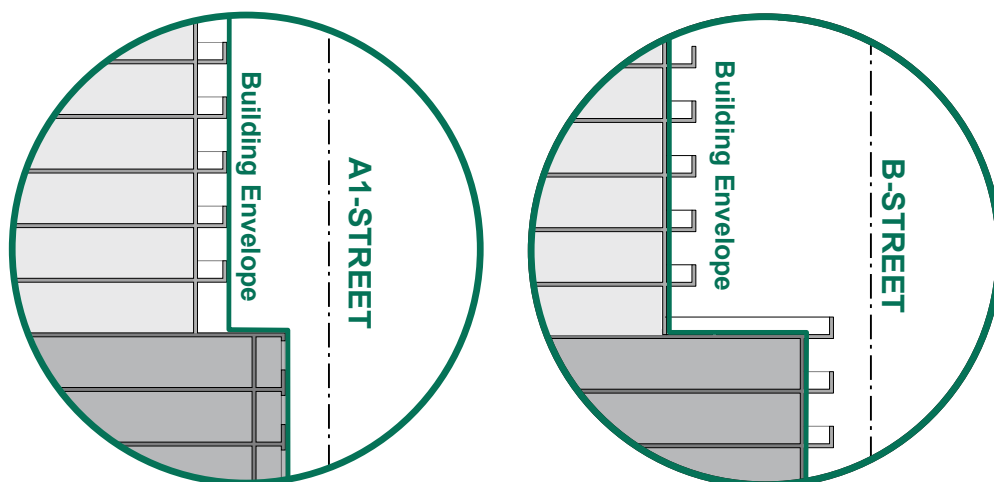


**Figure 19:** Ground floor conditions on B-Streets. Residential uses are elevated above the ground floor, but directly linked to the public sidewalk through individual points of access serving each unit.

### 3.3.2 Intersection of A and B Streets



**Figure 20:** When A and B Streets intersect, commercial uses should wrap around the corner while the tower setback should step back



**Figure 21:** Balconies are only allowed to project when a building fronts a B-Street. On A-Streets, balconies cannot project from the building envelope.

## 3.4 C-Streets and Pedestrian Connections

### 3.4.1 General Standards for C-Streets

C-Streets are tertiary streets that connect A and B Streets. In contrast to 'A' and 'B' Streets, C-Streets provide development blocks with access for deliveries, garbage pick-up, service and loading, including vehicular access to structured and off-street parking within development sites.

'C' Streets are also intended to support a pedestrian environment, by integrating a high standard of urban design to support street activity. C-Streets could also accommodate residential and retail uses in some circumstances.

**3.4.1.1** Ground floor elevations along 'C' frontages will have a minimum of 50% vision glazing with views into the building.

**3.4.1.2** Provide functioning main front entrances to buildings on 'C' Street frontages when there are no 'A or B - Street frontages;

**3.4.1.3** Where appropriate, provide functioning secondary entrance(s) for additional pedestrian access; and

**3.4.1.4** Service, parking and loading should be

consolidated to serve a number of buildings simultaneously from one area;

**3.4.1.5** Loading, garbage and service spaces will be located internal to the building to avoid noise and visual impacts;

**3.4.1.6** Prioritize pedestrian safety and the appeal of the public realm by incorporating special architectural and landscape treatments to screen servicing areas and providing a safe level of illumination.

**3.4.1.7** The height required for overhead loading for bulk refuse within a collection area should conform to the Region of Peel's standards.



**Figure 22:** Provision for vehicular access to off-street parking, access for deliveries, garbage pick-up, servicing and loading.

*Downtown Mississauga, Webb Dr & Duke of York Blvd*

### 3.4.2 General Standards for Pedestrian Connections

Mid-block and pedestrian connections can build upon the existing road network in certain locations, by providing improved permeability. Additional linkages and access between multiple development blocks and open space should be incorporated. They are intended to have strong civic quality and robust urban character that is inviting, comfortable, accessible and easy to use for pedestrians.

