

3115 Hurontario Street

Urban Design Study



IBI GROUP

Prepared for Clearbrook Developments Inc.
by IBI Group
October 4, 2022

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1 Introduction

This Urban Design Study has been prepared by IBI Group Professional Services (Canada) Inc. (IBI Group) on behalf of Clearbrook Developments Inc., the owners of the property municipally known as 3115 Hurontario Street, in the Cooksville community, Ward 7, City of Mississauga (henceforth referred to as the 'subject site' or 'site'). The proposed development consists of a primarily residential building with commercial uses at-grade, and non-profit community uses occupying the first and second floors.

An Urban Design Study for the subject site was requested by City Staff at the Development Application Review Committee (DARC) meeting held on March 30th, 2022. Responses and feedback received from City Staff were considered, and where possible, incorporated into the design evolution of the proposed development. Through permission of City Staff, this application is anticipated to be presented before the Mississauga Urban Design Review Panel on October 18th, 2022, following this first submission. The latest concept illustrated in this Study represents significant efforts to address the City's planning and urban design principles, objectives, and visions as outlined in the City of Mississauga Official Plan, Zoning By-law 0225-2007, Urban Design Guidelines and Standards, as well as considers policies proposed under Official Plan Amendments (OPA) 145 and 146 which have been presented to and approved by Council and are awaiting adoption. This Report should be read in conjunction with the Planning Justification Report prepared by IBI Group.

The Urban Design Study is based on the City of Mississauga's Terms of Reference for Urban Design Studies (2019) and contains a contextual analysis of the site and surrounding context to help demonstrate the proposed development's compatibility with existing and future conditions. The Urban Design Study also expresses how the City's planning and urban design principles and objectives are being addressed. In addition, there is discussion on how the proposed development achieves the City's objectives for managing growth through intensification in key locations. Matters of built form and architectural character are also discussed, including how these factors play a role in complementing the surrounding context and interfacing appropriately with the public realm.

The Urban Design Study is submitted in support of development applications for an Official Plan Amendment and Zoning By-law Amendment. The purpose of these applications is to introduce a 35-storey residential tower with a six-storey podium. The podium feature proposes to be in line with the adjacent developments to ensure a continuous and seamless corridor along Hurontario Street. The podium at-grade will include a residential entrance, a retail component, and the first floor of the large charity component. The Charity is the main purpose of this redevelopment as it will allow the Dam Youth Charity to continue their outreach in the community and allow more opportunities to expand the reach of this work, resources, and programs.

1.1 Goals and Objectives

1.1.1 Vision

The City of Mississauga has developed a series of Guiding Principles and Strategic Actions to assist the City in achieving their Vision for Mississauga, which includes a city that is strong and diverse in its economy, mobility options, housing, and community infrastructure to create distinct and complete communities. Achieving this Vision requires three major categories of focus:

Revitalizing Infrastructure	Conserving the Environment	Promoting Community Participation and Collaboration
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The proposed development of 3115 Hurontario Street has responded to Mississauga's Vision for the city, including for the Cooksville Area and the Hurontario Corridor, as well as its surrounding block context. The subject site is located within an *Intensification Corridor* and is within 500 metres of a Major Transit Station Area (MTSA) associated with the Cooksville GO Station. The future higher order transit along both Hurontario Street and Dundas Street will change the face of the two corridors and influence their transition into multi-modal, vibrant, sustainable, and mixed development epicentres. Growth and intensification will be directed towards these corridors and will be developed to be attractive, mixed-use communities with higher densities that can support frequent transit service. Focusing intensification along these corridors will support the preservation of existing residential communities while introducing new housing to meet the needs of residents.

A key element of the proposed development and redevelopment of the subject site is to ensure the DAM Youth Charity, which currently occupies the site, is preserved and enhanced to ensure it remains in the community in perpetuity. The redevelopment of the site and introduction of residential uses will support the continued provision of key services to vulnerable youth within the surrounding area, and broader city.

The proposed development will be a landmark in the Downtown Cooksville area and a marker for the evolution of built form typology and architectural character along the Hurontario Corridor.

The proposed development aligns to the City of Mississauga's Strategic Plan and aims to create a development that is dynamic and sustainable in all ways. The proposed development is not only located within proximity to the major transit hub in the area, it also serves as a hub for youth to gather that creates a sense of community and a connection to Mississauga. The mixed-use community focused nature of the development helps to embody the complete neighbourhood ideology.

1.1.2 Goals and Objectives

The proposed development of 3115 Hurontario Street is located along the future Hurontario LRT and directly between two major east-west corridors, in the center of the *Cooksville Character Area* as identified in the City of Mississauga's Official Plan, Schedule 9. This location along the corridor and frontage to the under construction Hurontario LRT line (completion date 2024) has been a guiding factor in the design progression. Responding to and capitalizing on the future Hurontario LRT is a significant goal of the development, while responding to the evolution of the area. Promoting development that will increase ridership of the LRT and positively impact modal split, will contribute towards the changing face of the Hurontario Corridor. In addition, proposing higher density development will position more people closer to transit and active transportation infrastructure, which will increase the desirability and livability of the area. Encouraging transit-oriented development aids in mitigating climate change and achieving the City's Climate Change Action Plan goal to reduce greenhouse gas emissions 80% by 2050, working towards a net zero community.

The proposed development also will create a community gathering place, with the inclusion and enhancement of the DAM Youth Charity. This integral portion of the development will create a safe place for youth in the community and broader city, so that they are able to feel more comfortable and included, which will help keep youth connected and immersed in the City of Mississauga, helping to facilitate a long term connection with the City and the community.

Responding to existing and future conditions of the corridor, the proposed development works towards bringing a contemporary architectural language along Hurontario Street. The Hurontario Corridor will not just transition in terms of a modal aspect, but in architectural and typological aspects as well. In addition, ensuring that the proposed development properly addresses the public realm through scale, appropriate setbacks, materiality and frontages, particularly along Hurontario Street, is a crucial goal. Lastly, the design acknowledges its physical presence near a corner of an arterial intersection and helps ensure the appropriate development of the block and an enhanced public realm.

Objectives

1. Celebrate Hurontario Street as a Vibrant Corridor

Hurontario Street is one of Mississauga's largest corridors, with the addition of the Hurontario LRT line further expanding its role within the City. By adding a mixed-use podium at-grade, this development further creates a pedestrian destination along Hurontario Street, in particular for youth in the community. The architectural design features create a unique podium design that will aid in attracting pedestrian traffic. The proposed development truly embraces Hurontario as a pedestrian corridor, as demonstrated through the podium design and enhancing the public realm at-grade.

2. Respond to Existing and Planned Context

The proposed development is in keeping with the direction of the Provincial Policy Statement, the Growth Plan for the Greater Golden Horseshoe, the Region of Peel Official Plan, the updates to the Region of Peel Official Plan, the City of Mississauga Official Plan, Hurontario / Main Street Corridor Master Plan, Cooksville Mobility Hub Master Plan, Dundas Connects Master Plan, and the Mississauga Urban Design Guidelines by introducing new housing along a key transit corridor, contributing to creating a complete community through the enhancement of charity and retail uses, and fostering the flexibility of uses while maintaining and enhancing a key community component in proximity to transit.

