



Lakeshore Transportation Studies

Lakeshore Transportation Studies from Winston Churchill Boulevard to Etobicoke Creek

New Credit River Active Transportation Bridge Study

**Public Feedback Report from Credit River Active Transportation Bridge
Design Workshop held May 3, 2022, and Online Survey available from April
26 to May 10, 2022**



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Public Feedback Report from Credit River Active Transportation Bridge Design Workshop and Online Survey

About This Report

The Lakeshore Transportation Studies include three infrastructure projects in the Lakeview, Port Credit and Clarkson communities that build from the 2019 Lakeshore Connecting Communities Transportation Master Plan. These are the Lakeshore Bus Rapid Transit (BRT) Study, Lakeshore Complete Street Study and New Credit River Active Transportation Bridge Study. The Credit River Active Transportation Bridge Design Workshop and Online Survey were undertaken to gather input on the vision and the specific design elements to inform the future detailed design of the bridge.

There were two ways to participate:

1. By attending the virtual design workshop hosted by Councillor Stephen Dasko held on Tuesday, May 3, 2022, from 6:30 to 8:00 p.m.
2. By visiting the Project Website to complete a survey of AT Bridge design elements. The survey was available 24 hours a day, 7 days a week from April 26 to May 10, 2022.

This report, prepared by the Community Engagement Facilitator Sue Cumming, MCIP RPP, Cumming+Company (cumming1@total.net) together with HDR Corporation, includes a summary of common themes and key messages with the verbatim public input that resulted from the Active Transportation Bridge Design consultations.

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For Additional Information on this project or to provide written comments at any time, please view the City's website at <http://lakeshoretransportationstudies.ca>. and contact:

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Lakeshore Transportation Studies

Public Feedback Report from Credit River Active Transportation Bridge Design Workshop and Online Survey

1. HOW THE PUBLIC CONSULTATION WAS ORGANIZED

The Lakeshore Transportation Studies include three infrastructure projects in the Lakeview, Port Credit and Clarkson communities that build from the 2019 Lakeshore Connecting Communities Transportation Master Plan. The new Credit River Active Transportation (AT) Bridge Study is being conducted as a Schedule B Class Environmental Assessment (EA) Study and Preliminary Design and is one of three infrastructure projects being undertaken in the Lakeshore Corridor. The EA Study for the Active Transportation Bridge is concluding. Following EA approval, the Active Transportation Bridge will move to detailed design, anticipated for later this year.

The preferred alternative is a *signature* bridge, which provides the opportunity for community input on the bridge aesthetics, function and experience which would be considered in the detailed design during the next phase of the project. Feedback gathered from the Bridge Design Workshop and Online Survey will inform design principles to be carried forward to the detailed design. The exact alignment is currently under review with adjacent property owners. The Context Map for the new Credit River Active Transportation Bridge is shown **Figure 1**.

Figure 1 – Context Map



The purpose of the consultation included:

- Providing information on the cross-section and design parameters:
- Gathering input on design aspects for the AT bridge to inform the future detailed design:
- Presenting preliminary concept sketches for an arch and truss bridge type for discussion.

Notification of the consultation included the following:

- Newspaper notices through Mississauga News on April 21, 2022
- Notice on City's webpage with link to online survey and registration for virtual workshop meeting up until May 10, 2022
- Email notice to Lakeshore Transportation Studies Project Mailing List (individuals who signed up for notification through Lakeshore Transportation Study POH 1 and POH2) on April 19 and April 20, 2022.
- Letters to Stakeholders and Public Agencies
 - a. Email notice to Indigenous Communities on April 20, 2022
 - b. Email notice to City Neighbourhood Associations on April 19 and April 20, 2022
 - c. Email notice to City Committees on April 19 and April 20, 2022
- Councillor Stephen Dasko, Ward 1, E-newsletter on May 2, 2022

Community members were able to participate in two ways as follows:

By attending the virtual Bridge Design Workshop hosted by Councillor Stephen Dasko held on Tuesday, May 3, 2022, from 6:30 to 8:00 p.m. The meeting was held as a webinar and residents registered in advance of the session. The format included a presentation and question and answer session with the project team. The project team presented bridge design elements and concepts and facilitated a series of questions (polls) to gather participants' input. The discussion was led by Councillor Dasko who had the project team respond to questions and provide clarification on design elements and concepts. 38 individuals participated in the Bridge Design Workshop.

By visiting the Project Website to complete an Online Survey. The survey was available 24 hours a day, 7 days a week from April 26 to May 10, 2022. The survey included bridge design elements and two bridge design concepts (arch and truss), and participants were able to respond to questions which asked about their preferences. In addition, survey respondents could provide other comments. 166 individuals completed the survey with 92 providing comments about the project.

Presentations at the Bridge Design Workshop were provided by the following:

- Gino Dela Cruz, City of Mississauga Project Manager
- Nico Malfara, HDR, Project Manager
- Mark Langridge, DTAH, Bridge Architect
- Sue Cumming, Cumming+Company, Community Consultation Facilitator

Information for Bridge Design Workshop Online Survey was organized around the topics shown at **Figure 2**.

Figure 2 - Consultation Topics and Materials

Background and Context for the new Credit River Active Transportation Bridge	<ul style="list-style-type: none"> • Context Map for the new Credit River Active Transportation Bridge showing the location. It was noted that the exact alignment is currently under review with adjacent property owners. • Preferred Cross-section from the EA Study and design parameters: <ul style="list-style-type: none"> — Bridge length of 66 metres, — Vertical clearance from water of 5.5 metres (+/- 1.25 metres) — Minimum cycling width (1.5 metres per direction) — Minimum pedestrian width (1.5 metres) — Safety and accessibility standards and, — Sustainable design practices • Reference photos along the Credit River from the west and east side in the vicinity of the AT Bridge location.
Bridge Design Elements	<p>Photos and images were provided in the workshop presentation material and in the survey to show ideas and examples of approaches for the following:</p> <ul style="list-style-type: none"> • Aesthetics: Look and feel of the bridge within the community • Function: Ways to accommodate all AT users • Experience: Elements that can create a good experience for all bridge users
Preliminary Bridge Concepts	<ul style="list-style-type: none"> • Bridge Concept 1: Above-Deck Arch • Bridge Concept 2: Bowstring Truss

2. COMMON THEMES AND KEY MESSAGES HEARD

This report section includes a high-level synthesis prepared by the Community Engagement Facilitator on the key messages that were noted through the Bridge Design Workshop and Online Survey. It is important that this synthesis be reviewed together with the verbatim detailed input found in this report. Section 3 includes the responses to the polling questions at the and feedback through the discussion. Section 4 includes the responses to the Online Survey and other comments noted. This synthesis and the detailed feedback will be used to develop the final report for the EA study and will ensure that the input on bridge aesthetics, function and experience is considered in the recommendations for the bridge and identifies and considerations for the detailed design.

There is much interest in the AT bridge. Questions about the timing and funding for the Bridge were discussed at the Bridge Design Workshop. There is interest in moving ahead in the short term to see this bridge built. Safety considerations were noted stressing the need to have a safe crossing for kids and adults across the river in this location. Comments about the desire for future connections to the GO Station and the need to connect into the broader city AT network were received. Several individuals expressed concerns about potential future traffic impacts at Mississauga Road and parking impacts. It was noted that these would be addressed during the detailed design process. These and other comments from the Bridge Design Workshop are included in **Figure 17** and from the Online Survey in **Figure 23**.

A preliminary topic for discussion was how important it is for the bridge to be designed to stand out as a statement piece in the community vs. how important it is that the bridge blend into the environment and its surroundings. There is significant support for the bridge to be special and beautifully designed and to do so in a manner that fits into the existing natural environment and history of the Credit River - a statement bridge that fits in.

There is also significant support for the bridge to be more than an active transportation corridor but also a special place where the community would enjoy walking and cycling to. As noted through the response to workshop polling questions, survey questions and additional comments, there is a high degree of support for the bridge to be a place which allows all users to pause and spend time on the bridge. Over 75% of participants in the polling questions at the Bridge Design Workshop and survey respondents indicated that this was very important. This is also reflected in the support for and strong interest in having rest areas (benches) and viewing areas along the bridge in addition to at the approaches on the west and east side. 74% of the participating workshop participants noted this to be very important and over 50% of the survey respondents noted this to also be very important and a further 38% noted it to be somewhat important.

The organization of the active modes of transportation and ability to accommodate cycling across the bridge in a manner that provides safety for other users was confirmed to be very important. Three example cross-sections were shown for discussion purposes which involved the following options:

1. Cycling and pedestrian modes that are shared with cyclists encouraged to slow down.
2. Cycling and pedestrian modes are separate, with centre running lane, and
3. Cycling and pedestrian modes are separate.