Mid-block pedestrian connections will:

**3.4.2.1** Be coordinated across adjacent blocks and generally located mid-block providing direct connectivity between existing and future streets, connections and open space;

**3.4.2.2** Be continuous, direct and barrier-free to be pedestrian and transit supportive.

**3.4.2.3** Have a minimum overall width of 12.0 metres; with a minimum 4 metre wide walkway zone.

**3.4.2.4** Incorporate a strong urban expression and civic presence through design and choice of materials;

**3.4.2.5** Be perpendicular to the public sidewalk, providing clear sightlines from one end to the other;

**3.4.2.6** Provide minimal grade changes to maximize accessibility;

**3.4.2.7** Provide a comfortable year-round enclosure that is open to the sky;

**3.4.2.8** Incorporate architectural and landscape treatments to mitigate any tunnel effect and is proportional to the built form;

**3.4.2.9** Provide a minimum of 50% vision glazing along the at grade condition to ensure informal surveillance along the entirety of the walkway and incorporate active uses where feasible.

### 3.4.3 Landscape and Amenities

To promote the pedestrian experience, the following landscape treatments will be provided within mid-block pedestrian connections:

**3.4.3.1** A combination of soft and hard landscape treatments to define the walkway edges and amenity areas such as trees, pavers and planters;

**3.4.3.2** Landscape species that create visual and seasonal interest;

**Figure 23:** Provide a minimum of 50% vision glazing along the at grade condition to ensure informal surveillance along the entirety of the walkway (i.e. "eyes on the walkway").

*Downtown Mississauga, a pedestrian connection between Prince of Wales Dr & Square One Dr*





**3.4.3.3** Continuous soil trenches that provide a minimum 20.0 m<sup>3</sup> per tree (note: structural soil will be discouraged);

**3.4.3.4** Provide a minimum clearance of 1.5 meters between the finished grade of the mid-block connection and the top deck of any below-grade structure;

**3.4.3.5** Sod will generally be discouraged;

**3.4.3.6** Integrate benches and/or built-in seating spaced at regular intervals;

**3.4.3.7** Provide pedestrian scale lighting, bike racks and waste and recycling receptacles;

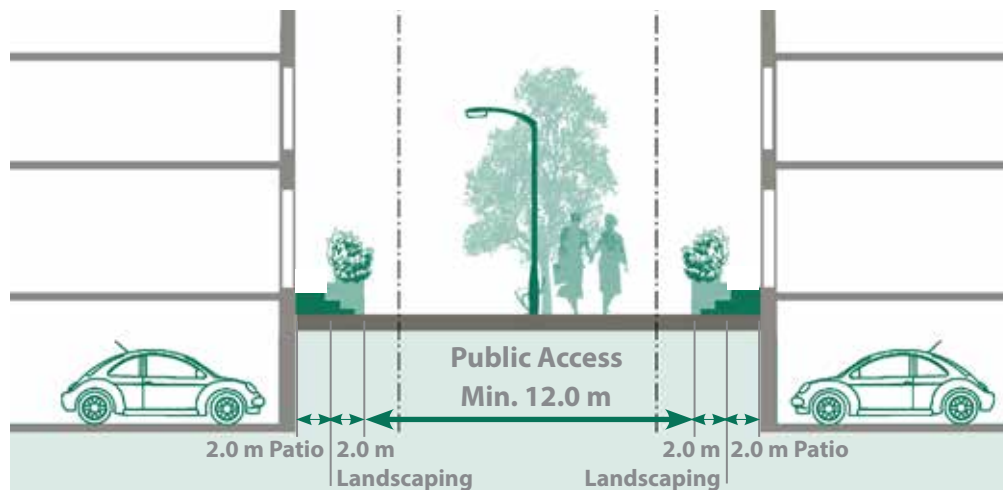
**3.4.3.8** Where desirable, patios internal to the walkway may be incorporated to support active uses, provided they do not obstruct the required (4.0 metre) walkway width; and

**3.4.3.9** High-quality paving with minimal grade transitions and materials that have a strong civic presence that also satisfy the AODA standards.