Located within 400 metres from the Dundas Street stop and within 200 metres of the Cooksville GO Station stop for the Hurontario Street LRT system, the proposed development provides much needed housing, provides a community hub with the expansion of the DAM Youth Charity, and provides opportunity for new retail services in a mixed use podium. The proposed development largely responds to existing and planned context as it is the direction of the Provincial Policy Statement and Growth Plan to optimize investments in and around transit supported and planned areas. The proposed development will contribute to the future vision of Mississauga and the Downtown Cooksville Area as the proposed density of the development, as well as the mix of uses, will contribute to the growth of the Hurontario Street corridor within the City.

1.2 Analysis of the Existing Site and Neighbourhood

Located just south of the Hurontario Street and Kirwin Avenue intersection, on the east side of Hurontario Street, the proposed development is mixed-use in nature with a large non-profit community component. The following Section outlines the existing context of the subject site and its surrounding area.

1.2.1 The Subject Site

The subject site is legally described as PT LT 15 CON 1 NDS TORONTO AS IN RO1112474, EXCEPT PT 14, 43R21969; CITY OF MISSISSAUGA, Regional Municipality of Peel, municipally known as 3115 Hurontario Street, City of Mississauga.

The subject site is located south of the intersection of Hurontario Street and Kirwin Avenue. The parcel is rectangular in shape, approximately 56.4 m deep with 44.6 m frontage on Hurontario Street, with a total area of approximately 0.25 ha. The subject site itself consists of a single detached dwelling currently being used by the DAM Youth Charity. The DAM Youth Charity is a youth-based community charity with a focus on helping young people find a place of belonging. The DAM Youth Charity has provided a safe space for Mississauga's youth for the last 25 years by providing youth programming and development opportunities. The remainder of the site is currently used for parking and landscaping.

The proposed development for the site introduces a 35-storey development with a mixed-use podium that includes a significant amount of space intended for the charity.

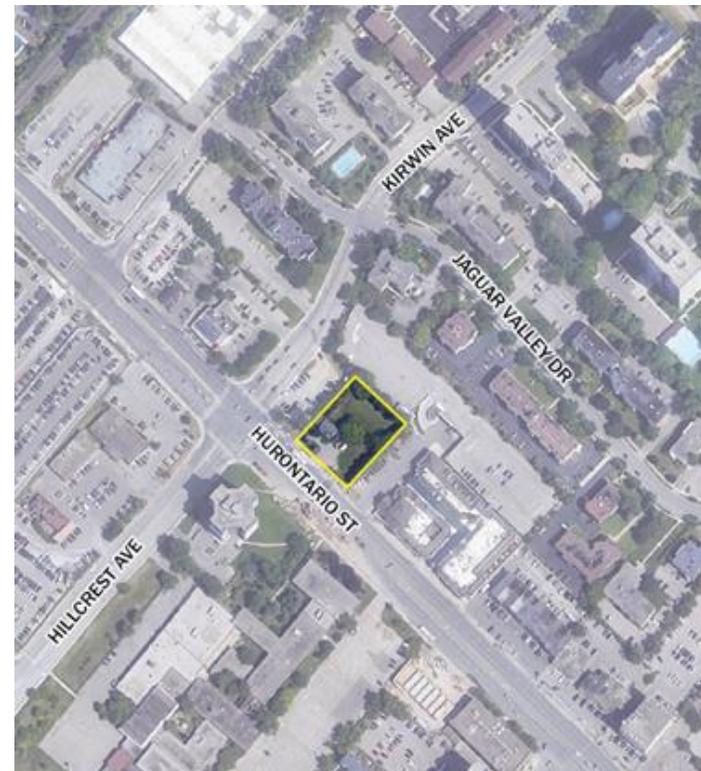


Figure 1: 3115 Hurontario Street 'Subject Site'

1.2.2 Surrounding Context

The subject site is located along Hurontario Street, a major north-south road within the City of Mississauga. This corridor is currently being redeveloped to incorporate a Light Rail Transit (LRT) system between Port Credit Go Station in southern Mississauga and City of Brampton Gateway Terminal. A stop is currently being proposed at John Street which is located approximately 180 metres north of the subject site towards the Cooksville GO Station. The Cooksville Go Station is part of the Milton GO Line which travels between Union Station in Downtown Toronto and the Town of Milton. This GO Station is the centre of Downtown Cooksville, the combination of the Hurontario LRT and the Cooksville GO Station is a large reason for the intensification being proposed and expected within Downtown Cooksville. Currently, the subject site is surrounded by a mix of residential, commercial, and retail uses, and is expected to increase in density into a mixed use community. The following lists the direct surrounding uses.

The subject site is situated in an area comprised of predominantly residential and commercial uses.

North: To the north is the Red Door Dental Clinic and an associated surface parking lot with access from Kirwin Avenue located at 3121 Hurontario Street. These lands are designated as *Mixed Use* in the City of Mississauga Official Plan. Further north, past Kirwin Avenue, is a small commercial plaza with a Scotiabank and Softron Tax, which also features surface parking. Further North is the C.N. Rail Line with the Cooksville GO Station located just north of John Street.

East: To the east of the site is a parking garage proposed for redevelopment. Further east is a 4-storey apartment building that fronts onto Jaguar Valley Drive and additional apartment buildings that range from 5- to 29-storeys. These lands are designated as *Residential High Density*.

South: South of the site is another commercial plaza with various restaurants, salons, a grocery store, a repair store, and surface parking with access from Hurontario Street. The lands are designated as *Mixed Use* and are also part of the proposed development to the east. Further south of the property and continuing along Hurontario Street are continued retail and commercial uses designated *Mixed Use* along the Hurontario Corridor.

West: Abutting the site to the west is Hurontario Street, followed by a 12-storey apartment building and TL Kennedy Secondary School. Northwest of the site is a commercial plaza that features liquor and beer stores and a restaurant. Cooksville GO Station is also located northwest along Hurontario Street, which provides easy access to various locations in the Greater Toronto Area.



Figure 2: Hurontario LRT Route

1.2.1 Surrounding Built Form

Hurontario Street is transitioning to become a major mobility hub in Mississauga, with transit modes shifting from being single-occupant vehicular focused to becoming more transit-oriented and walkable. As such, the urban form of the neighbourhood is changing. Currently, Hurontario Street consists of more single use commercial and residential developments on different parcels versus combined into mixed use types of buildings and land uses, with several high-rise residential towers, and several low-rise commercial facilities, all with at-grade parking. Newer applications and development being constructed in the area aim to make the urban realm more pedestrian friendly by utilizing the mixed-use designation and relocating parking to below grade, so that landscape features can be present, creating a more walkable community. The new proposed built forms along Hurontario Street are responsive to the City's new intensification guidelines and Downtown Character Area plans that are responsible for the growth and direction in the area. The surrounding developments aim to increase the pedestrian realm to encourage active transportation and transit-oriented movement.

Transitioning Corridors

Hurontario Street is undergoing significant change, as evident by the numerous development applications being proposed along this transit corridor. The current state and condition of the neighbourhood is not likely to reflect the future of Hurontario Street. However, it is important to develop the site in a way that responds to future vision, while remaining respectful to the current condition of Hurontario Street.

Currently there are residential apartment towers and strip mall commercial services immediately adjacent to the site. The architectural features on the residential towers utilize a neutral palette, with shades of beige, brown, grey, and white. These towers also feature balconies for many of the units and utilize different proportions of glazing along the façade. The commercial structures are low rise and feature branded stand alone features (such as the Scotiabank and Beer Store located in the northwest corner of Hillcrest Avenue and Hurontario Street) or large strip plazas with several smaller retailer utilizing a similar façade and aesthetic.