There is minimal interest in option 1 of having a shared cycling and pedestrian space. Most responses indicate significant support for separating cyclists from pedestrians. Many want to ensure that the bridge is a comfortable place which minimizes conflicts and while they would like to see cyclists slow down while crossing the bridge, there is recognition that a clear delineation of space is preferred to having to rely on the behaviours of individual users. The

survey respondents indicated a preference for option 2 (centre-running lane) and the workshop participants for option 3.

Another key focus of the consultation was to discuss how the bridge should function within the community with two scenarios highlighted as follows:

- the bridge as transportation – a link to go from place A to B or as a destination
- the bridge as a place to meet, socialize and leisurely stroll.

Given the desire for the bridge to be a place where people pause to rest or take in the views north and south, there is support for flexible approaches that do not block off one side of the bridge for cyclists only. They would like to see this explored further as part of the detailed design. The idea of a centre running cycling lane shown on the Arch Bridge Concept received positive feedback which supports the desire for pedestrian to have access to both the north and south sides of the bridge. The pedestrian area shown on the Truss Bridge Concept was also seen as highly desirable as it creates a separated area for leisurely strolling across the bridge. Participants would like to see a less barricade approach and a more flexible way of accommodating separated cycling and pedestrian.

There is also appreciation for the benefits of developing the bridge as an important active transportation corridor with keen interest in having the bridge become part of the larger area active transportation network appealing to different types of users. There is a strong preference for the bridge also as a place which can be enjoyed while travelling from one side to the other.

The width of the bridge is an important consideration that may require more review to ensure that the bridge can be designed to reflect the community's desire for the accommodation of separated cycling and pedestrian areas, while allowing access to the whole bridge and rest and viewing areas along the bridge.

There is support for including look out and seating areas with opportunities for viewing both to the south towards the park and across the river and to the north towards the existing rail bridge and valley.

Lighting that is integrated within the structure that provides for safety and for accenting the design and ambiance of the bridge is desired. Several individuals who indicated that they were local residents would like to ensure that the lighting is done in a manner that doesn't spill into the sky and has a low-level intensity at night to avoid light pollution.

There is support for incorporating interpretative elements and local and indigenous art.

With respect to preferences for the bridge design concepts, the workshop participants noted that Concept 2, the Truss Bridge Design better reflected their ideas about the bridge. The Online Survey Respondents noted that Concept 1 better reflected their ideas about the

bridge. Reasons cited for preferring each bridge type are shown in **Figure 3**. These represent different individual's comments.

Figure 3 – Reasons cited for Preferring each Bridge Design Concept

Bridge Concept 1 – Above Deck Arch	Bridge Concept 2 – Bowstring Truss
<ul style="list-style-type: none"> I would love to see an arch-type bridge that is as "invisible" as possible so that the natural beauty of the river and banks can be the main features. The curve of the arch design mimics the curve of the river. Provides pedestrian access and lookouts on both sides of the bridge – north and south. Arch illuminated at night enhances the outline of the bridge and during the day the curved design reflects nature better. 	<ul style="list-style-type: none"> Compliments the existing rail bridge better. It is less intrusive and may fit better within the natural environment. Provides better opportunities for look out areas and seating along the bridge. Creates an attractive and desirable pedestrian area for seating and lookouts on the south side of the bridge. Addresses the safety issue of separating senior walking groups from bike riders.

Specific ideas for consideration for lighting, seating and look out areas, interpretive elements and materials and other ideas are noted in **Figure 4**.

Figure 4 – Topics and Key Messages Heard

Topic	Common Themes and Key Messages Noted
Lighting	<p>Different approaches for lighting the bridge were discussed. The following feedback was noted:</p> <p><i>Functional and accent lighting to illuminate the above-deck areas and highlight key features:</i> 78% of both the workshop participants and survey respondents noted that functional and accent lighting was either somewhat or very important. Of the 78%, 40% of the workshop participants and 46% of the survey respondents identified functional and accent lighting to be very important.</p> <p><i>Concealed lighting or fixtures connected to the above-deck structure:</i> 75 to 80% of both the workshop participants and survey respondents noted that concealed lighting is either somewhat or very important. 50% of both indicated that concealed lighting is very important</p> <p>Common themes for consideration in the detailed design include the following:</p> <ul style="list-style-type: none"> The bridge should be appealing day and night. Desire for attractive lighting that accents the bridge without spilling up into the sky. Make it well lit but not so bright that it causes light pollution.

Topic	Common Themes and Key Messages Noted
Lighting (continued)	<ul style="list-style-type: none"> • Desire for low level lighting after sunset, with the potential for higher intensity lighting activated by motion sensor on the approaches to the bridge. • Ensure that the bridge has nighttime lighting which was noted to be a safety feature for use of the bridge at night. • The bridge should blend into the environment with concealed lighting, especially between 11 pm and 5 am, when lights are a nuisance for people living in the area • Consider using of solar power for the lighting.
Look out areas and seating	<p>Seating and look out areas were noted to be important elements to have incorporated in the bridge design to create a comfortable place. 74% of the workshop participants noted that look out areas and seating were very important and a further 22% that it is somewhat important. 50% of survey respondents noted that look out areas and seating are very important and a further 38% that it is somewhat important.</p> <p>Common themes for consideration in the detailed design include the following:</p> <ul style="list-style-type: none"> • There is a desire to have seating and lookout areas on the south side of the bridge. • Many would like to see views to the north as well as the south noting the opportunity view the historic rail bridge and natural beauty up the valley. • It is important to make places to pause inclusive to individuals using mobility aids like wheelchairs. Wide space between seating and rail, seating that takes into account differing mobility issues, and the placement of enhancement elements so they can be viewed/enjoyed from a seated position are important. • Both design concepts provide the opportunity for seating at the approaches to the bridge. • Concept #2 (Truss) provides better opportunity for look out areas and seating along the bridge.
Interpretative Elements	<p>Opportunities to enrich the pedestrian experience, educate onlookers on interesting aspects of the setting i.e., history, flora/fauna and opportunities for public art by local and/or indigenous artists were discussed. Participants at the workshop were asked how important interpretive elements are. Over 88% indicated that interpretive elements were either very important or somewhat important and would like to see these considered and included in the detailed design. 63% indicated that interpretive elements were very important.</p>

Topic	Common Themes and Key Messages Noted
Interpretative Elements	<p>While there was no specific survey question for interpretive elements, a number of comments were noted. Specific ideas noted for future consideration are:</p> <ul style="list-style-type: none"> • Education/signage on indigenous history and art created by hired and recognized Indigenous artists from the Region. • Plaques showing the historical significance of the Credit River. • Education on the history of the rail bridge. • Plaques and educational elements about the natural environment. • Information about broader trail network and how to connect to it. • Viewpoints that offer what a specific spot may have looked like in the past (dioramas with telescope viewing). • Painted designs and elements that celebrate the local area i.e., piano at Lakeshore and Hurontario, fish at Stavebank and Hurontario.
Material, Finishes and Other Ideas	<p>The following were identified in the comments from both the workshop and the survey as ideas that participants would like to see considered in the detailed design of the bridge:</p> <ul style="list-style-type: none"> • Canadian built. • Use of sustainable methods including long term maintenance and how the bridge would age over time. • Attention to winter maintenance. One individual noted interest in the possibility of heating elements provided these could be cost-efficient. • Traffic calming measures on the bridge to keep speeds low. Signage or other measures noting that some individuals do not pay attention to signs. • Graffiti proofing paint finishes. • Debris management noting concerns should items that could come from the rail bridge. • Review of wind conditions on the bridge. One resident noted concerns about the bridge being too windy. Other residents felt that this would not be a problem. If wind barriers were determined to be needed, there is a desire for these to be open to the sky and non-intrusive to the views and feeling on the bridge. Wind management to create a comfortable space with the potential for wind barriers. materials. • Features to deter jumping off the bridge were noted to be of importance. • Ample clearance for boating activities.

3. PUBLIC FEEDBACK FROM THE BRIDGE DESIGN WORKSHOP

This report section includes the public feedback that was received at the Bridge Design Workshop held on May 3, 2022, from 6:30 to 8:00 p.m. The workshop was hosted by Ward 1 Councillor Stephen Dasko and held via a Zoom Webinar. The format included a presentation and question and answer session with the project team. The project team presented bridge design elements and concepts and facilitated a series of guiding questions (polls) to gather participants' input on the vision for the AT bridge. The discussion was led by Councillor Dasko who had the project team respond to questions and provide clarification on design elements and concepts.