**Figure 24:** To promote the pedestrian experience, provide Landscape species that create visual and seasonal interest.  
*Downtown Mississauga, 352 Princess Royal Dr*



**Figure 25:** Section of mid-block connection.



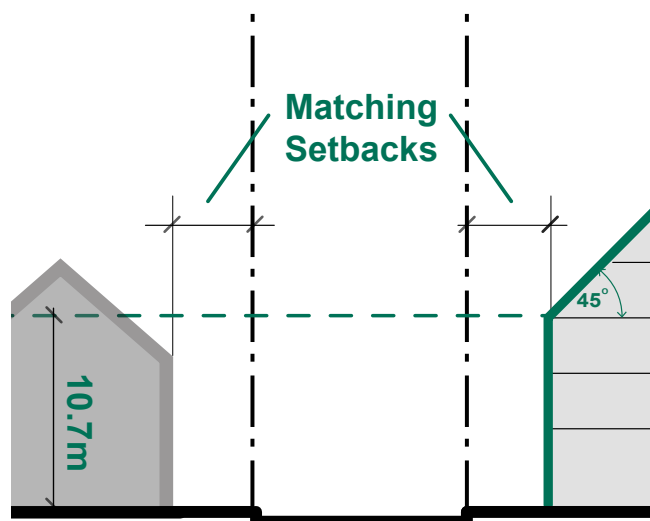


## 3.5 Transition from Established Neighbourhoods

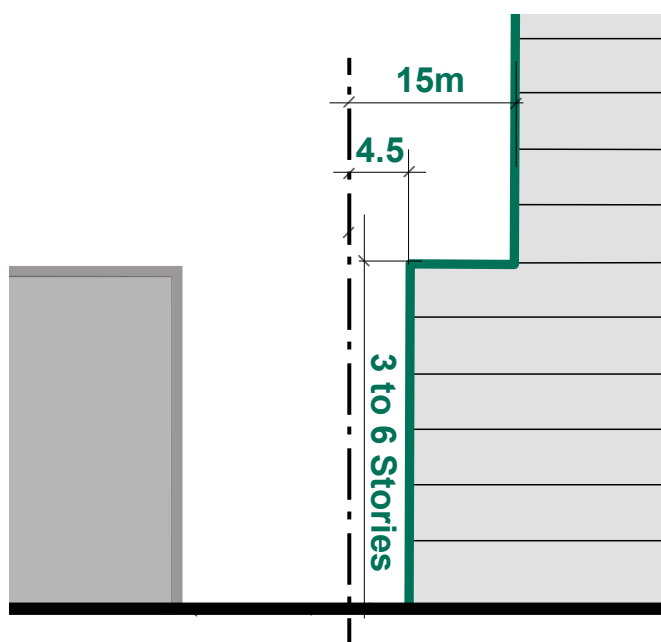
To create an appropriate built-form and to protect established neighbourhoods, parks and open spaces, the following standards provide setback and angular plane provision for new developments to create adequate transitions.

The following standards should be used wherever appropriate and could replace the other street frontage standards if deemed necessary by City staff or the applicant.

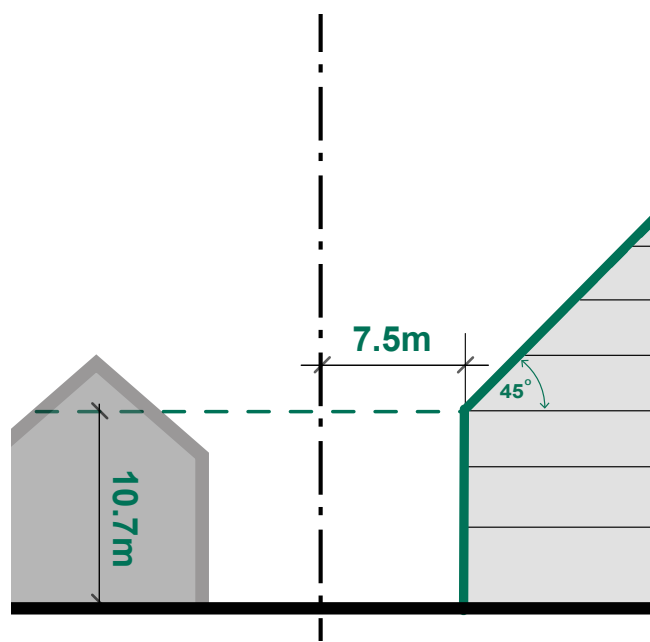
**Figure 26:** Transition from an established residential area separated by a road