Surrounding Open Spaces and Natural Features

The proposed development provides landscape elements in keeping with the Hurontario LRT plan and is providing shrubbery and trees along the perimeter of the site. The proposed development also proposes at grade and above grade landscaped amenity areas for both the residents of the residential tower and the users of the DAM Youth Charity.

Surrounding public open spaces include Sgt. David Yakichuk Park and TL Kennedy Soccer and Football, located approximately 450 m to the west, and John C. Price Park approximately 250m to the east. The Cooksville Creek Trail system runs along Cooksville Creek with connections to a community wide system of parks along the watercourse that offer a range of programming and activities. Heading north on the trail, there is a continuous off-road link to the Mississauga Valley Community Centre and the Mississauga Valley Trail.

The under construction Hurontario Street LRT plan also includes landscape design features that will add a sense of cohesion across Hurontario Street. The plan features widened sidewalks near stations that increase the walkability and pedestrian realm. The paved open space is intended to enhance the pedestrian realm, and as such is lined with raised planter boxes and deciduous trees. These natural elements enhance the public realm by helping to reduce the heat island effect by creating shade and shelter from the elements.

1.2.2 Transportation Networks

The subject site is at the intersection of several transportation networks, such as the Hurontario LRT system, the Cooksville GO station, the future Dundas Street BRT, several of the City's established cycling networks, and the pedestrian corridor along Hurontario Street. The access to many multimodal transportation systems makes Hurontario Street an equitable choice for intensification and development.

Metrolinx has significantly invested in transit infrastructure to service the people of Mississauga, 'The Big Move', is a Regional plan for a comprehensive transportation network. Metrolinx has identified three transit lines that will intersect in the Cooksville area. The transit lines include the Milton GO Transit rail line located near Hurontario Street and John Street, the new Hurontario LRT system with a stop at Hurontario Street and Dundas Street as well as Hurontario Street and John Street, and the proposed future BRT line along Dundas Street.

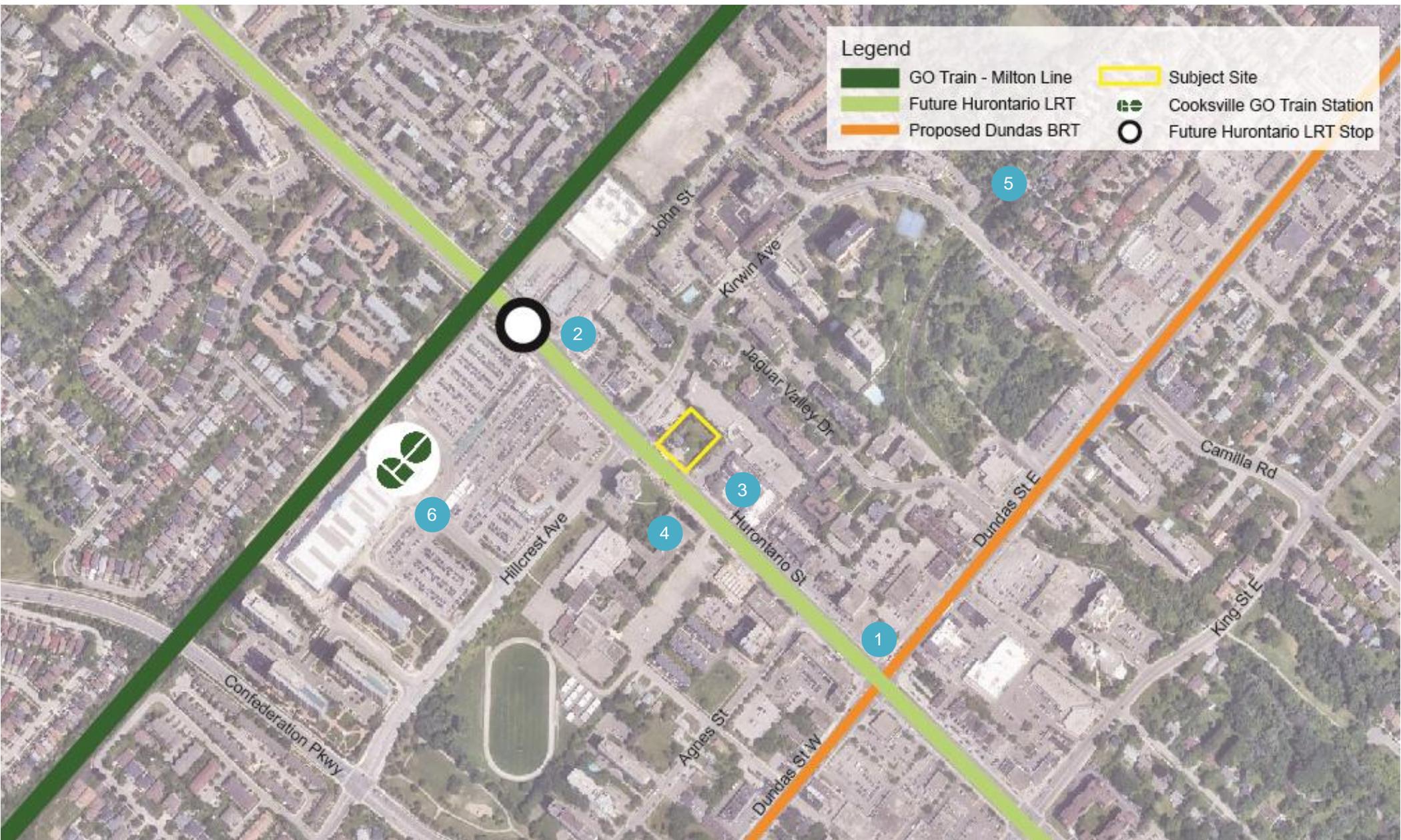
The site is also within the immediate proximity to some of the City's vast cycling network, with Hurontario Street also containing a *Primary On-Road* cycling route which connects with the rest of the City's cycling network. Dundas Street to the west of Hurontario Street also contains a *Primary On-Road* cycling network to connect to the west end of the City. The site is in direct proximity to some of the City's most established cycling networks, many modes of public transportation, and is along the planned Hurontario Street intensification pedestrian corridor which is also planned to have a cycling network.

Hurontario Light Rail Transit (LRT)

Hurontario Street is under development to become an Intensification Corridor, a Higher Order Transit Corridor with a Light Rail Transit Station planned at Hurontario Street and Dundas Street as well as Hurontario Street and John Street (Cooksville GO Station). The station at Hurontario Street and John Street is less than 200 metres from the proposed development, and the Dundas Street stop is less than 400 metres from the proposed development. The plethora of transit opportunities provides much needed transit options for the residents in this area.

The subject site is located within the *Mobility Hub – Gateway* and is located south of the GO Rail Express Rail and northeast of Other Rapid Transit Corridors. The closest GO Station is Cooksville GO which is part of the Milton GO Line and provides connections to Union Station. The Route 21H GO Bus is also available to Square One and Union Station.

The subject site is within close proximity to the under-construction LRT line along Hurontario Street, which will provide better transit infrastructure throughout Mississauga. The LRT line will connect residents north-south along Hurontario Street with access to other local, regional, and provincial transit routes within the GTA. The site is also in close proximity to the proposed Dundas Street BRT line which will run east-west along Dundas Street, providing further connection throughout the whole of Mississauga.