Participants were able to ask questions and provide comments through the Q and A by either providing written comments which were read aloud by the Councillor or by providing these verbally. 38 individuals participated in the Bridge Design Workshop. The following public input is organized by workshop discussion topics. The responses provided to the polling questions are included along with the verbatim comments and questions. All input provided in the Q and A are included in this report. Personal and identifying information has been omitted from the report.

3.1. Input on Bridge Aesthetics: Look and Feel

Different Bridge Design images were shown at the workshop and are included in **Figure 5**.

Figure 5 – Aesthetics Bridge Form and Image

Aesthetics | Bridge Form & Image

"Signature" bridge expression – what fits best in this setting?

Above-deck structure to maximize clearance below – arch or truss?

Examples of arch bridge



Examples of truss bridge



The following shows the polling questions, response choices and how participants responded:

Response Choices	How important is it that the bridge stands out as a statement in the community?	How important is it that the bridge blends into the environment and its surroundings?
Not important	6	2
Somewhat important	12	12
Very important	5	9
Total # Responses	23	23

Comments and questions noted through the discussion about the Bridge Design Aesthetics are included in **Figure 6**. These are numbered for reference purposes and are verbatim.

Figure 6 – Workshop Comments and Questions about Bridge Design Aesthetics

#	Comment/Question Noted	Response
1	Is it possible to have finishes that are graffiti-proof, so it doesn't look like the trestle over time?	<i>Response from Bridge Design Architect:</i> Yes absolutely. This is one of the advantages of having a premium paint finish approach to the bridge, also all the elements of a truss are fairly slender in comparison (its not like it is a big canvas for graffiti). There are very good anti-graffiti coatings out there now which would make it very easy to remove. The advantages of these premium coatings which are applied before it arrives on the site, is that it is very easy to do local touch-ups in an invisible way.
2	(Verbal question). My question is about heating elements. Will heating elements be built into the bridges so that they never ice over or have snow on them, have we considered that? (Follow up question) I have come across some interesting ways to heat not bridges but other platforms and shelters from the top and sides. I'd be curious – because I definitely don't want this to be a heavy maintenance bridge – but I would look forward for the opportunity for the public to share ideas I suppose on a survey would that be the right place?	<i>Response from Bridge Design Architect:</i> All of your questions on that would mostly need to be explored in detail design. The second one is a great idea, unfortunately long-term maintenance of heating cables cast into concrete has had problems and challenges. They work great for a few years but then if you have repair them or open it up to see where the cable isn't working properly, you have to basically jackhammer the entire concrete assembly there. So, I know there have been many challenges with doing it in our challenging exterior environment and winter conditions – but there is no doubt the bridge would need to snow cleared year-round. Follow up response: Yes, all the feedback is going to inform the detail design.

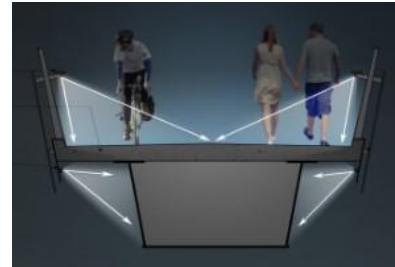
#	Comment/Question Noted	Response
3	(Verbal question) The one thing I would like to point out is has there been a consideration of some sort of barrier on the north side of the Bridge to deflect any kind of debris that could potentially be tossed up by the trains passing by, which could act also as a windbreak so basically some sort of a shield on the side of that crossing.	<i>Response from Bridge Design Architect:</i> This is a good idea - I guess I would say that you would want to build something as transparent as possible so that it doesn't become an eyesore and a solid barrier, and still allows views through. Rather than a tight mesh or something like that, that wouldn't look as nice. So, there might be approaches with polycarbonate panels or tempered, laminated glass that could be attached to the outside of a truss so that it would be invisible and provide that wind barrier. So, if it could be done in an invisible way, it could be a real amenity for pedestrians.
4	I think a wind barrier is unnecessary	Comment is noted
5	We don't like the idea of a wind breaker; it is not necessary and would be an eyesore.	Comment is noted
6	Any barrier along the bridge should also consider birds which often make use of the river valleys and ravines for their migration. https://cvc.ca/news/spring-weather-brings-annual-bird-migration/	Comment is noted
7	Will this be a Canadian built bridge?	<i>Response from Bridge Design Architect:</i> Absolutely. We have to do everything locally these days. We have many excellent metal fabricators around the GTA who could do an excellent job of this.
8	(Verbal question) I wanted to see if we've looked at prefabricated bridges like what is being brought into the Toronto Portlands redevelopment. They are getting four new bridges. They are taking a while to get there, but I believe it is more cost effective to bring them in prefab and they are bringing in several. I think there is an opportunity to get in more crossings on the river further North and I'm curious if that's been explored and that was one thought.	<i>Response from Bridge Design Architect:</i> Yes, obviously prefabrication is increasingly possible in all sorts of areas of construction, but it does need serious cranes and complicated logistics. But the extent to which they can be prefabricated in a factory setting and then transported and then erected needs a lot of technical investigation. 66 meters is a long span, but yes technically its all possible.

Different images for lighting shown at the workshop are included in **Figure 7**.

Figure 7 – Lighting Considerations

Aesthetics | Lighting

- Functional and accent lighting to illuminate above-deck areas and highlight key features
- Concealed lighting or fixtures connected to above -deck structure?



Concealed Lighting



Accent Lighting



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The following shows the polling questions, response choices and how participants responded:

Response Choices	How important is it that lighting be concealed and integrated within the structure?	How important is it to incorporate accent lighting that highlights key bridge features?
Not important	5	5
Somewhat important	12	8
Very important	7	11
Total # Responses	24	24

Comments and questions noted through the discussion on lighting are included in **Figure 8**. These are numbered for reference purposes and are verbatim.

Figure 8 – Workshop Comments and Questions about Lighting

#	Comment/Question Noted	Response
1	Imagine lighting can be finalized later. Darkness is important in Memorial Park West for shore fishing at night, viewing night sky (hard to do in GTA, and enjoying	<i>Response from Bridge Design Architect:</i> I would just reiterate that you're absolutely right that it's important not to overdo the lighting to really aim it and shield it so that it just highlights the structure and doesn't spill into

#	Comment/Question Noted	Response
	the glow of lighthouse beacon on the rippling water. More of a comment, but can lighting not compete with the lighthouse, or be too bright? Internal lighting seems good.	the sky or water. These days with all of the LED fixtures and shields that are available you can do a very nice job of both functional and accent lighting in very tight, restricted angles – so we absolutely can achieve that.

3.2. Input on Bridge Function

Approaches for how the bridge would function were shown and are included in **Figure 9**.

Figure 9 – How the bridge should function/ accommodate different users

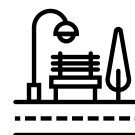
Function | Accommodating Different Users

How should the bridge function within the community?

1. As transportation – a link to go from place A to B
2. As a destination – a place to meet, socialize, and leisurely stroll

Options for organizing different users:

1. Separated cyclist and pedestrian areas with lane markings
2. Central cyclist area with pedestrian viewing areas on each side
3. Shared cyclist and pedestrian area on the bridge, with cyclists encouraged to slow down



24

The following shows the polling questions, response choices and how participants responded:

Response Choices	How important is it that the bridge allows all users to pause and spend time on the bridge?
Not important	0
Somewhat important	6
Very important	25
Total # Responses	31

Response Choices	Which type of functional cross-section do you prefer for this crossing?
Cycling and pedestrian modes are shared with cyclists encouraged to slow down	12
Cycling and pedestrian modes are separate with a centre-running lane	12
Cycling and pedestrian modes are separate	7
Total # Responses	31

Comments and questions noted through the discussion about how the bridge should function are included in **Figure 10**. These are numbered for reference purposes and are verbatim.