**Figure 27:** Transition from the rear property line abutting an apartment building in an established neighbourhoods.



**Figure 28:** Transition from the rear property line abutting an established residential area or an open space.



# Glossary

**Articulation** - architectural detail that refines and gives a building interest and added richness.  
**At grade** - refers to the uses located at the ground storey, and generally the manner in which they are expressed or articulated to positively support the public realm.

**Build-to line** - a designated line placed within the build-to area on a development block which informs the placement and orientation of the streetwall or building.

**Built form** - buildings and structures.

**Frontage Design** - refers to the built form urban design requirements associated with a category of frontage such as 'A' or 'B' frontage.

**Glazing** - clear or transparent glass windows whose physical attribute allows light to pass through completely. In reference to at grade uses, it enables views towards the inside of a building space from the exterior at the sidewalk level.

**Patio** - an outdoor space generally used for dining that adjoins a restaurant, grocery store, coffee shop or café.

**Pedestrian scale** - a size of building, or space that a pedestrian perceives as not dominating or overpowering.

**Main Building Entrance** - the primary means of access which serves pedestrians, patrons and/or users of a building through a common entrance, internal lobby or vestibule that provides access to uses other than retail or commercial uses that are located at grade.

**Podium** - means the base of the building, structure or part thereof, located at or above established grade that project from the tower portion of the building.

**Public realm** - the streets, parks and open spaces that are available for any member of the public to access, use and enjoy.

**Scale** - the size of a building or an architectural feature in relation to its surroundings and to the size of a person.

**Sidewalk** - unobstructed pedestrian travel route generally situated within the public right-of-way.

**Streetscape** - means the character of the street, including the street right-of-way, adjacent properties between the street right-of-way and building faces. Thus, the creation of a streetscape is achieved by the development of both public and private lands and may include planting, furniture, paving, etc.

**Street frontage** - the front and/or side of a property that faces an 'A' or 'B' street.

**Step-back** - a required articulation of the building massing that helps establish the streetwall; it serves to reduce the appearance and bulk of the podium, mitigate the perception of height from the street and reduce shadow and wind impacts.

**Storefront** - the front of a store, or a room at the ground floor of a building that contains a display window at its front designed for commercial and retail uses.

*N.B. Definitions in the Zoning By-law should be consulted and will take precedence over this Glossary.*

## Appendix 9

## Appendix 9 – Applicable Policy Analysis

The proposed Downtown Fairview, Cooksville and Hospital Mississauga Official Plan Amendments (OPA) are consistent and conform with current Provincial and Regional policies, and recently adopted Regional policies as summarized below. They also reinforce the current policies and objectives of Mississauga Official Plan. The following assessment provides a summary of the intent of the policies.

### ***The Planning Act***

Section 2 of the *Planning Act* states the matters of provincial interest that authorities shall have regard for in exercising their responsibilities. Relevant matters of provincial interest include, amongst other matters: (h) the orderly development of safe and healthy communities; (i) the adequate provision and distribution of educational, health, social, cultural and recreational facilities; (j) the adequate provision of a full range of housing, including affordable housing; (k) the adequate provision of employment opportunities; (p) the appropriate location of growth and development; (q) the promotion of development that is designed to be sustainable, to support public transit and to be oriented to pedestrians; and (r) the promotion of built form that, (i) is well designed, (ii) encourages a sense of place and (ii) provides for public spaces that are of high quality, safe, accessible, attractive and vibrant. These provincial interests are further articulated through the PPS and the Growth Plan.