Cooksville Sign at Hurontario Street and Dundas Street



Hurontario LRT John Street / Cookville GO Stop



Proposed Development at 3085 Hurontario Street



T.L. Kennedy High School Across from the Subject



Richard Jones Park Entrance Near Kirwin Avenue



Cooksville GO Station Parking and Entrance

2 Analysis of the Proposed Development

2.1 Site Design

The proposed development will comprise a mix of non-residential uses at grade and the second floor, followed by amenity space and residential units above. The overall layout and distribution of uses have been designed to maximize the use of land and to support the overall objective of a compact and transit-supportive development. The site design proposes to redevelop the property with a 35-storey tower and a 6-storey podium with non-residential uses at grade and on the second floor.

The proposed development will be accessed off Kirwin Avenue in conjunction with the neighbouring application (3085 Hurontario Street) to limit the egress onto and off of Hurontario Street. The podium portion of the building is setback 2 metres from the property line and 5.3 metres from the sidewalk, allowing a landscape buffer between the building and the sidewalk which helps form a more pedestrian friendly streetscape and sense of scale.



Figure 3: Site Design in Context with Existing Land Uses

2.1.1 Relationship Between Built-Form and Right of Way

The proposed development optimally utilizes and capitalizes on the public realm and surrounding context. The property is proposed to share a vehicular access road off Kirwin Avenue with the neighbouring development. This conscious decision and partnership will allow for the vehicular entry to both sites to be consolidated to the back portion of the properties to limit vehicular access onto Hurontario Street and encourage a more pedestrian friendly environment, as well will reduce the in and out vehicular transportation onto and off of Hurontario Street.

The frontage along Hurontario Street includes a widening of the pedestrian realm and is met with a large ground floor retail façade which promotes pedestrian friendly design and good sense of scale. This orientation responds to the road condition of Hurontario Street and allows more room for pedestrians to enjoy the public realm without being in direct proximity to the busy Hurontario Street.

2.1.2 Streetscape Design and Conditions

The proposed development design is intended to provide a range of high-quality and comfortable outdoor amenity spaces for a variety of users. The DAM Youth Charity will have two private outdoor amenity spaces that are accessible to the users of the organization.

The public realm and streetscapes of the site will reflect high quality pedestrian environments, with coordinated landscape features, built form, infrastructure, and utilities. Intended to establish an alluring, inclusive, and active urban character, the streetscape design provides and encourages pedestrian activity and active transportation use through connected sidewalks, walkways, and access points. As part of the Hurontario LRT process, the Hurontario Street right-of-way streetscape is currently being designed by Metrolinx. The following outlines the proposed streetscape design features within the property line along Hurontario Street, Kirwin Avenue, and the private shared street.

Hurontario Frontage

Hurontario Street is a major north-south transportation corridor for Mississauga, designated as an *Arterial Street* and a *Major Transit Area* connector which is why the transformation of the area is expected and planned. The Hurontario frontage of the site features a widened paved outdoor amenity space directly along Hurontario Street, which will allow for better pedestrian flow. This space will feature natural elements and a paved ground plane and will serve as a connector between the public and semi-public realm. In order to maintain this pedestrian flow, through this development application, the subject site will no longer be vehicularly accessed by Hurontario Street, but rather in partnership with the neighbouring development, the access will be from Kirwin Avenue in the rear of the proposed development.

Hurontario is undergoing a massive change as a response to the new LRT Line and will soon be further defined by new architectural forms and uses, including high density residential, commercial, office, and public open spaces. The proposed setbacks along Hurontario Street allow for tree planting in soil cells, raised planting beds, incorporating some shrub/perennial planting opportunities and by continuous soil volumes within the paved zone, to keep in line with City of Mississauga's Urban Design Guidelines.

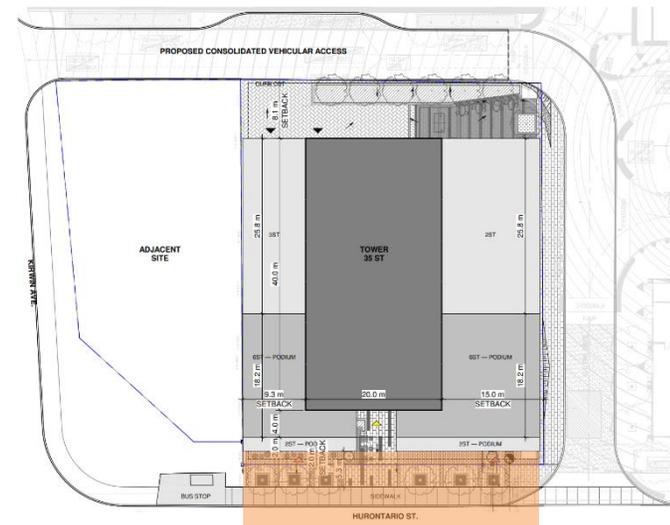


Figure 4: Site Plan - Hurontario Street

Kirwin Avenue

Designated in the Mississauga Official Plan as a *Major Collector Road*, Kirwin Avenue is designed to accommodate moderate volumes of traffic and will be a focus for active transportation facilities. As such, the design of Kirwin Avenue will respond to the anticipated use and level of pedestrian activity, with the streetscape utilizing more landscape features such as a green buffer and trees so that it responds to the low rise residential. The Kirwin Avenue frontage will draw on the elements present in the neighbouring residential buildings to create visual cues that the street is more of a pedestrian thoroughfare.