Figure 10 – Workshop Comments and Questions about How the Bridge Should Function

#	Comment/Question Noted	Response
1	My comment is to please make places to pause inclusive to individuals using mobility aids like wheelchairs. Wide space between seating and rail, seating that takes into account differing mobility issues, and the placement of enhancement elements so they can be viewed/enjoyed from a seated position.	Comment is noted.
2	I like the cycle track on the north side!	Comment is noted.
3	Thinking of this as part of a transportation system, what is the relationship between this bridge and the existing one by the lighthouse? That one is primarily pedestrian. Shouldn't this one ensures that it is an effective active transportation corridor.	Response from City Staff: Yes. Both bridges should accommodate cyclists and pedestrians. However, the main goal of this bridge is to provide additional connections and improve connectivity over the river for people walking and cycling, specifically to the GO station.
4	It would be great to have a separate pedestrian area as shown in the Truss design. Cyclist and pedestrian shared space are everywhere and sometimes makes for unpleasant walking.	Comment is noted.
5	We can also anticipate that there will be people looking to take photos of the trains passing over the railway bridge and this may cause issues for cyclists moving across the bridge.	<i>Response from Bridge Design Architect:</i> I think that's a very good point, the design team had discussion about this – there are beautiful views up the valley and up the river, so I think we can't just pretend that all

#	Comment/Question Noted	Response
		the nice views are to the south, so this is really the rationale behind my pitch for the whole bridge to act as a shared space, with signage telling people to slow down. This was also done at Garrison Crossing and Fort York where there's actual signage on the multi-use trails telling everyone to take care and look out for others.
6	Any sense of the projected cycling use for this bridge?	<i>Response provided by City Staff following the workshop:</i> Although, future projects for walking and cycling trips are not available, the 2019 TMP estimated that approximately 94% of existing trips (or 24,161 daily trips) in the surrounding vicinity of the bridge meet the criteria for potentially cyclable trips (i.e. trips <2 km that are currently not walked or cycled); therefore, there is a significant opportunity to shift these trips by creating a safe and convenient connection. Furthermore, available data consistently shows Lakeshore Road as a location where cycling activity is high and is an important cycling route. The demand for cycling will continue to increase in the in the future as redevelopment occurs and new rapid transit is built (i.e., Regional Express Rail and the Hurontario LRT). As noted in the City of Mississauga's Cycling Master Plan (2010 and updated in 2018), Lakeshore Road and the future bridge are important corridors for cycling.
7	The plans mention the minimum width required for the bridge to fit cyclists and pedestrians. I hope the city would do more than 'the minimum,' especially if people are expected to stop while crossing.	Comment is noted.
8	Should traffic calming measures be put in place in the cycling lanes to keep risk down	<i>Response from Bridge Design Architect:</i> Yes, I would put my hand up for reasonable ways to encourage cyclists to slow down. I'm an urban cyclist myself, and when you're in mixed traffic you want to make good progress, but when you arrive at a bridge its always necessary to slow down. As much as we can do to traffic calm, to sign, to encourage cyclists to slow down is

#	Comment/Question Noted	Response
		important. Not continuing the lane markings across the bridge, we have found is a very good way of messaging to cyclists that they are entering a special shared zone – and I think signage would need to be done in addition to this so that you treat it like a public space.
9	Regarding shared space and signage, are there other design considerations that could be introduced for those who don't really pay attention to the signage (for whatever reasons)	<i>Response from Bridge Design Architect:</i> This can be looked at during detailed design.
10	In response to having room in the cycling lane for people to stop etc., this will lead to the existing mess of the Palace Pier Bridge whereby there is no structure to the bridge usage, so people do as they please. Let's not create that mistake here.	Comment is noted.

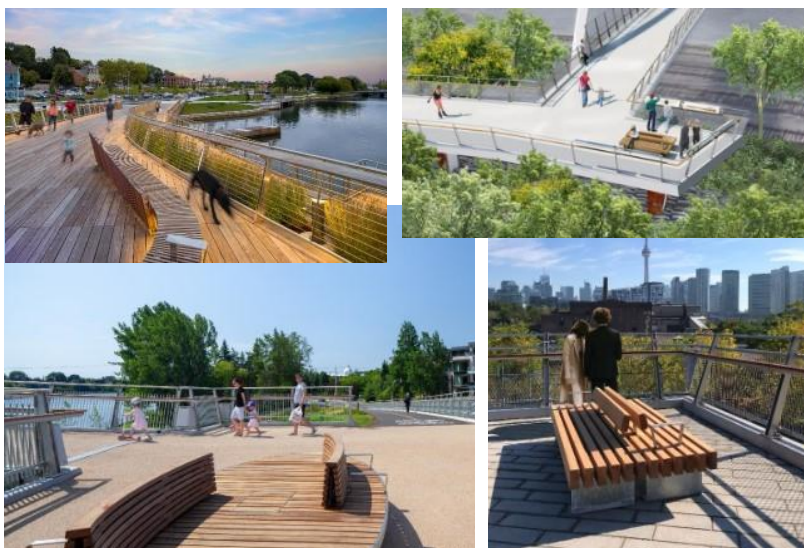
3.3. Input on Bridge Experience

Images were presented for different experiences for the bridge as shown in **Figure 11**.

Figure 11 – Viewing Opportunities and Places to Sit

Experience

- Viewing opportunities at approaches
- Places to sit and gather



The following shows the polling questions, response choices and how participants responded:

Response Choices	How important is it to include look out areas and seating?	How important is it that the bridge functions as a place to pause, admire the view, socialize, spend some time instead of just a means to get from Point A to Point B?
Not important	1	0
Somewhat important	6	12
Very important	20	15
Total # Responses	27	27

Comments and questions noted about viewing areas and places to sit are included in **Figure 12**. These are numbered for reference purposes and are verbatim.

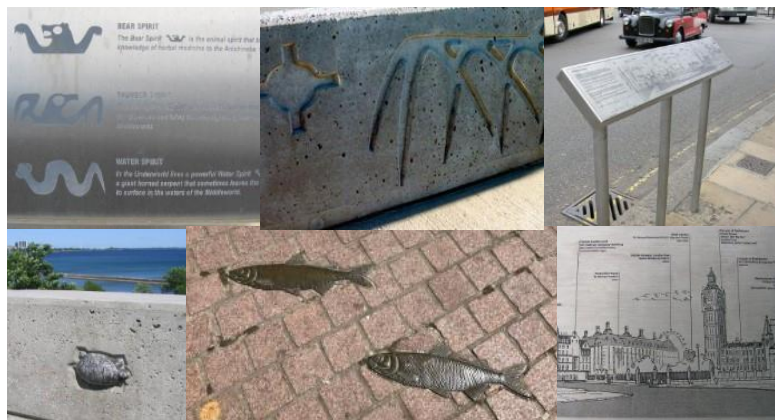
Figure 12 – Workshop Comments and Questions about Viewing Areas and Places to Sit

#	Comment/Question Noted	Response
1	Can the bridge be designed to have a lookout on the north and on the south?	<i>Response from Bridge Design Architect:</i> This can be looked at in detailed design.
2	With the rail bridge close by, how are the sightlines looking north up the river?	<i>Response from Bridge Design Architect:</i> There are beautiful views up the valley and up the river, so I think we can't just pretend that all the nice views are to the south, so this is something that could be looked at.
3	Can one lookout be at the north, another at the south	<i>Response from Bridge Design Architect:</i> This can be looked at in detailed design.

Images were presented for different interpretive elements as shown in **Figure 13**.

Figure 13 – Interpretive Elements**Experience | Interpretive Elements**

- Opportunities to enrich pedestrian experience, educate onlookers on interesting aspects of the setting e.g., history, flora/fauna etc.
- Opportunities for public art by local and/or indigenous artists may also be considered.



33

The following shows the polling questions, response choices and how participants responded:

Response Choices	How important is it to include interpretative elements?
Not important	3
Somewhat important	7
Very important	17
Number # Responses	27

Comments and questions noted about how interpretive elements are included in **Figure 14**. These are numbered for reference purposes and are verbatim.

Figure 14 – Workshop Comments and Questions about Interpretive Elements

#	Comment/Question Noted	Response
1	Public art / interpretive elements can be added later and would be great community project (re indigenous, etc.). I worry up-front cost of too many add-ons might deter quick budget approval. Basic architecture of truss design is beautiful on its own. Can the costs be broken out?	Response needed.
2	How have Indigenous communities been engaged, as well as the Credit Valley Trail organization? I see this bridge as a key element for	<i>Response from HDR:</i> Indigenous communities have been invited to all public meetings and have been circulated all available Natural Environment and

	education/signage on indigenous history and art created by hired and recognized Indigenous artists from the Region.	Archaeological reports. They will also be consulted on the final project report.
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3.4. Input on Bridge Concepts 1 and 2

Two bridge concepts were presented for discussion. These are shown in **Figure 15**

Figure 15 – Bridge Concepts

Bridge Concept 1 | Above - Deck Arch



38

Bridge Concept 2 | Bowstring Truss



41

The following shows the polling questions, response choices and how participants responded

Response Choices	How does Concept #1 (Arch) reflect your ideas for the new Credit River bridge?	How does Concept #2 (Truss) reflect your ideas for the new Credit River bridge?
Does not reflect my ideas for the bridge	8	3
Somewhat reflects my ideas for the bridge	15	3
Reflects my ideas for the bridge pretty well	6	23
Total # Responses	29	29

Comments and questions noted through the discussion about the two bridge concepts are included in **Figure 16**

Figure 16 – Workshop Comments and Questions about Bridge Concepts 1 and 2

#	Comment/Question Noted	Response
1	How many metres from the trestle will this bridge be?	<i>Response from HDR:</i> It is up to 17m at its closest point, and up to 22m at its farthest point.
2	For the Truss design (Concept 2), what is width of the cycling section?	<i>Response from Bridge Design Architect:</i> I agree it looks perhaps narrower in this perspective, so I think our answer to that would be as wide as we think it needs to safely and functionally be and this is something that would be worked out in detail design. There are cyclists going each way and some of these aspects would have to be looked at closer. We would recommend that it be two-way cycle but with some sort of generous buffer at the side.
3	I really like Concept 2	Comment is noted.
4	Concept 2 really addresses the safety issue of separating senior walking groups from bike riders	Comment is noted.
5	Is there much difference in cost between the designs?	<i>Response from Bridge Design Architect:</i> In most situations the arch approach would be more expensive for the actual fabrication of the structure given the curves of the design. With that being said, the truss option with a cantilevered sidewalk makes it into a slightly special truss so my gut feel is that they would be fairly similar in overall cost.