The proposed OPAs have regard for the above-noted provincial interests as they allow for more growth and development in a strategic growth area with new housing and employment opportunities. The OPAs propose policies to achieve walkable, transit-supportive development and promote a built form that is well-designed, encourages a sense of place and provides for public spaces.

### **Provincial Policy Statement, 2020 (PPS) and the Growth Plan for the Greater Golden Horseshoe 2020 (Growth Plan)**

The Provincial Policy Statement (PPS) and the Growth Plan for the Greater Golden Horseshoe (Growth Plan) provide policy direction on matters of provincial interest related to land use planning and development and directs the provincial government's plan for growth and development that supports economic prosperity, protects the environment and helps communities achieve a high quality of life.

The PPS recognizes that the official plan is the most important vehicle for implementation of these policies as "comprehensive, integrated and long-term planning is best achieved through official plans". Official plans shall also set out appropriate land use designations and policies.

Under the *Planning Act*, all planning decisions must be consistent with the policy statements and conform with provincial plans.

### Consistency with Provincial Policy Statement (2020)

The Provincial Policy Statement (PPS) provides direction on managing growth and creating communities that are liveable, healthy and resilient. The PPS highlights the importance of the efficient use of land and resources, a range of housing options, a mix of employment opportunities and access to recreation, parks and open spaces. The proposed Downtown Fairview, Cooksville and Hospital Official Plan Amendments (OPA) are consistent with the PPS. Please see more details below:

- **Development and Land Use Patterns:** Sections 1.1.1 and 1.1.3 require efficient land use and development patterns that promote liveable, healthy and safe communities where a mix of land uses and densities efficiently use land and resources, and support active transportation and are transit-supportive. The proposed policies support the development of walkable, transit-supportive communities in Downtown Fairview, Cooksville and Hospital with a mix of uses, an expanded street and pedestrian network, and complete street design.
- **Employment:** Section 1.3 requires an appropriate mix and range of employment opportunities. It encourages compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities. Policy 1.7.1.d seeks to maintain and enhance the vitality and viability of downtowns and mainstreets. The OPAs propose policies that support a range of employment opportunities by retaining existing office uses, and increasing the amount of non-residential uses in key locations.
- **Housing:** Section 1.1.1 and Section 1.4 require a range and mix of housing options, including affordable housing to meet projected market-based and affordable housing needs. Section 1.4 requires transit-supportive development in proximity to transit as a means to increase the amount of housing supply to meet future housing needs. The proposed policies aim to facilitate transit-supportive development within Downtown Fairview, Cooksville and Hospital and expand on the range and mix of housing options currently available, including affordable housing.
- **Public Spaces, Recreation, Parks, Trails and Open Space:** Section 1.5 provides direction for safe public streets, public spaces, parks and open space that meet the needs of pedestrians, fosters social interaction and facilitates active transportation. Policy 1.7.1.e also encourages a sense of place by promoting a well-designed built form and cultural planning. The OPAs propose policies that facilitate an expanded pedestrian, park and open space network. New policies also seek to achieve a sense of place through good urban design, and promote cultural uses in Downtown Cooksville.
- **Transportation System:** Policies 1.5.1.a and 1.6.7 promote active communities, active transportation and a multi-modal transportation system. The OPAs propose policies that

will achieve an expanded public road and pedestrian connection network through new development.

### **Conformity with A Place to Grow: Growth Plan for the Greater Golden Horseshoe (Office Consolidation, 2020)**

A Place to Grow (the Growth Plan) is the Province's growth management strategy. It highlights the importance of building complete communities, supporting economic development, and directing intensification to strategic growth areas to make efficient use of land and optimize infrastructure. The proposed Downtown Fairview, Cooksville and Hospital Official Plan Amendments (OPA) conform to the Growth Plan. Please see more details below:

- **Complete Communities and Climate Change:** Sections 2.1, 2.2 and 4.2.10 promote the concept of "complete communities." These are communities that are well designed to meet people's needs for daily living throughout an entire lifetime by providing convenient access to an appropriate mix of jobs, local stores and services, public service facilities, and a full range of housing. Policy 4.2.10 directs municipalities to support the achievement of complete communities, and reduce dependency on the automobile by promoting transit and active transportation. The OPAs propose policies that will achieve complete communities in Downtown Fairview, Cooksville and Hospital through a balanced mix of uses, range of housing options, and an expanded active transportation network.
- **Manage Growth:** Section 2.2 directs future population and employment growth to settlement areas within the Greater Golden Horseshoe. Strategic growth areas such as urban growth centres will be planned to serve as high-density employment centres and accommodate significant population and employment growth. Development within major transit station areas on priority transit corridors will be supported by a diverse mix of uses and transit-supportive densities. The proposed policies will facilitate population and employment growth to achieve a diverse mix of uses and meet provincial and regional minimum density targets within proposed protected major transit station areas in Downtown Fairview, Cooksville and Hospital.
- **Employment:** Section 2.2.5 directs retail and office uses to locations that support active transportation and have existing or planned transit. With existing and planned rapid transit in Downtown Fairview, Cooksville and Hospital, the proposed policies aim to retain existing office uses within Downtown Fairview, Cooksville and Hospital. New policies are also proposed directing at-grade retail along corridors and additional retail and office uses in key locations.
- **Housing:** Section 2.2.6 provides direction to support a range and mix of housing options, including affordable housing, to meet the needs of current and future residents. The OPAs propose policies that will provide for a range of housing options including affordable housing to help meet projected needs of current and future residents.



- **Transportation System:** Sections 3.2.2 and 3.2.3 directs municipalities to integrate land use planning and transportation planning, facilitate improved linkages to urban growth centres, major transit station areas, and other strategic growth areas, prioritize active transportation and transit, and adopt a complete streets approach. The proposed policies aim to expand the existing road and pedestrian network, improve multi-modal access to existing and planned transit, and promote complete streets.
- **Public Open Space:** Policy 4.2.5.1 encourages municipalities to develop a system of publicly-accessible parkland, open space, and trails. The proposed policies provide direction to expand the public park and open space network within the Downtown Fairview, Cooksville and Hospital Character Areas.

## Peel Regional Official Plan (Office Consolidation, 2021)

The Regional Official Plan (ROP) provides direction and a strategic policy framework to guide development and growth in Peel Region and Mississauga. The over-arching theme of the ROP is sustainability – supporting the needs of present populations without compromising future generations. The sustainability framework encompasses environmental, social, economic and cultural pillars in order to ensure that the Region develops a holistic approach to planning and creates the conditions for thriving communities. The proposed Downtown Fairview, Cooksville and Hospital Official Plan Amendments (OPA) conform with the ROP. Please see more details below:

- **Complete Communities:** Section 5.3 provides direction to establish complete urban communities that contain living, working and recreational opportunities, and which respect the natural environment, resources and characteristics of existing communities. The OPAs propose policies that will achieve complete communities with a mix of housing, employment, retail, commercial, service, community and recreational uses in Downtown Fairview, Cooksville and Hospital.
- **Employment:** Section 5.2 requires local municipalities to address the provision of opportunities for residents to live and work within the urban growth centre with a wide range and mix of land uses. Policy 5.6.2.10 encourages high density employment uses in proximity to major transit station areas. The proposed policies seek to maintain existing office uses and provide additional non-residential uses in key locations.
- **Housing:** Section 5.8 provides direction for municipalities to plan for a range and mix of housing. Specifically, policy 5.8.2.3 encourages and supports municipalities to plan for a range of housing options and forms, including affordable housing to enable all residents to remain within their communities. The proposed policies reinforce these housing policy directions.
- **Active Transportation:** Policy 5.9.10.2.1 provides direction for integrated transportation planning with pedestrian and cycling networks that are safe, attractive and accessible, and provide linkages between areas and to adjacent neighbourhoods and transit

stations. The OPAs propose policies to expand the road and pedestrian network in Downtown Fairview, Cooksville and Hospital.

### **New Region of Peel Official Plan (Adopted, 2022)**

On April 28, 2022, Regional Council passed By-law 20-2022 to adopt the new Region of Peel Official Plan (RPOP), which is currently awaiting Provincial approval. While the policies of the new RPOP are not in full force and effect, it represents Regional Council's most current vision to guide growth and development within Peel Region and the City of Mississauga to the year 2051.