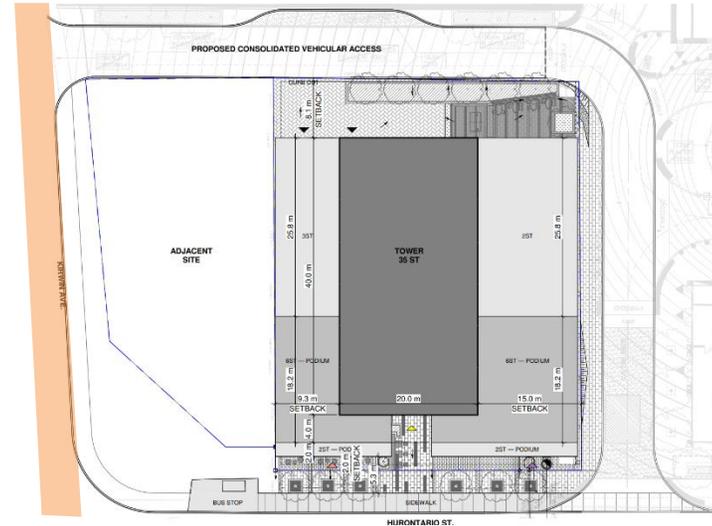


Figure 5: Site Plan - Kirwin Avenue

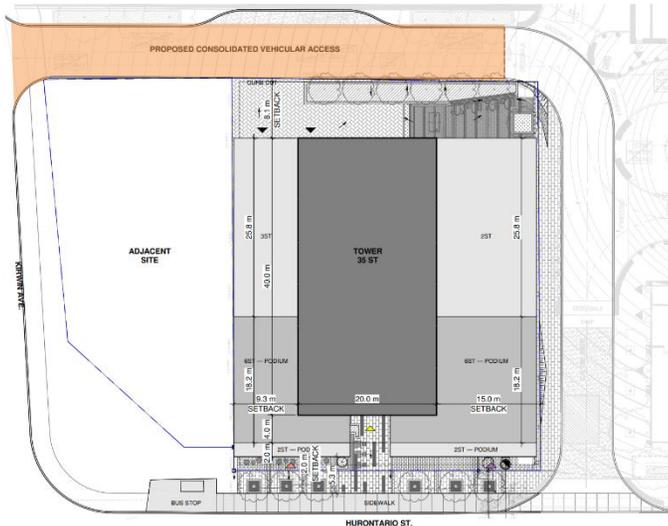


Figure 6: Site Plan - Private Shared Street

Private Shared Street

In the rear of the subject site, a private shared street is proposed to provide the vehicular and secondary pedestrian circulation to the site and the neighbouring property, to help with vehicular and pedestrian flow. The 7.6m roadway will contain planting beds, sidewalk and a buffer while providing a visual link from Kirwin Avenue that shows that the road is for slower vehicular movement. The shared street will allow for increased safety along the other pedestrian routes by mitigating the vehicular access to the site. This private shared road will provide natural and landscape elements though that will serve as a divider as well as a visual cue that the space is the transition from pedestrian only to a space with vehicular traffic.

2.1.3 Open Space and Landscape

The Landscape Plan for the proposed development at 3115 Hurontario Street was designed to achieve the following objectives:

- Complement the internal program of spaces within, and the architectural character and form of, the proposed building;
- Provide amenities to enhance the quality of life of future residents;
- Achieve an appropriate transition between the urban streetscape along Hurontario Street and the suburban character of Kirwin Avenue;
- Emphasize the pedestrian realm while accommodating practical vehicular circulation;
- Afford flexibility to address the multi-faceted nature of the proposed 'commercial and charity space along Hurontario Street;
- Enhance user comfort through the mitigation of adverse wind effects and capitalization on microclimatic conditions;
- Integrate with existing streetscape condition along Hurontario Street while accommodating the modifications that will be implemented in conjunction with the future Metrolinx LRT project; and,
- Afford accessibility for users of all ages and abilities.

The landscape for the proposed development comprises the following components:

1. A private courtyard surrounded by trees at grade to be occupied by the DAM Youth Charity;
2. Herringbone vehicular paving in the rear of the building;
3. Landscape area separating the consolidated vehicular access and DAM Charity Courtyard;
4. Front entrance landscape bed with seatwall, cubical seating, and decorative paving;
5. Six tree grate planting beds with seating, spanning the frontage of the site, in line with Hurontario Corridor plans; and
6. Area dedicated to bistro seating in front of the Retail area.

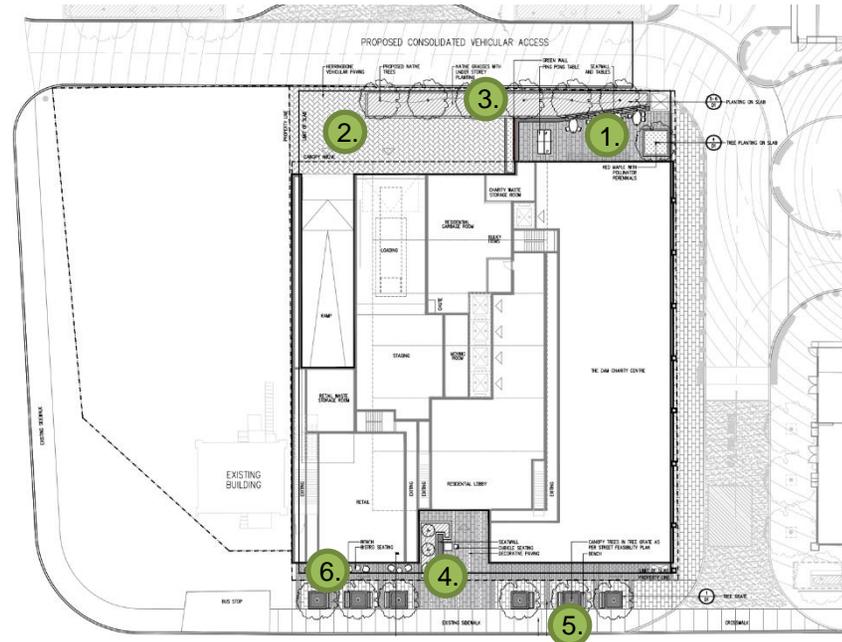


Figure 7: Site Plan - Landscape Features

2.2 Built Form and Uses

The proposed development is a 35-storey mixed use residential tower, inclusive of a six-storey podium. The tower portion is stepped back from the podium at all building faces. Located at-grade is a commercial space and a two-storey non-profit community component belonging to the DAM Youth Charity. The design splits these components up along the façade of the building by recessing the residential entrance located in the middle of the podium at the ground floor. This feature encourages a seamless transit between uses, making it easier to decipher the different uses while exhibiting an integrated design. The seventh floor of the building is stepped back 6 metres from the front edge of the podium to encourage a more walkable and pedestrian friendly design by not imposing on the street frontage with the entire height of the building.

The podium features thick architectural column details outside the curtain wall façade adorned with thin mullions. The floor slabs are accentuated with thick banding detail to not only allude to the interior uses and focus, but to also create a unity and visual interest along the podium exterior. The tower is centered in the podium, with generous setbacks that provide visual interest and additional exterior amenity space. The tower utilizes a concrete slab construction wrapped in a curtain wall exterior with balcony details facing Kirwin Avenue, and the development to east. The spacing between the glass panes and the mullions is smaller so that the tower reads with an architectural language different than the podium below. The roof of the tower has a mechanical penthouse roof with an angular top that slopes downward from Hurontario Street to the back of the site, where the private shared road is.



Figure 8: Aerial View from Hurontario St. Looking East

Gross Floor Area	Total: 27,109.0 m² (291,799 ft²)
Charity:	1,406.4 m ² (15,138 ft ²)
Retail:	195.3 m ² (2,102 ft ²)
Residential:	25,507.3 m ² (274,558 ft ²)
Density	10.89
Setbacks	
Front Yard:	2 m (6.56 ft)
Side Yard:	0 m (0 ft)
Rear Yard:	8.1 m (26.57 ft)
Height	35 storeys 120 m (incl. M.P.H)
Residential Units	Total: 431
1 BD – 1 BD+DEN:	297
2 BD – 2 BD+DEN:	125
3 BD:	9
Parking	Total: 280 spaces
(Res: 0.65 ratio)	Visitor: 65 spaces
(visitor: 0.15 ratio)	Barrier Free: 17 spaces
	Bicycle: 281 spaces

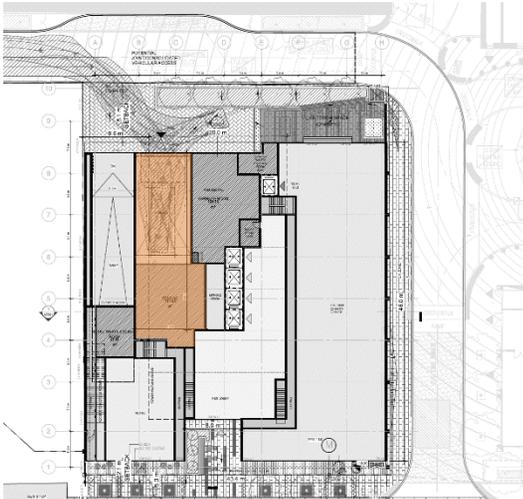


Figure 9: Site Plan - Staging and Loading

Parking

The proposed development contains a total of 280 underground parking spaces, 65 of which are for visitors. This results in a parking ratio of 0.50 for residential parking and 0.15 for visitor parking, with 4% of all parking spots having a barrier free designation. There are also 281 bicycle racks consisting of 259 long-term and 22 short-term spaces.