3.5. Other Comments and Questions Noted about the Bridge

Other comment and questions noted at the workshop are shown in **Figure 17**. These include feedback on the location, timing, funding, transportation connectivity to the broader active transportation network, traffic and parking. Comments are numbered for reference purposes and are verbatim.

Figure 17 – Other Comments and Questions Noted at the Workshop

#	Comment/Question Noted	Response
	<i>Timing and funding considerations:</i>	
1	Why do we need a bridge in this location?	<p><i>Response from HDR:</i> This was the focus of the 2019 TMP. The questions that were answered there were what type of bridge was needed, and where and why was it needed. We looked at two different types of bridges in the TMP, bridges that would accommodate vehicles (all users) and then we looked at active transportation only bridges. We did some transportation modelling to look at the future and see how these would be used, and what we found was that if we were to provide another vehicle crossing of the Credit River in the vicinity of the existing bridge south of the railway corridor, basically both of them are congested. There's so much pent-up or latent demand in the area that both become congested. So, the study found for the significant environmental impacts that that vehicle bridge would have, it didn't really make sense to provide one. There's still a need to provide connectivity to pedestrians and cyclists especially to the GO station as was mentioned earlier. So, through the TMP process we recommended looking at the EA for an active transportation only bridge, so that's what we've done, and we evaluated a do nothing scenario versus a few of these bridge options, and we found that with slight impacts on either side of the bridge we could create a really great connection for the community.</p> <p><i>Response from Councillor Dasko:</i> Just wanted to mention that this is something that has come up time and time again, and a few people have looked at a few different ways of</p>

#	Comment/Question Noted	Response
		<p>putting a potential bridge across in a few different areas – one being a vehicular bridge as HDR was just mentioning. Full disclosure, I'm not in favour of a vehicular bridge going across the Credit these are community killers: in this particular situation, it would rip right through memorial park and further continue through and kill the Legion where on the converse side this would be of significant benefit for the Legion as they'll be getting thousands of new people going by on a regular basis and gives people other options for getting out of Brightwater. I'd be happy to chat with anybody offline if you'd like another conversation with regards to the merits of a pedestrian/cycling bridge. I just want to make sure that as we do this bridge we do this right, and we're being sensitive to the Legion here.</p>
2	<p>Can we please talk about realistic timelines? When was this bridge initially contemplated and when will it be completed? Councillor Dasko please share with us your history since day one.</p>	<p><i>Response from Councillor Dasko:</i> Absolutely, I can do that – this was a concept that came up many years ago to be honest. It didn't have a whole lot of life to it and some of the others on the call can speak to that. For me it's been a very important thing, so I have been helping push that as part of our masterplan and to keep it moving forward. One thing I really did push for and was able to get was to get funding through our budget and that was for the environmental assessment process to kick off and be started. That was in the 2020 budget I believe and again we had it budgeted and put through in 2021-2022. We are on a good path right now and are hoping to break ground next year.</p>
3	<p>What is the timeline? I like Bridge Concept #2</p>	<p><i>Response from Councillor Dasko:</i> The timeline we're discussing (if all goes according to the way we'd like it, we have one more budget cycle to go through) will hopefully break ground in 2023.</p>
4	<p>Sorry that I fell of the call did my question get answered re timelines? (In reference to Question #2)</p>	<p>Councillor Dasko reiterated the response provided for #2.</p>
5	<p>Can't believe that it was originally in 2041 Capital Budget AGHHH</p>	<p>Comment is noted.</p>

#	Comment/Question Noted	Response
6	Fifteen years!!! (In reference to #2)	Comment is noted.
7	Thank you so much! (In reference to Councillor Dasko's response to #2)	Comment is noted.
8	Great session!! Can't believe it, actually...been so long!	Comment is noted.
9	What are the lifecycle costs of the materials used in terms of their carbon footprint?	<i>Response from Bridge Architect:</i> All the costing is yet to be completed in detail for these options, that would be part of the detail design process. This would depend on the type of materials that are selected so unfortunately, I can't comment on the costing of those detail items at this time.
	<i>Transportation Considerations:</i>	
10	Do you foresee a parking issue on Mississauga Rd near the bridge?	<i>Response from City Staff:</i> We're aiming to not change the existing condition significantly, so I don't expect there to be a significant parking issue although this is as we mentioned at the beginning, under discussion with our current stakeholders, so depending on how much of the legion's parking lot is impacted or not so it is hard to comment at this time.
11	<p>I have already shared this thought with Councillor Dasko, and I am sharing it with the community members attending tonight for consideration:</p> <p>I would be very pleased to see the bridge be a first step in reconnecting Queen Street through active transportation designs, with the next steps being a level connection to Stavebank, a redesign of the parking lot between Stavebank and the GO station, and someday an active transport bridge over Hurontario Street.</p>	Comment is noted.
12	Is there a link to the environmental template used for the transportation modelling?	<i>Response from HDR:</i> This Study follows the process and requirements of a Schedule B environmental assessment
13	(Verbal question) Unless I've not seen it already, why was consideration not brought to people crossing right at the bridge directly to the GO station? It seems to me that	<i>Response from City Staff:</i> This is one of our ultimate goals - although it is outside of the scope this first phase I would say – there is some constraint in completing that connection especially the level grade crossing with

#	Comment/Question Noted	Response
	the paths are meandering whereas it would save traffic from the GO train station if people could just walk across the bridge from the west side.	Stavebank and the railway and creating a safe connection to the GO station at the moment. So, for now this would be the first phase and hopefully in the long-term that is the goal.
14	<p>At the west end of the bridge, the route will end at a very tricky/dangerous location: Mississauga Rd @ Front St., the tunnel under the railway, the Kane Rd path, the Mississauga Rd. bike lane. How will these all resolve?</p> <p>It will be a bottleneck west of the Legion. Mississauga Rd. is a sharp blind bend under the rail bridge - traffic in both directions descends into a small valley under the tracks. Sightlines are terrible and cars tend to speed through there. But it's the only place to cross and connect to the Kane Rd. path and points further north or west. How about a set of traffic signals on Mississauga Rd. - one at Kane Rd., one at Front St N. - to treat the space in between as one big intersection?</p>	<p><i>Response provided by City Staff following the workshop:</i> The Schedule B EA will recommend as a future commitment during detail design that a traffic signal warrant and intersection assessment be completed for the Mississauga Road/Front Street intersection to assess the concerns you've raises.</p>
15	Earlier there had been discussion vis a vis Brightwater about allowing a shuttle bus (only) to traverse the bridge for the GO Station destination. Take it that is off the table for good?	Response from City Staff: This is a topic that we can talk about further in the future but for right now the focus is on pedestrians/cycling. What this question is referring to is not just a shuttle but an autonomous shuttle. At this point it is not part of our scope. For now, getting this forward is something we will have to hold off on.
16	This has many transportation benefits: GO Station 1st/last km; recreational for tourists/sightseers; and during festivals, provides a way to bring Legion patio into the "quadrangle" of landmarks for festivals (Lighthouse, Library/playground, Arena, Legion). Also, an opportunity for Legion to take on [office] tenants to generate revenue, and these tenants might	Comment is noted.

#	Comment/Question Noted	Response
	come in by GO Train or walk over from downtown Port Credit.	
17	Note the Front Street/Lakeshore Road signalized intersection is a key safe connection to the bridge area from the south of Lakeshore Road, so usage if multiplied. Then up Stavebank Road up to the new ped/cycling bridge over the QEW!!!	Comment is noted.
	<i>Comments about the format:</i>	
18	Hi there, how many participants do we have today? Why is the chat disabled? I have seen it on for about half of City consultations and not once seen any poor language or otherwise reason to close the chat. Please maximize engagement not turn this into a very isolating webinar, where we cannot see or interact with anyone. This format is very disappointing and feels like residents are being punished for some reason. Please reflect on this and follow-up on how you will do better next time.	<i>Response from Community Consultation Facilitator:</i> There are 38 participants plus presenters. It is important to ensure comfort for all that attend and not everyone is comfortable having their names visible. The presenters are visible to everyone. We generally do not show the names of those in attendance nor, do we read their names aloud when questions are read out. Attendees were advised that they could raise their hand and share input verbally. There is no limit on the number of questions or comments provided by any one individual. This commenter provided verbal comments at the meeting which are included in this feedback report.