Once approved, the new RPOP will have delineated major transit station areas across Peel Region and minimum densities requirements for each major transit station area. Local municipalities will be directed to address: land uses that support complete communities; minimum densities; minimum and maximum heights where appropriate; and improved multi-modal access and connectivity, among other matters.

The City of Mississauga is proposing an amendment to its Official Plan (draft Amendment No. 144) that will introduce protected major transit station areas to conform with the new RPOP. Downtown Fairview, Cooksville and Hospital Official Plan Amendment (OPA) 146 proposes additional protected major transit station area policies that support mixed use transit-supportive development. OPA 146 will come into force and effect once Amendment No. 144 is in force and effect.

### **Mississauga Official Plan (Consolidation, 2021)**

The proposed Downtown Fairview, Cooksville and Hospital Official Plan Amendments (OPA) reinforce the current policies and objectives of Mississauga Official Plan (MOP). Please see more details below:

- **City Structure and Growth:** Chapter 5, Section 5.3 identifies an urban hierarchy and recognizes the different functions of various areas of the city. The Downtown will contain the highest densities, tallest buildings and greatest mix of uses. The OPAs propose a vision and mix of uses that reflect the position of Downtown Fairview, Cooksville and Hospital within the City's Urban Hierarchy.
- **Employment:** Section 5.3.1 identifies the Downtown as the primary location for mixed use development. Policies 5.5.10 and 10.4.1 encourage major office and retail development to be located within major transit station areas. The proposed policies aim to retain existing office uses and increase the amount of non-residential uses in key locations to achieve the vision of a vibrant Downtown where residents can live, work and play.
- **Parks and Open Spaces:** Chapter 6, Section 6.3 provides city-wide direction for Mississauga's green system that is comprised of the natural heritage system, urban forest, natural hazard lands and parks and open spaces. Section 6.3 introduces the one-

zone concept and two-zone floodplain management concept. The proposed policies provide further direction for development within the floodplain and the expansion of parks and open space to serve future growth.

- **Complete Communities:** Chapter 7 provides city-wide direction for complete communities. The Chapter identifies the need to plan for complete communities that meet the daily needs of people and offer a range of services, employment and residential opportunities. Specifically, Policy 7.1.3 encourages compact built environments that integrate a mix of uses, support alternative modes of transportation, and encourage recreational activities. Section 7.3 identifies the need for community infrastructure as a vital part of complete communities and quality of life. The OPAs propose policies that support the development of complete communities with a balanced mix of uses, including community infrastructure, and different modes of transportation.
- **Housing:** Section 7.2 provides city-wide direction to ensure suitable housing for people of all stages of life. MOP encourages the creation of new housing to meet the needs of diverse populations, younger and older adults and families. The OPAs propose policies to increase the amount of housing that is available in Downtown Fairview, Cooksville and Hospital.
- **Multi-Modal Transportation:** Chapter 8 aims to create sustainable communities with multi-modal transportation networks, encourages a shift towards more sustainable modes of transportation and prioritizes the creation of a fine-grained road pattern in Intensification Areas. The proposed policies will support a fine-grained multi-modal transportation network by introducing new public roads and pedestrian connections and promoting complete streets in Downtown Fairview, Cooksville and Hospital.
- **Urban Form:** Chapter 9 provides city-wide direction to build a desirable, sustainable built form with high quality urban design and public realm that contributes to a strong sense of place. Specifically, Sections 9.2.1 and 9.5 requires development in Intensification Areas to contribute to a vibrant, pedestrian realm, support pedestrian and cycling connectivity, incorporate appropriate podium conditions and ensure appropriate transitions in height with surrounding areas. The proposed policies in Downtown Fairview, Cooksville and Hospital provide direction to activate street frontages along key streets, introduce new pedestrian connections, contribute to a vibrant pedestrian realm, and ensure appropriate podium conditions and height/built form transitions.