Loading and Staging

The proposed development features 136 m² of residential garbage facilities and 112 m² of residential staging area. The retail waste room is 43 m² and the charity component is 20 m². The garbage facilities are consolidated in one area for ease of waste removal from the City. All waste facility numbers conform to the Region of Peel's Waste Management Guidelines. The proposed development also features 8 garbage bins, 10 recycling bins, and 2 organics bins on site as per the requirements from the City.

Amenity Space

The proposed development has approximately 2,465 m² of amenity space. The DAM Youth Charity has approximately 459 m² of outdoor amenity space on floors 1 and 2, and the residential tower has approximately 534 m² of outdoor amenity space on floors 3 and 7. The indoor amenity space of the building totals approximately 1,298 m² and is disbursed throughout the proposed development.

The amenity space has the following features:

1. Outdoor amenity courtyard for the DAM Youth Charity at grade.
2. Outdoor amenity space for the DAM Youth Charity on the second floor.
3. Outdoor amenity space for the Residential Tower on floors 3 and 7.
4. Flexible interior residential amenity space on the 3rd floor podium.

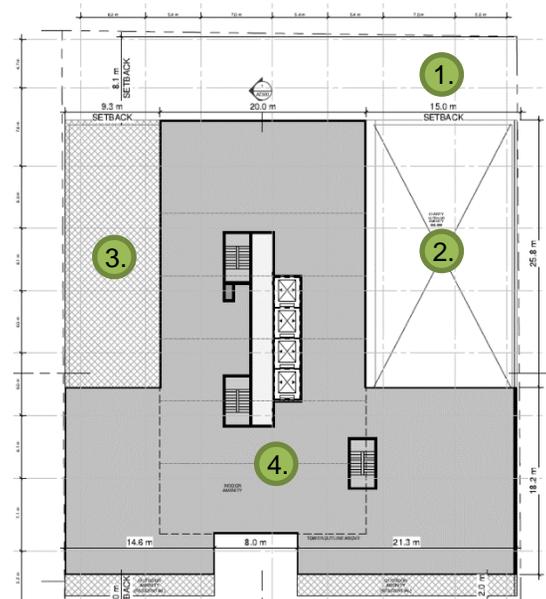


Figure 10: Site Plan - Amenity Space

2.2.2 Height and Massing

The proposed development consists of a 35-storey tower with a mixed-use, community-oriented podium facing Hurontario Street. The podium is divided for two main uses, retail and community space. The portion of the podium fronting Hurontario Street, closer to Kirwin Avenue contains public retail space. The podium contains a purposeful divide from the three uses at-grade, with retail on one side, the entrance to the Charity use on the other, and split in the middle with a recessed residential entranceway. This recessed entranceway contains the residential lobby and main access point off Hurontario Street. The podium component closer towards Dundas Street contains the DAM Youth Charity. This space extends all the way to the back of the building, with an outdoor area, providing a much-needed large open space for the youth of Mississauga to utilize. The podium is slab construction with a curtain wall exterior with exterior columns and slabs. The podium is higher along Hurontario Street and steps back towards the rear of the property.

2.2.3 Building Circulation

The proposed development has established setbacks to allow for proper circulation around the site. The building has pedestrian circulation from both the front and the back of the site so that there is easy entry from both Hurontario Street and Kirwin Avenue. The site utilizes different paving strips as well as other landscape elements to distinguish the circulation areas that are private versus the circulation areas that are public.

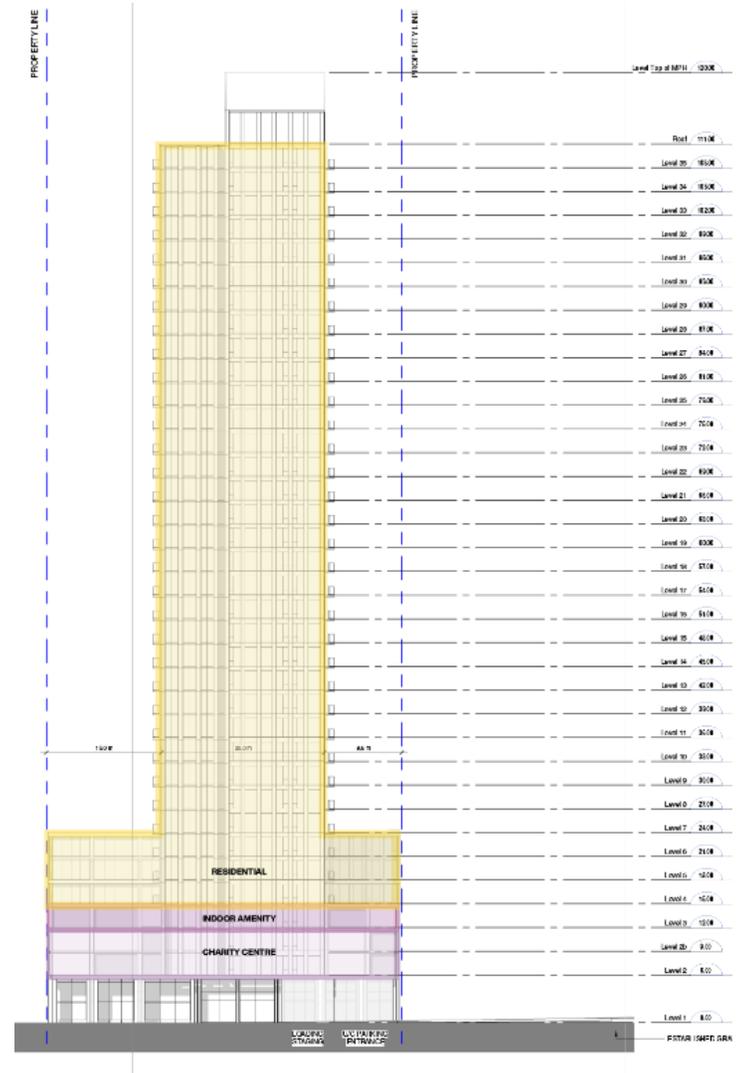


Figure 11: North Building Elevations

2.2.4 Setbacks and Floorplates

The building's podium is setback 2 metres from Hurontario Street and 8.1 metres from the rear property line and the vehicular access point at the rear area of the site. In order to maximize the use of existing infrastructure, this property proposes 0 metre setbacks in order to incorporate appropriate space for each proposed use. The Hurontario Street frontage steps back an additional 8.0 metre where the residential entrance is to create a recessed entrance vestibule. The DAM Youth Charity, which occupies the majority of the southern portion of the lower podium, has a rear setback of 8.1 metres, this area also contains a landscaped outdoor space specific for the Charity use.

On the second floor, the Hurontario Street frontage setback remains the same as the previous floor. The Charity portion of the site steps back approximately 26 metres from the rear property line and 15 m from the side property line to allow for an outdoor amenity space for the DAM Youth Charity. This setback from the rear is mirrored on the other side of the podium of the residential tower on the third floor, thus keeping the six-storey podium to correspond with the surrounding proposed developments while utilizing the rear portion of the property for outdoor amenity space, where it is expected to be more private and have less noise from the Hurontario Corridor. The rest of the building setbacks remain the same from the previous floor. Also on the third floor is a 2 metre setback for outdoor residential amenity space and terraces on the floors above.