4. PUBLIC FEEDBACK FROM THE ONLINE SURVEY

This report section includes the responses to the Online Survey. 166 respondents completed the survey between April 26 and May 10, 2022. The feedback in this section is organized by question and includes comments noted through the additional comment section at the end of the survey. 92 individuals provided comments. All comments are numbered for reference purposes and are verbatim.

Survey Question 1: What bridge type do you prefer for the new Credit River Bridge?

Response Choice	Number of Respondents
Concept #1: Arch Bridge	103
Concept #2: Truss Bridge	62
Total # responses	165

Figure 18 – Comments Noted about the two Bridge Design Concepts

#	Comments received
1	It is quite obvious that you are trying to discourage the Truss Bridge by your selection of pictures. In one a centre abutment is shown which is not at all the concept presented. People have a park to linger on the shores of the Credit River. That is not the function of a narrow ribbon bridge. This bridge should be as unadorned, low slung and simple as possible. Your survey bias is quite unprofessional and I as a taxpayer who is paying for this, I'm looking for a much more even-handed analysis. How much work did it take to find these sample pictures?
2	The bridge could (and should) be much closer to the water so that its height doesn't soar above the railway bridge, which has already set a standard for vertical clearance from the water.
3	Which of the two bridge types is stronger and would be likely to span the river without need for a central support? Constructing a pier in the river might increase the cost substantially.
4	The design should not impede the use of the actual river surface there is already an issue with people climbing up the rail bridge and jumping off which causes a huge public safety risk. Places to stop and admire the view in my opinion will cause more issues. the design of this bridge needs to minimize the options for people to do that from yet another structure There also needs to be ample clearance underneath and to allow visibility along the river.
5	I would love to see an arch-type bridge that is as "invisible" as possible so that the natural beauty of the river and banks can be the main features. Please ensure the historical significance of the Credit River is also highlighted in plaques, or viewpoints which offer a glimpse into what a specific spot used to look like (i.e., a diorama viewed through a telescope)
6	The artist concept of the Truss Bridge looks like the side of the truss bridge separates the bike & walk lanes. This seems risky because pedestrians won't easily be able to step out of the bike lane. Fully mixed doesn't seem like a good idea either. Both walking & biking down near Snug Harbor gets stressful on nice summer days, because there are so many pedestrians & bikers weaving around each other.
7	Design of the bridge is important in that it should be visually appealing both day and night. The arch design illuminated at night which enhances the outline of the bridge would make it appealing. During the day a design which highlights the curved arch design would present a look which represents nature. Nature is very curvy in appearance.

Survey Question 2: How important is it that the bridge stands out as a statement in the community?

Response Choice	Number of Respondents
Not Important	44
Somewhat Important	71
Very Important	51
Total # responses	166

Survey Question 3: How important is it that bridge blends into the environment and its surroundings?

Response Choice	Number of Respondents
Not Important	20
Somewhat Important	64
Very Important	82
Total # responses	166

Figure 19 – Comments Noted about the how important it is for the bridge to stand out as a statement in the community and, or how important it is that it blend into the environment and its surroundings

#	Comments received
1	The bridge should blend into the environment with concealed lighting, especially between 11 pm and 5 am, when lights are a nuisance for people living in the area. The bridge could (and should) be much closer to the water so that its height doesn't soar above the railway bridge, which has already set a standard for vertical clearance from the water. Walking and cycling lanes should be provided because bikers can be aggressive. Finally, there will be many rest areas and interpretive signs leading to the bridge, on both sides of the river, so let the prime purpose of the bridge be transportation across the river, not loitering.
2	Should blend in and support the rowing and canoe clubs. Allow people to view the boats during races. Minimize the shadow on the river.
3	The entire area is much more natural when compared to Humber. The Port Credit bridge should not stand out as an architectural statement, but more as a well blended environmental piece...that happens to have a useful function. Although completely different in every way, the Rattray Marsh boardwalk could be an inspiration in how it blends into the area and does not draw away or deter from the beauty of our area.
4	The Humber Bridge is a waterfront icon. This bridge is part of a river and marsh habitat and should reflect the qualities and natural environment of the conservation area. Mini design and lighting. Also, since it is next to a high use triple rail corridor, it's not a great place to stop and hang out socially, but there good be using opportunities. Please no fishing allowed.
5	Make it functional and pretty.
6	The most important element here is safety. Preventing people, especially kids, from crossing at either the train tracks or on the frozen river should be the priority. At the same time, it's important to plan with aesthetics in mind and create some thing beautiful in our environment. If having cosmetic vs. safety elements raises the

#	Comments received
	expense of the project so much that it can't be completed, then those should be scaled back to make sure it gets done, even if it doesn't turn out to be pretty.
7	Finally, a bridge to cross to help the kids travel on bikes or walk (and not close to lakeshore traffic!). Makes it easier to access arena and library and park for us on the west side of bridge. Also great for those running to catch the GO train. I see the bridge as functional for life in Port Credit. I do enjoy watching the train cross over the train bridge so I feel the new bridge should not impede the view! Recommend bridge without top to feel open and airy! or anything birds would sit on and poop. It has to be something easy to clean! Just an easy open airy space to cross. Nothing fancy. Keep it simple. Not an attraction. The river is the attraction already! Nature is the highlight
8	The colour can not be white. Colour should match the rail bridge to avoid contrast
9	Ensuring it blends in the surrounding infrastructure. Ample clearance height from the waters surface to ensure that any traffic (kayaks in the summer, skaters/walkers in the winter) can be easily move underneath.
10	Make it pretty!!!
11	A bridge like this is an opportunity to create a usable but beautiful public space much like the Humber Bay bridge and other such areas that attract people.
12	It would be nice if integrated some aesthetics that are popular in the local area like: Many of the new buildings integrate glass and brick materials; the local crosswalks have painted designs (piano at Lakeshore and Hurontario, fish at Stavebank and Hurontario).
13	Build the best bridge keeping human nature in mind. Some people will blast over the bridge on their bikes as they commute. Many dogs will bring their people out. And many people will stop and look out from the bridge while others walking in groups will try to dominate the space. Make it easy for everyone
14	Have you considered that the existing 120-year-old railway bridge is a local landmark, and have you looked at it this project from the perspective of conserving its cultural heritage value (I would suggest that it does meet the Provincial criteria)? It would seem to me, based on your renderings, that you could come up with a design the respects the view towards the historic bridge and still carefully insert a new bridge in front of it. This is an important consideration to me, and I would suggest that it should be a consideration of the project team if it isn't already.
15	It's too bad you can't integrate with the train bridge - should try to integrate somehow.

Survey Question 4: How does Concept #1 (Arch) reflect your ideas for the new Credit River Bridge?

Response Choice	Number of Respondents
Does not reflect my ideas for the bridge	31
Somewhat reflects my ideas for the bridge	54
Reflects my ideas for the bridge pretty well	80
Total # responses	165

Survey Question 5: How does Concept #2 (Truss) reflect your ideas for the new Credit River Bridge?

Response Choice	Number of Respondents
Does not reflect my ideas for the bridge	43
Somewhat reflects my ideas for the bridge	63
Reflects my ideas for the bridge pretty well	59
Total # responses	165

Survey Question 6: How important is it that lighting be concealed and integrated within the structure?

Response Choice	Number of Respondents
Not Important	39
Somewhat Important	43
Very Important	83
Total # responses	165

Survey Question 7: How important is it to incorporate accent lighting that highlights bridge features?

Response Choice	Number of Respondents
Not Important	36
Somewhat Important	62
Very Important	67
Total # responses	165

Figure 20 – Comments Noted about Lighting on the Bridge

#	Comments received
1	Make it well lit.
2	Don't let it be too bright. We have too much ambient lighting already.
3	Lighting for user safety only. We do not need the bridge lit up like a Christmas tree. It disturbs nearby houses and wildlife along the river. No more light pollution. Lights off by 11 pm.
4	The bridge should blend into the environment with concealed lighting, especially between 11 pm and 5 am, when lights are a nuisance for people living in the area.
5	Lighting on winter nights is a very important safety feature for those who will be walking alone after dark, not late.
6	Use a low level of lighting after sunset, supplemented with higher intensity lighting activated by motion within 25 or so metres of the bridge.
7	It is very important to us that you minimize light on the structure at night. In fact, no light would be best. We like the low-level lighting to illuminate the path only.
8	Putting lights on it is just adding to light pollution.
9	Use solar power for the lighting.