Floors 4 through 6 follow the same built form as floor 3 following the same setbacks and architectural form as below. Above the podium starting on the seventh floor is the tower portion of the development which is setback 8 metres from the front property line along Hurontario Street, 15 metres from the south property line, 9.3 metres from the north property line and in keeping with the rear of the podium, 8.1 metres setback from the rear property line.

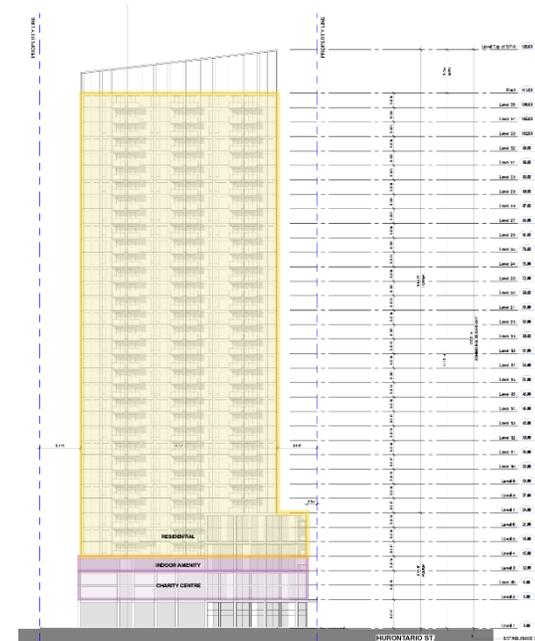


Figure 12: West Building Elevations

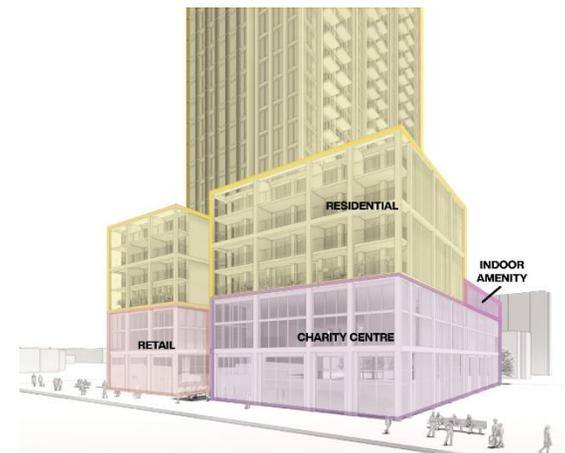


Figure 13: 3D View of Building Uses

2.2.5 Transitions and Angular Plans

The City of Mississauga Official Plan and Urban Design Guidelines seek a minimum 30 metre tower separation distance between towers. The neighbouring development is proposing, at the time of writing this Study, 3 to 4 towers which are abutting the subject site to the east and south. In response to the existing contact, the project architects, Sweeny&Co. Architects undertook a Block Plan (Figure 14) analysis to ensure that the proposed development is in keeping with this policy. It was determined that the Tower portion of the proposed development is situated 31.1 metres from the proposed towers on the neighbouring development.

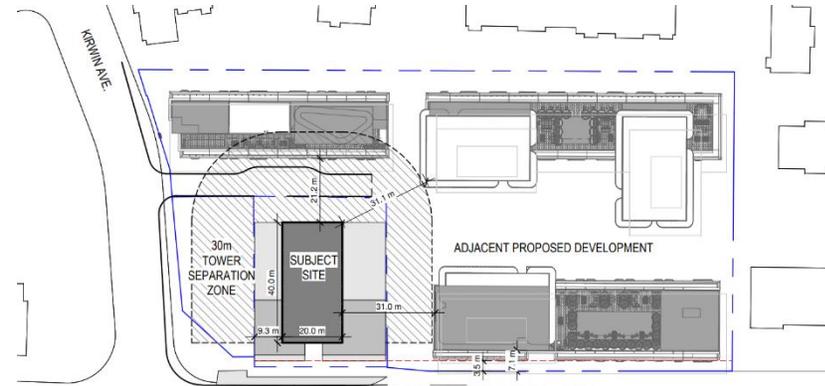


Figure 14: Block Plan

2.2.6 Visual Impact Before and After Development

The proposed development's massing, setbacks, step-backs, and orientation has been designed to respond to the surrounding context in a respectful way. The building form provides a more open and human scale frontage and massing along the vibrant Hurontario Street and a more residential focused frontage along the side and rear property line. The proposed development's height is reflective of the adjacent development proposed on 3085 Hurontario Street. These developments respond the evolving landscape of Hurontario Street and provide much needed density along the corridor while remaining respectful to the pedestrian realm and surrounding residential neighbourhoods. The height and massing are a perfect balance of added height and density while remaining cognisant of the local community, their needs, and the infrastructure in the neighbourhood that exists currently, and that is planned for the future.



Figure 15: Aerial View from Hurontario St. Looking North



Figure 16: Aerial View from Hurontario St. And Kirwin Ave. Looking East

2.3 Access, Circulation, Parking, and Services

The proposed development has been designed to aid in the circulation of pedestrians, cyclists, and vehicles in and around the site. Safe and effective vehicular circulation has been developed without obstructing the pedestrian realm.

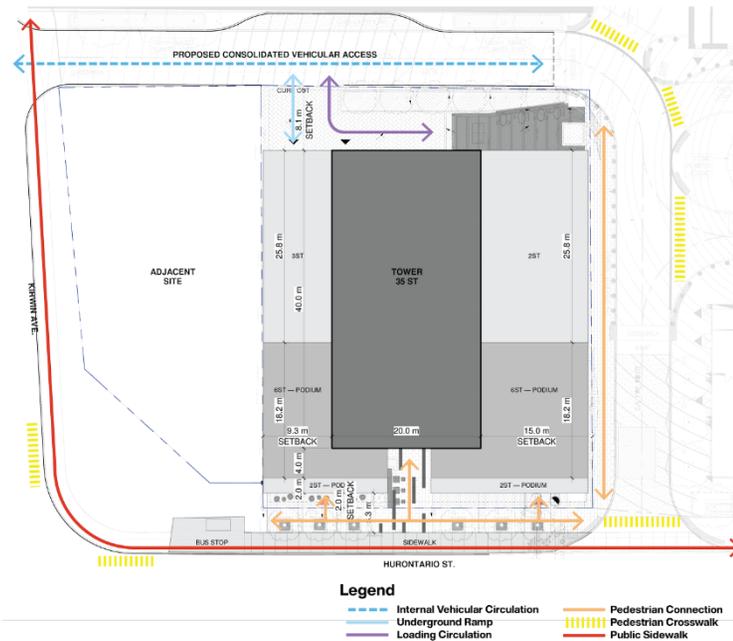


Figure 17: Access and Circulation Map

It is intended that a reduced width roadway, in combination with a clearly defined sidewalk network and reduced building setback, will lower vehicular speeds and reinforce a comfortable, pedestrian realm.

2.3.1 Pedestrian Access and Circulation

A key principle for the development is the pedestrian connection. The site is within a 5-minute walk of the future Hurontario LRT line, the Dundas Street BRT line, and the Cooksville GO station, which makes the pedestrian circulation especially important. The proposed development not only has a sidewalk along the front building face, but also has a building setback, so that there is ample space for pedestrian interaction between the site and neighbouring sites. Sidewalks and walkways proposed within the development area will directly link with the public sidewalks on Hurontario Street to encourage pedestrian connections within and throughout the surrounding development area.

2.3.2 Vehicle Access and Circulation

Vehicular access into the subject site will be off of Kirwin Avenue to the rear of the property. This access is possible through a partnership with the neighbouring development and will loop to the south abutting the property line. The proposed consolidated vehicular access will connect to a 6.0 metre driveway ramp leading to the below grade parking. The space leading to the driveway entrance of the building is approximately 8.1 metres wide and allows for easy truck movement into and out of the loading space located on the ground floor in the rear of the building.

2.3.3 Servicing

Waste removal is located at the back portion of the site on the ground floor to allow for consolidation of this service and minimal impact to the public realm. The facility has been sited to enable garbage trucks to easily maneuver for pickup within the planned private street framework. The consolidation of the services between the three programmatic needs of the building allows the development to maximize the usability of these elements and in turn has created servicing that goes above the minimum requirement.

2.3.4 Underground Parking and Access

The site can be accessed off Kirwin Street through the consolidated vehicular access road that is shared with the adjacent development. This vehicular path leads into the underground parking which wraps around the east side of the site and underground and occupies 4.5 storeys below grade for a total of 280 vehicle parking spots. The proposed development also has 281 bicycle parking spaces to help further promote and encourage a healthy lifestyle to its residents.



Figure 198: Parking Plan

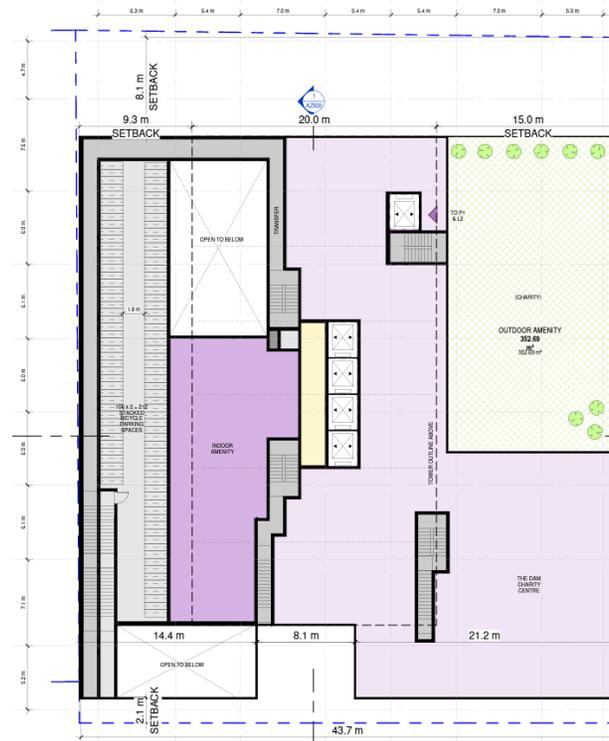


Figure 189: Second Floor Charity Use

2.4 Supporting Studies

2.4.1 Sun Shadow Analysis

Sweeny&Co. Architects completed a sun shadow analysis for the proposed development. The study shows that the shadow impact is in keeping with the surrounding developments and proposed development and produces the largest shadows to the north and northwest in the mornings during the summer and winter months, and the least amount of shadows in the summer months in the mid afternoon hours.

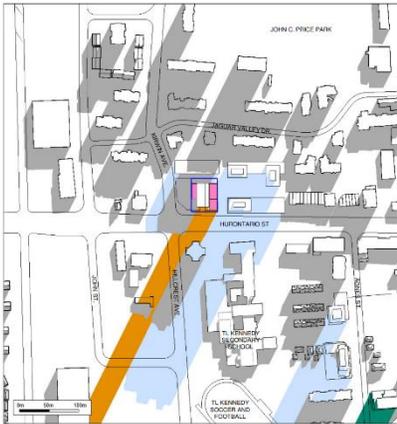


Figure 220: June 21st at 7:07 am



Figure 20: June 21st at 2:20 pm

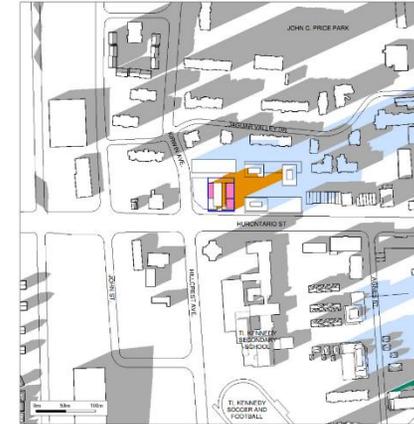


Figure 22: June 21st at 7:33 pm



Figure 213: December 21st at 9:19 am



Figure 24: December 21st at 11:17 am



Figure 23: December 21st at 3:15 pm

2.4.2 Pedestrian Wind Study

RWDI was retained to conduct a pedestrian wind assessment for the proposed development. This assessment was based on the wind tunnel testing conducted for the proposed development site under the existing and proposed configurations of the site and surroundings. The predicted wind conditions are summarized as comfortable for the intended use throughout the year, with winter winds being labelled as uncomfortable in some areas such as the outdoor Charity amenity area and floors 3 and 7 outdoor amenity spaces. Ultimately, the Wind Study concludes that the existing wind conditions are comfortable for sitting or standing in the summer and walking or better in the winter. These conditions are typical for this area of Mississauga and are suitable for the intended pedestrian use. The pedestrian wind safety criterion is met at all areas assessed in the existing configuration.

2.4.3 Noise and Vibration Impact Study

RWDI was retained to prepare a Noise and Vibration Impact Study for the proposed development. This study was required to assess the noise impact from the surrounding roadways, Hurontario Street, Dundas Street, and Hillcrest Avenue, as well as the CP Rail located to the north, on the proposed residential development. This Study also assessed the impacts of the proposed development on its surroundings and itself which is predicted to meet the applicable criteria.

The study concludes with noise control measure recommendations for the proposed development such as materials for windows and balcony doors, implementing perimeter noise barriers along the outdoor amenity areas, and noise warning clauses related to transportation sound levels. It was also determined that vibration from the future Hurontario LRT is not expected to affect the proposed development therefore no mitigation measures for vibration are required.

3 Summary and Conclusion

The proposed development aids in the realization of the vision for an intensified, activated Hurontario Street. The development will provide an active street edge along Hurontario Street, establishing a vibrant and urban public realm with high quality materiality.

The built form respects the intensification of the corridor while also allowing for an appropriate transition to the lower density residential land use to the west. These varying land uses are respected through the use of similar materials, the employment of a setback with landscape buffer and privacy screening, as well as the incorporation of building step backs. Further, the supporting studies, including a Shadow Study and Wind Study, illustrate that the built form will not negatively impact the existing surroundings.

The proposed development seeks to respect the existing conditions, while also being flexible to change as the corridor develops and becomes an intensified transit-oriented hub. Increased density will support transit ridership and the development of a vibrant multi-modal corridor. The proposed development for 3115 Hurontario Street works towards achieving the future vision for Hurontario Street.

Respectfully submitted this 4th day of October, 2022.

Regards,

IBI GROUP