#	Comments received
10	Bridge must be well lit at night!
11	There also needs to be consideration of light pollution into the river surface affecting wildlife and users
12	Also, as a local resident looking down upon Memorial Park, I would be concerned if the bridge were lit up like a beacon at night with light spilling out everywhere.


Survey Question 8: How important is it that the bridge allows all users to pause and spend some time on the bridge?

Response Choice	Number of Respondents
Not Important	9
Somewhat Important	48
Very Important	128
Total # responses	166

Survey Question 9: Which type of functional cross-section do prefer?

Response Choice	Number of Respondents
Cycling and pedestrian areas are separate	58
Cycling and pedestrian areas are separate, with centre-running cycling lane	83
Cycling and pedestrian areas are shared, with cyclists encouraged to slow down	24
Total # responses	165

Figure 21 – Comments about Accommodating Different Users

#	Comments received
1	If the goal is a transportation bridge to get around faster, it should have a priority lane for those moving around faster and not mix the two speeds of travel (walking and biking). If the intent is to be more scenic, I'd prefer the mixed traffic path.
2	Ensure the bridge is accessible for people with disabilities including but not limited to wheelchair users, people with sight impairments or hearing impairments.
3	Please, please separate the cycling from pedestrian areas. As both, I am aware of the needs of both whereas most pedestrians are oblivious to cyclists. Most pedestrians expect cyclists to go really slowly which as a bike commuter is annoying. I hope this bridge will encourage more bike commuting especially to the rail services.
4	For safety there should be a division in the middle of the bridge. Cyclists & runners use one side of the bridge with eastbound westbound arrows keep them moving in the separated direction. And same for pedestrians & disabled population using walkers/  . Take a look at Melbourne, Australia's Point Orman park that is ideal & safe. Create this bridge properly the first time around.
5	Walking and cycling lanes should be provided because bikers can be aggressive.

#	Comments received
6	I think there needs to be a physical barrier between the pedestrian and biking lanes. It doesn't need to be huge but high enough to prevent pedestrians from wandering into the biking lanes without knowing. The Humber Bridge is a biker's nightmare as pedestrians are focusing on the view, taking pictures. and even at a slow speed, bikers have to be ready for the unexpected. It's not safe for either party.
7	Clear signs to explaining pedestrians and cyclists bridge rules
8	Pedestrians should maintain right of way on the bridge with cyclists advised to slow down or dismount when crossing
9	Separate bike & walking lanes seems important for safety reasons, but there shouldn't be a physical barrier / separation between the two. If a big group of pedestrians is walking down the bridge and there's nobody else on the bridge, they'll spread out to use the bike lane because why not? Then if some cyclists show up, they'll want to be able to step out of the bike lane back into the pedestrian lane. Dividing the bridge into bike & walk lanes seems ideal. Maybe the lines can be designed into the bridge, like different material for the ground or different colored wood / stone, instead of just paint. Something artistic. I think the pedestrian side should be on the south side, since that will have the better view. The north side just looks out at the rail bridge. As an engineer I do think the rail bridge is awesome and I want a close look at it, but I think most people will prefer to look out over the lake & park.
10	Need very clear cycling vs. walking dividers. The side looking towards the city needs space for standing and viewing. We don't want people to stop and push walkers into cycling lanes. The bridge needs to be wide; the Humber Bridge is a great example
11	We are avid cyclists, and we highly recommend no designated lane on the bridge itself. This actually encourages people to ride fast, and they should not. A minute delay on a long ride is just fine for the sake of safety and enjoyment by all. The Humber Bridge works very well as an example.
12	Remember in the spacing of the pedestrian corridor that there will be people with wheelchairs and walkers using the bridge. There are quite a few in my building and they will use the bridge as it is very close to the building.
13	Having a crossing where pedestrians and bikers share the same space and asking bikers to slow down is dangerous and doesn't work. Need separate spaces.
14	I recommend keeping spaces for both pedestrians and cyclists defined on the bridge. As an avid cyclist along the Waterfront Trail, the Humber Bay Arch Bridge is always a mess and both pedestrian traffic and cyclist traffic are merged into a shared space, which always creates traffic on the bridge and conflict. Having defined space for each party would improve mobility for cyclists and pedestrians without obstruction.
15	Please have very clear markings for the bike lane. Some of the green paint used for city/bike intersections along Eglinton really stand out.
16	As for "separate bike vs pedestrian lanes" I would say that should match the inbound/outbound trails. If there is bike lane from bridge to Port Credit GO Station that is amazing, and if it connects with bikelines on Mississauga Road that is an amazing outcome.
17	Making bike and pedestrian areas very clear and defined is extremely important. In other projects, it is very easy for all users to co-exist if it is clear and reasonable where everyone is expected to be. Simply telling some users to slow down doesn't work.

Survey Question 10: How important is it to include look out areas and seating?

Response Choice	Number of Respondents
Not Important	19
Somewhat Important	63
Very Important	82
Total # responses	164

Figure 22 – Comments about Look Out Areas and Seating

#	Comments received
1	I am concerned that incorporating viewing areas in the middle of the bridge is potentially dangerous. As a frequent user of the Waterfront Trail, I am concerned about the speed of cyclists and the potential blocking of traffic by "viewers". I am concerned that a viewer might step back into the cycling lane while stopping. I think viewing platforms should be at either end of the bridge, not in the middle. Besides what is the point of a viewing area on the north side anyway when it is hard up against the train bridge? Also, as a local resident looking down upon Memorial Park, I would be concerned if the bridge were lit up like a beacon at night with light spilling out everywhere.
2	Pedestrians are most likely going to want to stop on the park side, not the railroad bridge side, so seating or pausing areas should be on the park side. So, if cycling is in the middle, then the downstream pedestrian side should have the seating areas, and if cycling is on one side the cycling should be on the upstream side.
3	There will be many rest areas and interpretive signs leading to the bridge, on both sides of the river, so let the prime purpose of the bridge be transportation across the river, not loitering.

Survey Question 11: How important is it that the bridge functions as a place to pause, admire the view, socialize, spend some time instead of just a means to get from Point A to Point B?

Response Choice	Number of Respondents
Not Important	21
Somewhat Important	61
Very Important	84
Total # responses	166

Figure 21 – Comments about the function of the Bridge as place to spend some time instead of just a means to get from Point A to Point B

#	Comments
1	Definitely a place to stop and view but not encourage people sitting indefinitely. It's not a picnic area.
2	There are lots of places to pause along the Credit River, including in the park where the bridge will be. I would recommend that the primary purpose of the bridge - as a means for transportation/connections - be kept as the focus of the design.

Additional comments provided that respondents would like considered as the final concept is developed are included in **Figure 23**. These are verbatim.

Figure 23 – Other Comments received

#	Comments received
	<i>About the suitability of the AT bridge:</i>
1	This whole project is a complete waste of tax dollars and in fact may create another safety hazard for users of the river. Currently the existing train bridge is often used as a diving platform for kids jumping into the river. Installing another place to jump from or drop things from onto unsuspecting river users is a huge liability. This bridge is unnecessary- it is only a block south to access the two current underused pedestrian bridges on Lakeshore Rd.
2	In our opinion this bridge is in the wrong place, the concept is good, but the bridge would be better and used more (especially for pedestrians) if it is built adjacent to the Lakeshore bridge on the north side of the existing Lakeshore bridge over the Credit River. Sadly, the trestle bridge which is a part of history in Port Credit will be obscured if the proposed designation goes ahead.
3	Not sure how much pedestrian traffic is expected for this bridge. Cyclists will definitely use it to bypass lakeshore from Mineola to get to Mississauga Road, but I'm not sure how many pedestrians are taking the same route. Most of the scenery and retail is down on Lakeshore. Looking forward to the bridge though, as a cyclist.
4	Not sure what would attract people to go about 3/4 kilometer from Lakeshore Road to this bridge.
5	It's going to cause problems with events, such as the Southside Shuffle. Will need to either be closed for the event, or organizers will have to fence off the pathway and direct Bridge users into the Arena parking lot, past the skateboard park.
6	Why is this bridge so far away from the Lakeshore? I don't understand who will be using it considering the bus routes and all the shops and action happen along the Lakeshore. Seems like a waste of money to make a visually appealing bridge over by the train tracks where no one will really see it. Have you seen the credit river? It's muddy and boring. There's nothing to look at there, why on earth would you consider seating or "lookout points". Putting lights on it is just adding to light pollution. No woman will want to walk along there alone after dark, it's too far from the main road, where other people might be around. Money would be better spent with a visually

#	Comments received
	appealing bridge closer to the Lakeshore where people will actually see it, use it, and connect it to the visual appeal of Port Credit. If you insist on putting it way over by the train tracks, make it basic/utilitarian and don't waste money on making it "pretty".
7	The sooner the better! I'd love to ride to the trail on Mississauga Rd, but I don't feel at all comfortable riding on Lakeshore Rd. to get there and I don't like to ride on the sidewalk because people get mad (and I think it's illegal). This bridge would be very welcome for cyclists like me (a senior).
8	I think it is a wonderful plan! I live close to that area, and I go at least twice a week to enjoy its beauty 💕. Wish you the best 😊. God bless You all!
9	Fantastic idea that would encourage more people to walk and cycle to the Go Train, taking cars off the already jammed Lakeshore.
10	This is a great idea and we'll overdue. I'm really looking forward to it becoming a reality.
11	Long overdue
12	This is a fantastic idea altogether...!!!
13	Good idea, but with the current routing proposed, there still remains the problem that Stavebank does not connect with Stavebank over the QEW. Please try to connect these streets into a single point of travel for cyclists and pedestrians. The only N-S option remains Hurontario if there is no connection! Also, I hope that there will also be a bridge alongside the QEW over the Credit River, and that this is not the only one that there will be. The golf course is a massive impediment to active transportation in the area presently.
14	Going to be amazing!!
15	Important project, I'm very supportive.
	<i>About cost and timing for implementation</i>
16	Length of time to build it and not a penny over budget and builders minimum 10 plus years guaranteed with no issues or they replace no cost to the City of Mississauga communities' taxpayers, no fishing from the bridge.
17	Let's stay on budget and on schedule. Looking forward to the improvements. Thank you.
18	Stop delaying and get on with the project. Your timeline has been a joke. Public groups would have had this done years ago.
19	Please build this bridge ASAP. The existing bridge is dangerous for cyclists.
	<i>About transportation connectivity and traffic and parking</i>
20	The bridge should link with path or trail to the Go Station to promote non-car commutes to the station. As well as connect well for people visiting the area travelling by bike or foot coming in on the Go train.
21	There needs to be accommodation for cyclists and pedestrians crossing Mississauga Road in the location of the active transportation route. Presently, there is no signal, which is dangerous. It could be pedestrian activated.
22	There should be adequate parking.
23	Just make sure that access through parking lots on both sides is clearly protected
24	What happened to the road bridge to the GO station in exactly the same location to alleviate traffic on Lakeshore, in particular the new development on the oil lands? Without the roadway, there is really no need for any bridge, except to send tax dollars

#	Comments received
	unnecessarily. The walkways currently by lakeshore road are never busy for pedestrians or cyclists.
25	Yes - you will need to make Mississauga Rd south more of a family and bike friendly road. I would reduce car traffic through landscaping and dedicated bike paths. Mississauga Rd can not be a primary car route to access Lakeshore, it should be the primary, walking and cycling road to Port Credit. Crating a bridge such as this from a road that is extremely busy and overrun with speeding cars. Someone will be run over for sure.
26	The pathway to Stavebank / the Go station/LRT should be built at the same time or before this - right now there is a dirt hill with a barrier that kids and others can hurt themselves on
27	Look at a solution next for creating a straight passageway behind Loblaws in order to facilitate a safer means to get all the way from Lorne Park Rd to the arena parking lot (avoiding a detour through lakeshore). Also, a safe way to cross Mississauga Rd. which can get quite busy would be a great way to make it more usable.
	If bikers are encouraged to use this bridge, the city should ensure that there are connected bike paths/lanes to the rest of Mississauga to accommodate for the increased demand of bikers that come as a result of the bridge & path.
28	How are we considering the bridges connections to rest of waterfront trail? The connection on east side beside the roundabout seems odd. This bridge should have a straight-line connection for west-of-credit-river residents able to walk to PC go station most efficiently. As the city considers credit river west parks, how can we continue Mississauga Rd bike lanes through these new parks/adjacent to them and stem all the way down to the waterfront?
	<i>About Interpretative Elements (public art, planters, signage, etc.)</i>
29	Include a public art component if possible. Is it possible to incorporate colour elements? So many recent public projects are white coated steel or unpainted/brown steel.
30	Some type of flower planters or hanging planters would make the area much better and more scenic as well as the other art forms.
31	Some history about the rail lines and the trains that use to service Port Credit. Also, some aboriginal history.
32	Please ensure the historical significance of the Credit River is also highlighted in plaques, or viewpoints which offer a glimpse into what a specific spot used to look like (i.e., a diorama viewed through a telescope)
33	Support the idea of hiring indigenous artists for design/interpretive elements.
34	I also really love the plaques that the city puts up in parts. It would be cool to have a plaque showing historic information of the rail bridge, maybe one for the rail bridge that goes over Mississauga Road too. Historic plaques are awesome.
35	References to the adjacent buildings (MCC, Don, arena, rail bridge, legion) and individuals having history with them should be included
36	Be sure to include an Indigenous placard along with some history about the rail corridor as well as the community of Port Credit.
	<i>About materials, safety features and maintenance</i>

#	Comments received
37	Please consider using sustainable natural granite pavers. They are 1000 times better for the environment than our crumbling concrete man made products that are so commonly used here.
38	Material choice (ensuring its weather resistant). Planting along the bridge (incorporating year-round interest and lushness to the structure).
39	Will the bridge have safety features to make sure people will not jump off the bridge? Will the bridge be patrolled at night to prevent people hanging around making noise after hrs? Will it be closed off after hrs to prevent graffiti or damage to the bridge? Will it be maintained in winter months?
40	In order to encourage an increase in Active Transport for commuting and recreation alike, please ensure that the bridge design accommodates snow and Ice removal services by the city. Many pedestrians and cyclists capable of using sidewalks, bike lanes or multi-use trails are forced into motor vehicles when ice and snow is allowed to accumulate. This is not because of bitter cold in Mississauga, but hazardous walking or cycling surfaces due to poor urban design and maintenance. Thank you.
41	Safety for children is important
	<i>Other comments about the design</i>
	Make sure there are trash cans incorporated to prevent littering.
42	Surface of the bridge is important, safety, maintenance Don't want it to be a hang out area for kids at night but to make a space that is inviting, useful and relatively maintenance free. Balance these objectives. This will be a great addition to PC. So pleased this is going ahead.
43	Want to make sure it's high enough above the water to allow paddle boarding under it without needing to kneel to get underneath unless the river is flooded
44	Make sure to minimize any negative impact on the rowing and canoeing community.
45	There are a lot of rowing boats and kayaks that need to pass under the bridge. Will the proposed height ensure safe passage for boaters when the water is high? The current train Bridge creates hazardous conditions when the water levels are high.
46	The design should not impede the use of the actual river surface there is already an issue with people climbing up the rail bridge and jumping off which causes a huge public safety risk. Places to stop and admire the view in my opinion will cause more issues. the design of this bridge needs to minimize the options for people to do that from yet another structure There also needs to be ample clearance underneath and to allow Visibility along the river there also needs to be consideration of light pollution into the river surface affecting wildlife and users
47	Should blend in and support the rowing and canoe clubs. Allow people to view the boats during races. Minimize the shadow on the river.
48	Earth is flat and NASA and the gov lies. Please learn and share the truth.
49	Thank you for the work that went into this survey. I appreciated the concept photos!
50	Make sure to include enough trash/recycling bins on either side of the bridge. Will electric vehicles be allowed? Hope not. Needs to be safe for pedestrians & wheelchairs.

#	Comments received
51	Just a small faux pas in your wording. Since the Credit River runs north and south, it would be more appropriate for you to refer to the west side and east side of the river, not the south and north side
52	Access to waters edge i.e., canoe/kayak drop, perhaps a bike locker or stand while enjoying the surroundings
53	How does the bridge relate to the Train Tracks, and should the language of the bridge be similar? How will the bridge impact the rowing club and their predominate usage of the river? Fireworks are lit at the arena so the bridge should consider how it attracts and promotes viewing of those key community events. Having the bridge copy the Humber bridge doesn't make it iconic. Many Bridges in Mississauga are very similar so how will this bridge be set as a landmark separate from those. How does the bridge respond to the lighthouse and reflect the cultural elements Port Credit already resembles?
54	Need to be able accommodate the FIRE boat to pass under. Need to have fire access and anchor points for firefighters to do rope rescues and water rescues.
55	Additionally, please consider how the bridge is likely to be used by people fishing and if there need to be any rules (i.e., no fishing off bridge, designated sections of bridge). My preference would be no fishing from bridge Considerations for a small advertising/billboard somewhere tasteful to allow local Port Credit businesses to buy a rotating ad space.